



## PLANNING & DESIGN REPORT **(revised)**

415-421 BOLER ROAD, LONDON

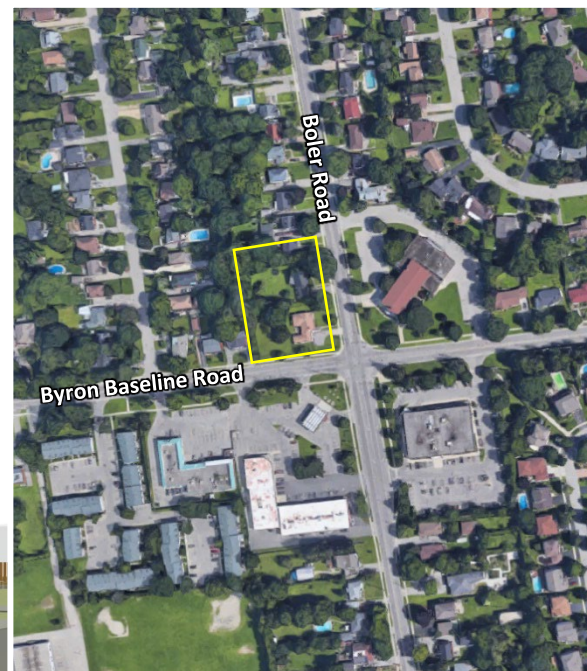
### Proposed Site-Specific Zoning Bylaw Amendment to permit a 6-Story Apartment Building

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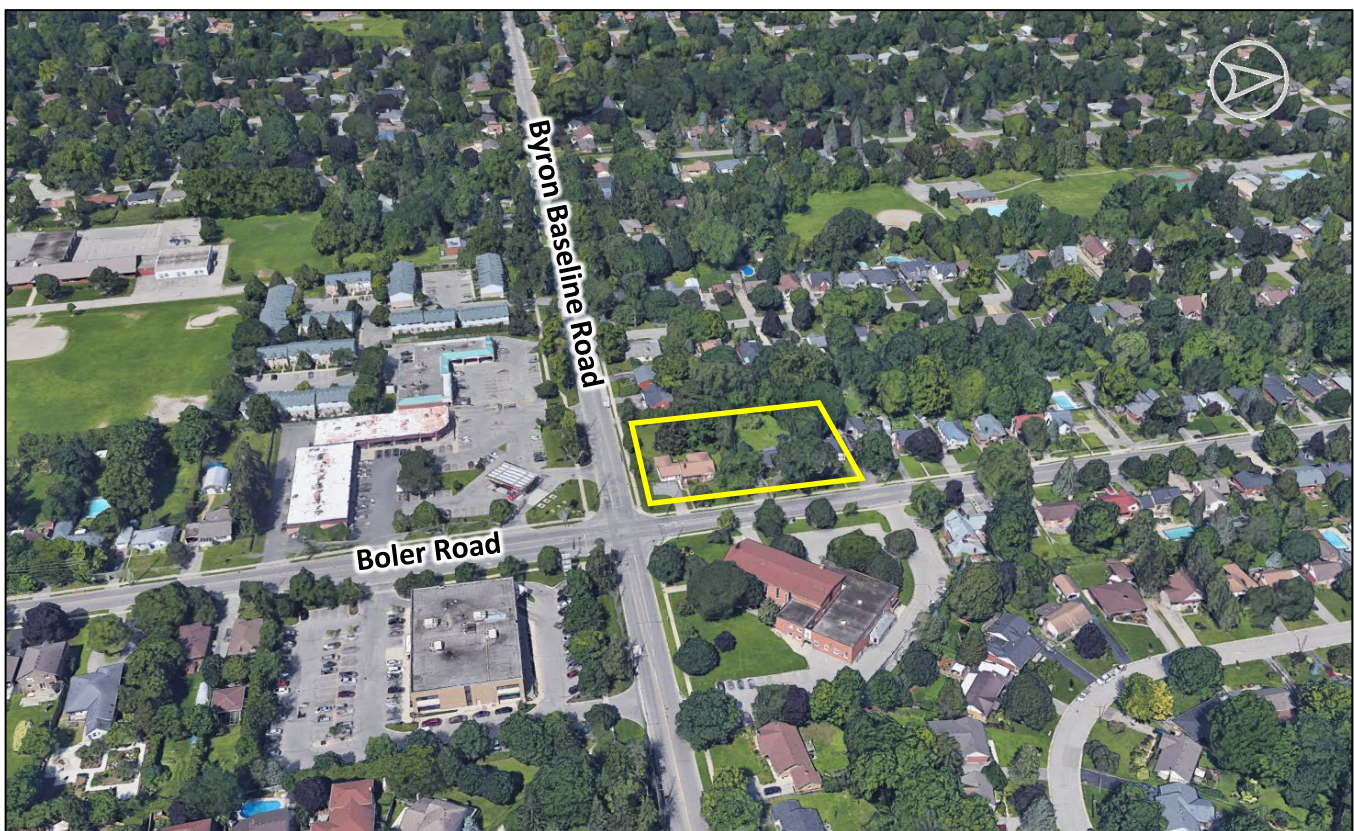
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## 1 INTRODUCTION

The purpose of the following land use Planning Justification Report is to evaluate the proposed Zoning By-Law Amendment (ZBA) application for the lands at 415-421 Boler Road (the “subject site”) within the context of existing land use policies and regulations, including the Provincial Policy Statement, The London Plan, and the City of London Zoning By-law.

The applicant is seeking to amend the Zoning Bylaw to permit the consolidation of three single detached lots to permit the development of a medium density, mid-rise apartment building with 62 dwelling units and associated parking.



**Figure 1.** Aerial view of subject site at 415-421 Boler Road, looking west  
Source: Google Earth)

### 1.1 PRE-APPLICATION CONSULTATION

A Pre-Application Consultation meeting was held on January 20<sup>th</sup>, 2020, including staff from the City of London and the former landowner. At the meeting, several site design recommendations were received, and the building design had been revised.

## 2 SITE DESCRIPTION

The Subject Lands are located in the Byron community of London and situated at the intersection of two prominent roads: Boler Road and Byron Baseline Road (see **Figure 1**). The Subject Lands would be an amalgamation of three (3) existing lots (#415, #417, and #421 Boler Road), which are currently occupied by single family residential homes (refer to **Photos 1-3**). The resulting Subject Site would consist of a rectangular-shaped lot that is approximately ±0.44 ha (±1 ac) in area. The Subject Site is provided with frontage onto both Byron Baseline Road and Boler Road which are both Civic Boulevards (as identified on Map 3 of the London Plan) with traffic volumes between 8,000-14,000 vehicles per day (per CityMap online) and located at an intersection that is controlled by traffic lights.

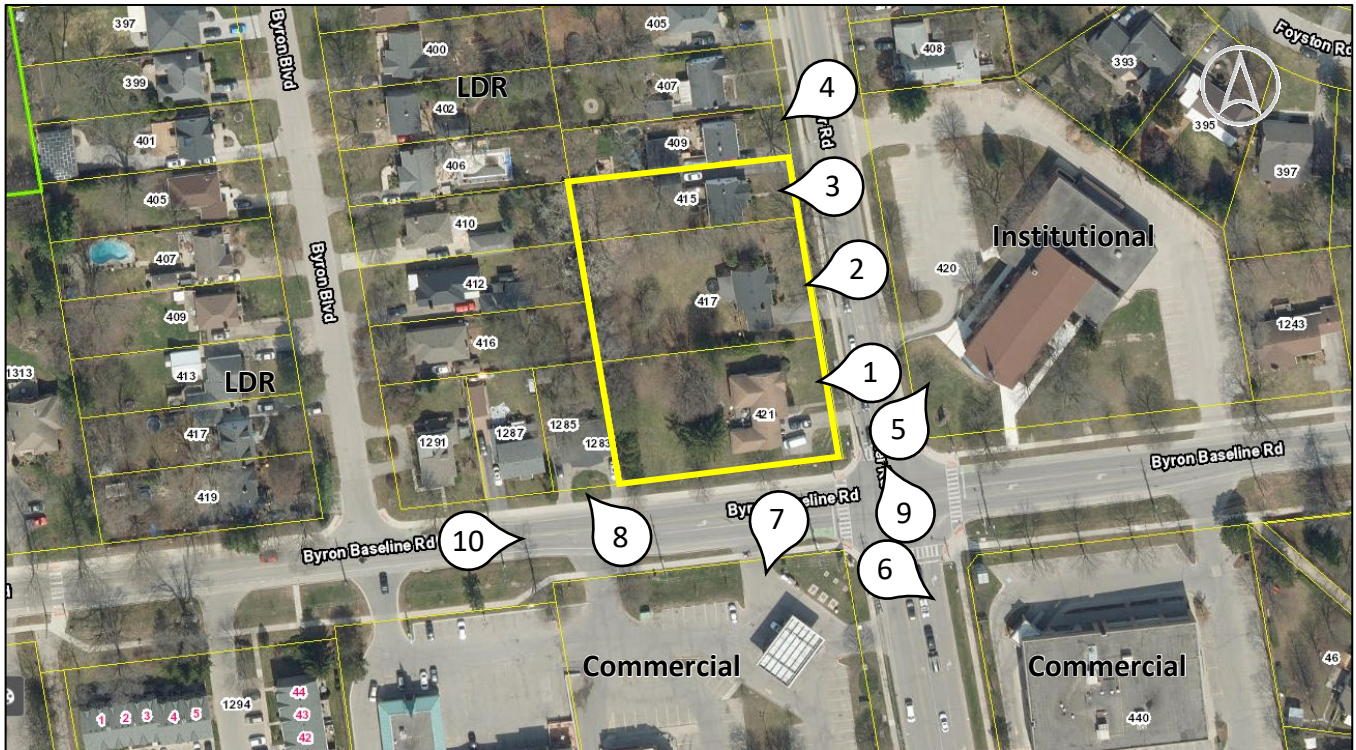


Figure 2. Location and Photo Key Map



**Photo 1.** View of 415 Boler Road looking west from Boler Road.



**Photo 2.** View of 417 Boler Road looking west from Boler Road.



**Photo 3.** View of 421 Boler Road looking west from Boler Road.

### 3 SURROUNDING LAND USES

The surrounding land uses consist of a mix of low density residential development, institutional and commercial uses. More specifically:

- To the north and west are single and semi-detached dwellings (R1-8 Zone) within the Byron community (See **Photo 4** and **Photo 8**);
- To the east across Boler Road is the Byron United Church (NF Zone – **Photo 5**) and single detached dwellings (R1-9 Zone); and
- To the south of the site and across Byron Baseline Road, includes townhouses (R5-4 Zone), professional offices (OF Zone), and commercial development (NSA1/SS Zones) consisting of gas station, restaurants, retail and personal services uses, among other uses (**Photo 6** and **Photo 7**).



**Photo 4.** View of single detached lots to the north (407 and 409 Boler Road) looking west from Boler Road.



**Photo 5.** View of Byron United Church looking east from Boler Road.



Photo 6. View of (Royal Bank) professional office centre looking southeast from Boler Road.



Photo 7. View of community shopping centre looking south from Byron Baseline Road.



**Photo 8.** View of 1283-1287 Byron Baseline Road, looking north of Byron Baseline Road.

## **4 SPATIAL ANALYSIS**

### **4.1 REGIONAL SPATIAL ANALYSIS**

The Subject Site is located within the Byron Neighbourhood which is a mature neighbourhood comprised of low to medium density residential, commercial amenities, office/employment opportunities, open spaces, and schools. **Figure 3** illustrates the community context within a 400m and 800m radii, representing straight line walking distances of 5 and 10 minutes, respectively.

Within walking distance, the Subject Site is offered several amenities that is attractive for new residential development including:

- Northwest of the site, Byron Optimist Community Centre offers community recreation opportunities, playgrounds, fields, pool, and tennis courts, among other programming.
- Southwest of the site is the Byron Southwood Public School and playground.
- North along Boler Road is the Byron “Main Street”, approximately 800m in distance, offering a variety of shops, banks, restaurants, grocery store, among other amenities.

Throughout the community and within an 800m distance, surrounding features include several school and public parks, pedestrian pathways, and other recreational opportunities within Springbank Park.

Bus transit is provided to the Subject Site via Boler Road and Byron Baseline Road, as well as Commissioners Road to the north, providing access to Routes #5 and #17 Byron – Argyle Mall. Several Bus stop facilities are located to within 100m on Boler Road and Byron Baseline Road, providing convenient access to transit.



Commercial/employment uses are allocated in two nodes within 800m of the Subject Site. Along Commissioners Road to the north is the *Byron Main Street Commercial Corridor* and directly south of the Site, across Byron Baseline Road, is a shopping center serving the day-to-day needs of the community with gas station and several commercial uses, including retail, restaurants, service uses, and dry-cleaning, and pharmacy.

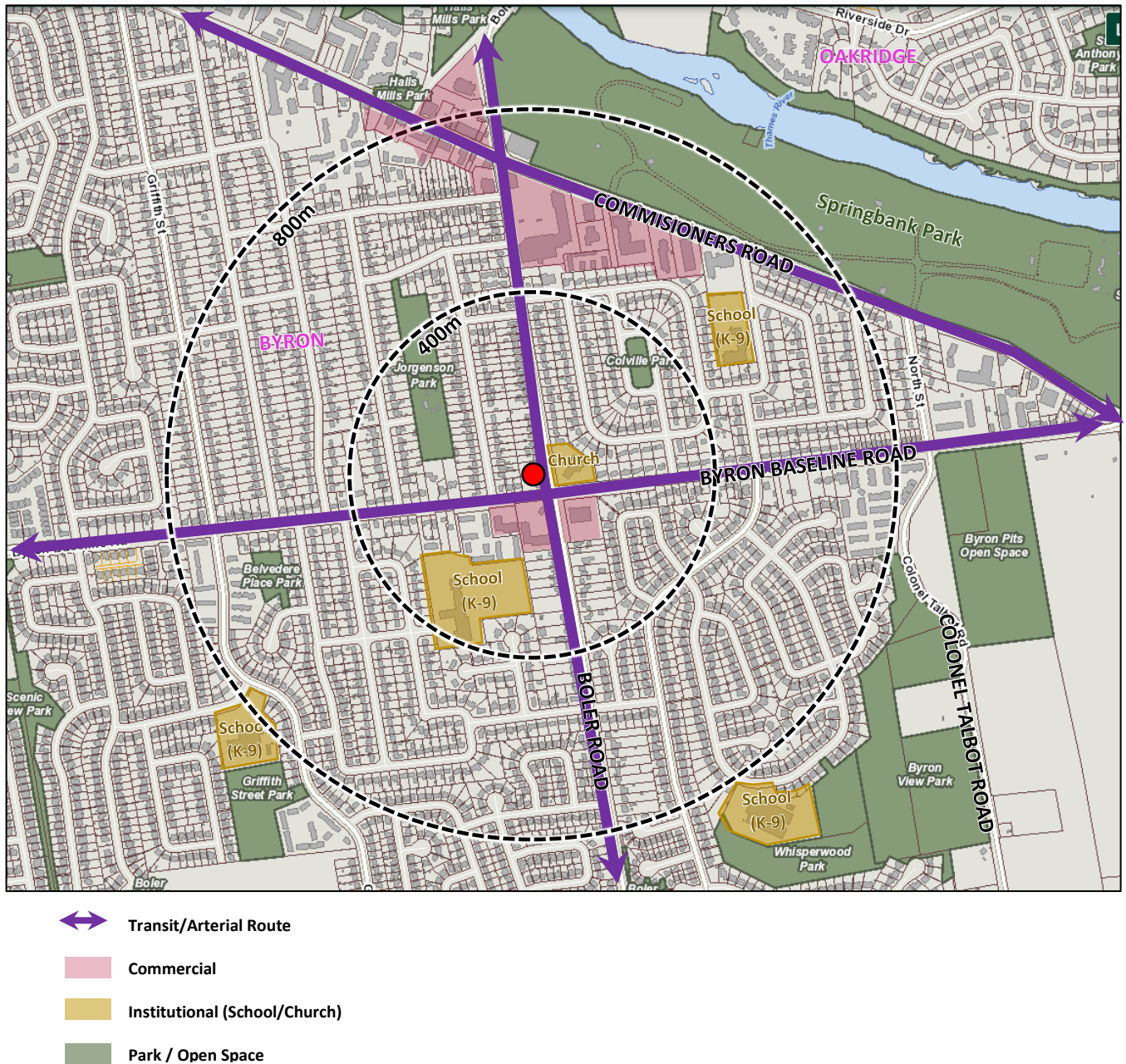


Figure 3. Regional Spatial Analysis (400m and 800m Radius)

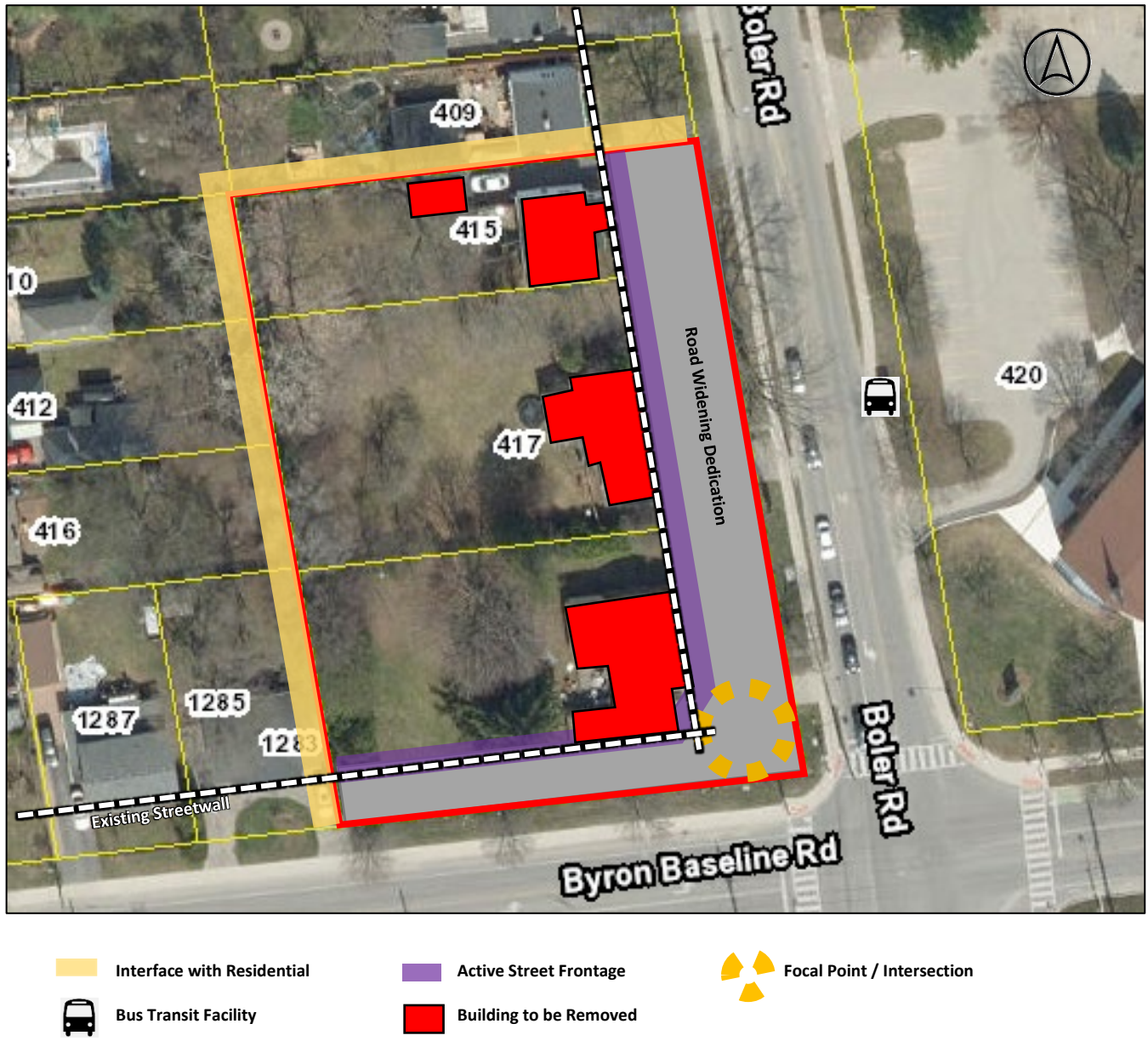


Figure 4. Site Specific Spatial Analysis

## 4.2 SITE SPECIFIC SPATIAL ANALYSIS

A Site Specific Spatial Analysis was conducted on the Subject Site (Figure 4). The three existing single detached dwellings would be demolished and removed from site to make way for the proposed building.

The subject site is located at a prominent intersection with “Active Street Frontages” serviced by bus transit services within 100m. The two “frontages” interface with the public realm (Byron Baseline Road and Boler Road), including sidewalks. The intersection of the two streets create a focal point that that are viewed by the travelling public approaching the intersection in all directions.

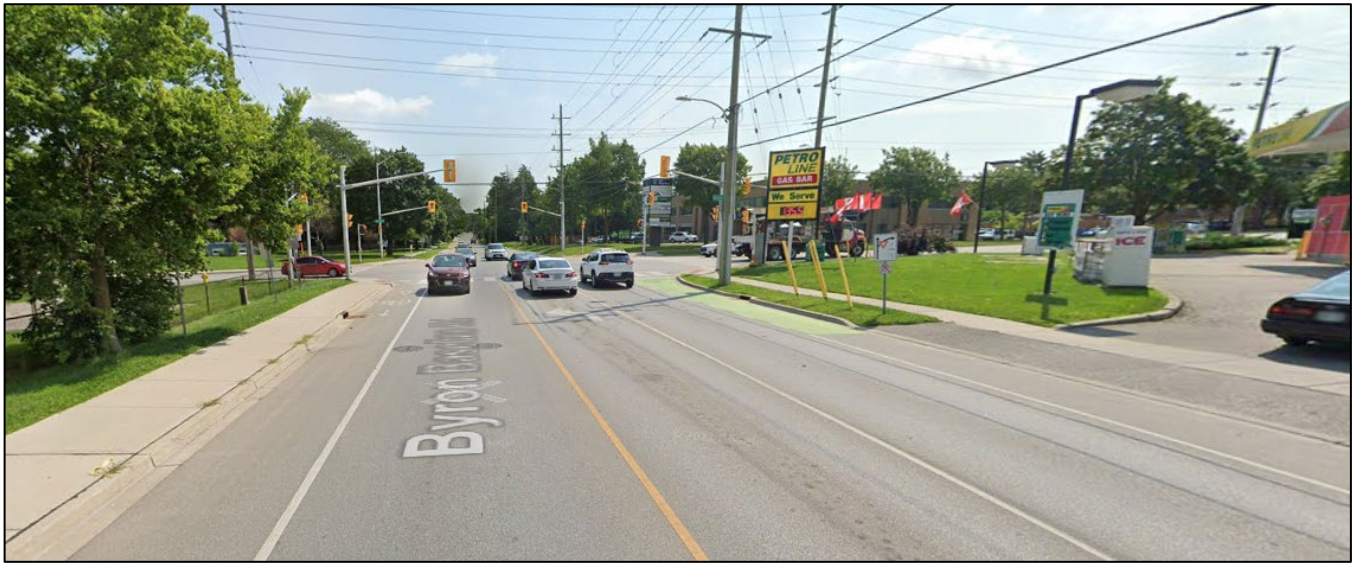
The north and west property boundaries abut low density residential land uses with single detached and semi-detached buildings. The proposed development would be situated with the greatest setbacks from these two boundaries, maximising separation from the existing residential dwellings.

Conversely, significant road widening (land dedication) is also required along both street frontages, reducing the subject site's developable area and shifting the building to a more central location within the existing property boundary.

Sidewalks are currently located on both sides of Boler Road and Byron Baseline Road allowing direct pedestrian connectivity to the proposed development. Additionally, on-street bicycle lanes, both east bound and westbound, are located on Byron Baseline Road, providing additional east-west connectivity for cyclists. **Photo 9** and **Photo 10** depict the existing streetscapes along the south and east boundaries of the Subject Site.

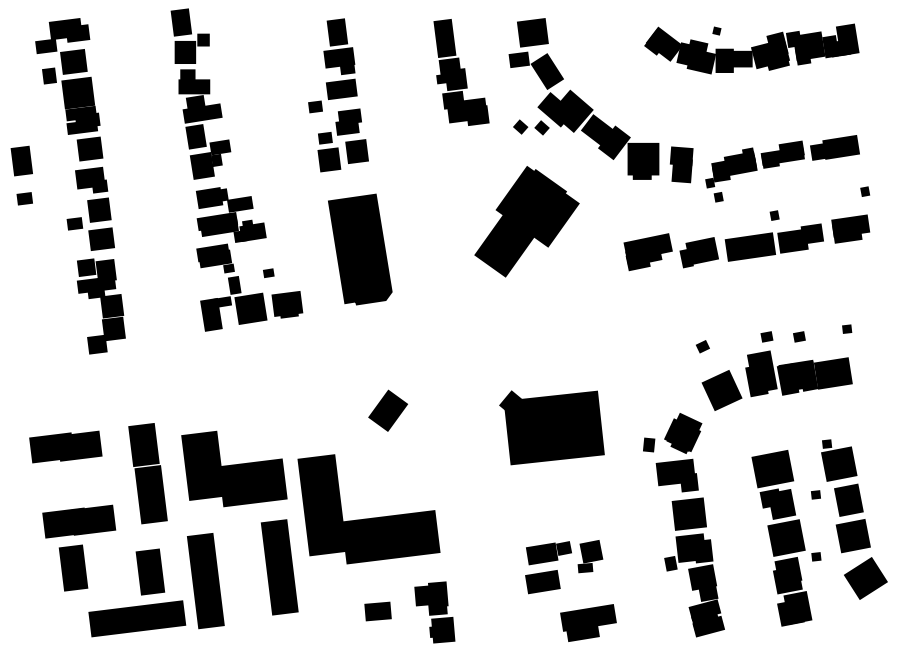


**Photo 9.** Existing roadway and sidewalk context on Boler Road, looking north from intersection  
Source: Google Streetview



**Photo 10.** Existing roadway and sidewalk context on Byron Baseline Road, looking east toward intersection  
Source: Google Streetview

The Figure-Ground Diagram (**Figure 5**) illustrates the arrangement of buildings (black) and unbuilt space characterizing the built form that is surrounding the subject site. The proposed building is shown, reflecting a comparable built form with proximal larger buildings at each corner of the Boler/Byron Sideline Road intersection, representing the existing church, office building and shopping centre. Smaller buildings are located further from the intersection, representing the existing 1- and 2-storey single/semi-detached buildings. Longer building are located in the southwest corner of the diagram, representing a medium density residential townhouse development west of the shopping centre. The proposed building, depicted in this diagram, would be generally consistent with existing building pattern.



**Figure 5.** Figure-Ground Diagram

## 5 **DESIGN GOALS & OBJECTIVES**

The proposed development at 415-421 Boler Road intends to intensify and reanimate the prominent intersection of Boler Road and Byron Baseline Road with a 6-storey residential apartment building. The following overarching goals for the proposed development are:

- To create a landmark building and focal point to enhance the Boler Road / Byron Baseline Road intersection.
- To design and build an attractive residential building that is integrated with the aesthetic of the existing streetscape and context.
- To maximize infill opportunities that make efficient use of land and available municipal services, infrastructure, and transportation facilities.
- To contribute to the provision of alternative housing types that are reflective of market demand and supportive of housing choice.

To achieve these goals, the following design objectives have been used to guide the conceptual design:

- Design a residential apartment building that has a modern and contemporary aesthetic that establishes a welcoming entrance to residents and visitors on both Boler Road and Byron Baseline Road.
- Design street façades at the human-scale using variations in colour/lighting/materials, building articulation, weather protection, and emphasis on entrances to direct pedestrian flows.
- Reduce the impact on neighbouring properties using building setbacks and architectural techniques to reduce massing.
- Locate screen surface parking to allow for building and landscape elements to enhance the public realm.
- Provide large private balconies and amenity space that will foster a livable residential environment for its inhabitants.
- Utilize high-quality materials, such as but not limited to brick masonry veneer and glass, with particular emphasis given to building entrances and human scale architecture.
- Provide walkways that connect public sidewalks to building entrances, and parking area.
- Promote walkability and alternative modes of transportation by integrating sidewalks throughout development, providing bicycle parking/storage, and direct access to nearby transit facilities.

## 6 DEVELOPMENT CONCEPT

Located in the established Byron neighbourhood of London, the proposed development is akin to the community's dynamic culture and character. The proposed development envisions a 6-storey, 62 unit apartment building, to be developed on the 0.46 ha site as an infill project. The conceptual Site Plan layout is illustrated in **Figure 6**.

The proposed building would be oriented to the intersection of Boler Road and Byron Baseline Road, along the street edge after road widening allowances. The building would be located furthest from the neighbouring single detached residences, as much as possible, to reduce the perceived height impacts along the west and north property limits of the Subject Lands. Window placement has also been considered to reduce overlook.

As a corner lot, the focus of the site design would be to create a sense of enclosure for the pedestrian realm and appropriately relate to the commercial nodal intersection. Ground floor entrances would have direct access to the streets, connected via walkway to the public sidewalk and parking areas.

A significant building stepback on the north side of the building would reduce the height adjacent to the single detached buildings. A rooftop patio would be provided above the fourth floor to create a private outdoor amenity for residents.

Vehicular access to the site would be provided from both adjoining streets. Entrance from Boler Road and Byron Baseline Road would provide access to parking located to the rear of the building as well as located within the first floor of the building. In total, the proposed development would accommodate 63 parking spaces. .

The proposed intensification can be considered an efficient use of available land and existing municipal resources while providing compact development in the form of apartment housing in a predominantly single detached residential neighbourhood. This diversification allows for a wider range of household types, ultimately increasing affordability within the highly desirable Byron community, as well, it allows for the opportunity of aging generations to "age in place".

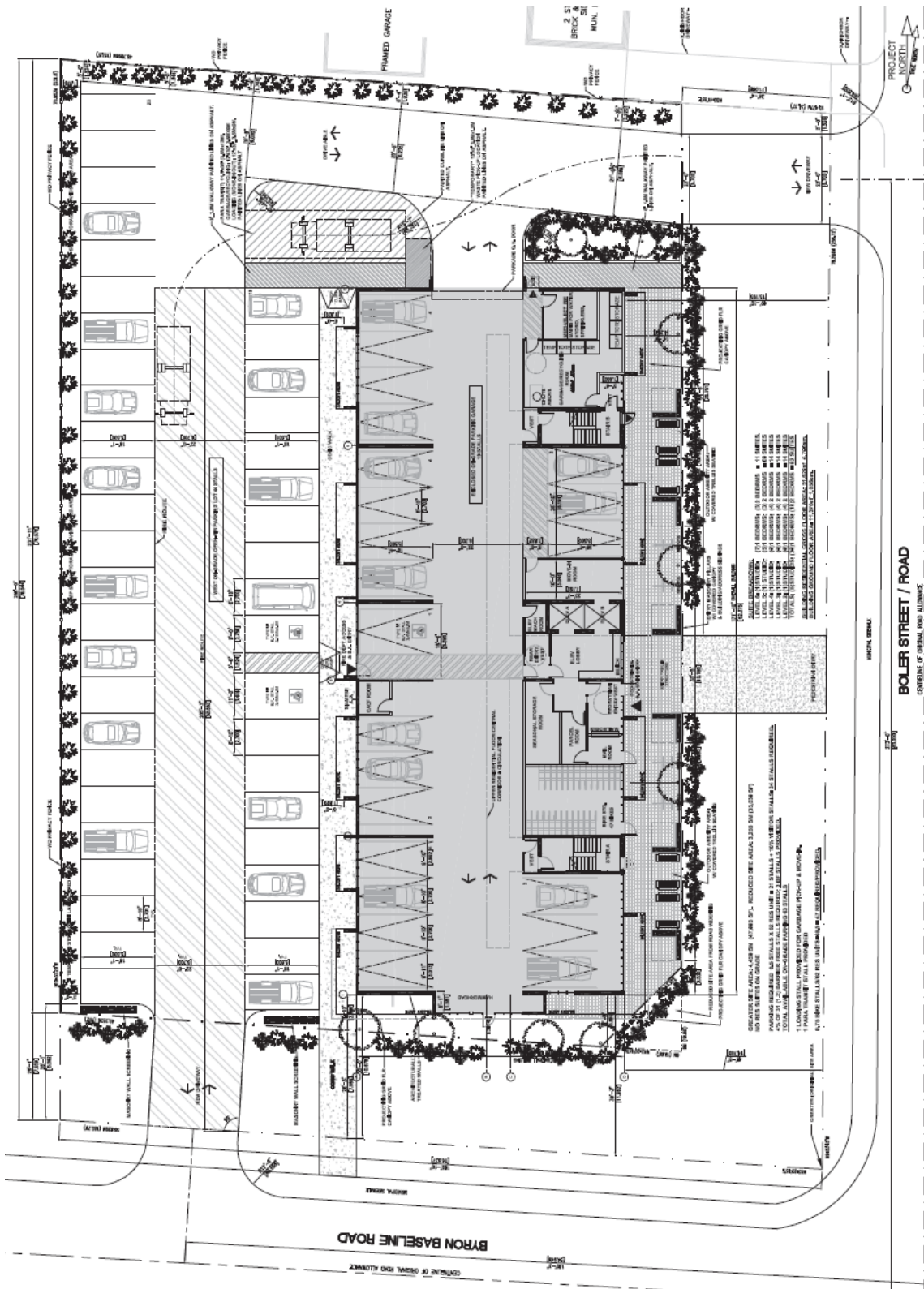


Figure 6. Conceptual Site Plan Layout  
Source: Patrick David Trottier Architect



**Figure 7.** Perspective rendering looking northwest  
Source: Patrick David Trottier Architect



**Figure 8.** Perspective rendering looking southeast  
Source: Patrick David Trottier Architect



## **7 BUILDING DESIGN CONSIDERATIONS**

The proposed building would be designed to an aesthetic built form that enhances the public realm and particularly addressing the intersection of Byron Baseline Road and Boler Road. The proposed building renderings are portrayed in **Figure 7 to Figure 10**.

As a high traffic area, the ground floor corner would be provided with glazing and building entrances. Horizontal and vertical elements would be incorporated to differentiate the ground floor and to break up the massing of the building. Pergolas and canopies would also be incorporated on the ground floor to activate the street edge at each building entrances and used to screen parking areas from the public realm. On the north side of the building, a stepback above the 4<sup>th</sup> floor provides an appropriate transition to the neighbouring single detached lots.

The use of vertical and horizontal planes are explored through the placement of balconies, mullions and variation of high quality materials successfully combine and create a dynamic façade. Large balconies provide private amenity spaces and build on the notion of the outdoor realm that the Byron community embodies. The balconies help to dissect the building mass into smaller residential sections.

The mullion fins are orientated to add privacy between balconies and create a sense of verticality. The varying heights of materials further add to this verticality. These elements further enhance the contemporary style, making the changes in materials and architectural fenestration details more apparent.

The corner of the building nearest the intersection would maximize glazing and utilize varying materials to create a visually interesting pedestrian scaled façade and focal point. The building's main entrance is located from Boler Road and accessed via public sidewalk.

The proposed building renderings (**Figure 7 to Figure 10****Figure 9**) illustrate a variety of materials, textures, and articulation that would be used to highlight architectural elements and create interest along the façades. The proposed building would use a selection of high-quality building materials, including but not limited to brick, natural stone, glazing, glass railings, and metal materials to accentuate building windows, entrances and canopies.

Key Development features:

- Activated ground floor, trellis/pergola screening and street-oriented, human-scale facade
- Pedestrian connections from Byron Baseline Road and Boler Road
- Rooftop Amenity above fourth floor and large private balconies
- Internal parking and secure bicycle parking on the ground floor



**Figure 9.** Rendering of South/East Elevations – View from Boler Road and Byron Baseline Road Intersection  
Source: Patrick David Trottier Architect



**Figure 10.** Rendering of South/West Elevations – View from Byron Baseline Road  
Source: Patrick David Trottier Architect

## **8 SUPPORTING STUDIES**

### **8.1 SANITARY SERVICING FEASIBILITY STUDY**

A Sanitary Servicing Feasibility Study (SFS) has been completed by SBM, dated June 2022, and submitted under separate cover. The study was completed to determine the adequacy of the existing sanitary sewer on Boler Road between the Subject Site to the sanitary sewer downstream of the Boler Road and Commissioners Road West intersection in support of the OPA/ZBA applications and subsequent site plan development. The following summarizes the analysis, recommendations, and conclusions of the study.

The study anticipates that the site's occupancy load of 144 people would generate a peak sanitary flow for the proposed development of 1.8 L/s when combined with infiltration. A new sanitary sewer PDC would be needed to connect to the existing 375 mm diameter sanitary sewer in the Boler Road ROW. Generally, the existing sanitary sewers downstream of the proposed development all have adequate capacity to convey the increase in flows from the proposed development.

A more detailed design and servicing evaluation would be conducted through the Site Plan Approval design phase.

### **8.2 ARCHAEOLOGICAL STUDY**

An Archaeological Assessment (Phase 1 and 2) study has been completed by Lincoln Environmental Consulting and is submitted under separate cover.

The Stage 1 assessment identified that the Subject Site had high archaeological potential and a Stage 2 Assessment was recommended. The Stage 2 field assessment was conducted, and no archaeological resources were identified.

### **8.3 TRAFFIC IMPACT ASSESSMENT**

A Transportation Impact Assessment (TIA) was conducted by Paradigm Transportation Solutions Ltd, dated December 2024, to evaluate the existing and future traffic forecasts through to 2032. The intersections evaluated within the study include:

- Boler Road and Byron Baseline Road (signalized);
- Boler Road and Commissioners Road West (signalized); and
- Access intersections on Boler Road and on Byron Baseline Road.

The findings of the existing traffic conditions and intersection operations at Boler Road and Byron Baseline Road included queueing that exceed the 95<sup>th</sup> percentile in several directions during the AM and PM peak hours.

The proposed development is forecast to generate 16 and 25 trips during the AM and PM peak hours, respectively.

The development is not a significant trip generator and will add less than 1.0% of the future road traffic volumes on Boler Road and less than 1.5% of the future road traffic volumes on Byron Baseline Road. Therefore, the nearby intersections are forecast to operate with similar levels of service as under existing traffic conditions. The proposed development is forecast to have negligible impacts on the surrounding road network as the site generated trips are forecast to impact the intersection operations by less than two seconds of delay.

Vehicular access to the site will replace the three existing driveways on Boler Road with a single full-moves access located approximately 83 metres (centreline to centreline) from Byron Baseline Road. A second full-moves access is proposed on Byron Baseline Road approximately 55 metres (centreline to centreline) from Boler Road with clear sightline clearance. Both accesses would be located furthest from the intersection as possible. Based on the forecasted intersection operations and queueing analyses, the impact to the intersection of Boler Road and Byron Baseline Road and the queueing into the site are considered minimal.

Further, the TIA does not recommend left-turn lanes to be added to Byron Baseline Road or on Boler Road under the forecasted traffic conditions as the proposed development is a low trip generator with minimal trips assigned to both driveways. In addition, the site accesses are forecast to operate with queues no greater than one vehicle for outbound traffic.

The assessed 5-year traffic forecast and proposed site accesses would be sufficient to accommodate the needs of the development. Recommendations have been provided with consideration of the existing background traffic, including potential intersection improvements and signal timing with left-turn advanced phasing.

## **9 PLANNING POLICY FRAMEWORK**

### **9.1 PROVINCIAL PLANNING STATEMENT (PPS), 2024**

The Provincial Planning Statement (PPS), issued under the authority of Section 3 of the Planning Act “*provides policy direction on matters of provincial interest related to land use planning*” in order to ensure efficient, cost-efficient development and the protection of resources. The proposed development is consistent with the PPS and more specifically supports the following policies:

#### *2.1 Planning for People and Homes*

##### *6. Planning authorities should support the achievement of complete communities by:*

*a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including schools and associated child care facilities, long term care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;*

**Response:** The proposed development contributes to the housing supply by creating 63 new residential apartment units that are more attainable in comparison to the single detached residential lots surrounding the subject site and predominantly located within the highly sought after Byron community. The proposed development is compact, cost-effective, and minimizes the consumption of land by utilizing existing capacity within existing municipal services and infrastructure. The proposed development promotes cost-effective development by utilizing existing infrastructure and being proximal to amenities, such as parks, shopping, schools, and transit facilities.

## *2.2. Housing*

*1. Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:*

*b) permitting and facilitating:*

*1. all housing options required to meet the social, health, economic and wellbeing requirements of current and future residents, including additional needs housing and needs arising from demographic changes and employment opportunities; and*

*2. all types of residential intensification, including the development and redevelopment of underutilized commercial and institutional sites (e.g., shopping malls and plazas) for residential use, development and introduction of new housing options within previously developed areas, and redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3;*

*c) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation;*

*d) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.*

**Response:** The Subject Site is within a Settlement Area, Built Area Boundary, and Urban Growth boundary in the city of London. The proposed redevelopment of the subject site would create one mid-rise (6-storey) residential building, introducing apartment housing in a compact form of development. The proposed development makes efficient use of land and municipal services, and is located in proximity to shopping/services, parks and amenities promoting walkability, and within bus transit facilities.

### *2.3.1 General Policies for Settlement Areas*

*1. Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas*

*2. Land use patterns within settlement areas should be based on densities and a mix of land uses which:*

*a) efficiently use land and resources;*  
*b) optimize existing and planned infrastructure and public service facilities;*  
*c) support active transportation;*  
*d) are transit-supportive, as appropriate;*

*3. Planning authorities shall support general intensification and redevelopment to support the achievement of complete communities, including by planning for a range and mix of housing options and prioritizing planning and investment in the necessary infrastructure and public service facilities.*

**Response:** The Subject Site is located in the Built Area Boundary of the city of London and within the Settlement Area (Urban Growth Boundary). The proposed development would represent an intensification and redevelopment of land by removing three (3) single detached lots and creating 62 apartment units with sufficient servicing capacity and direct access to bus transit facilities, commercial/retail uses, schools/parks, and other amenities that support a complete community.

### *3.6 Sewage, Water and Stormwater*

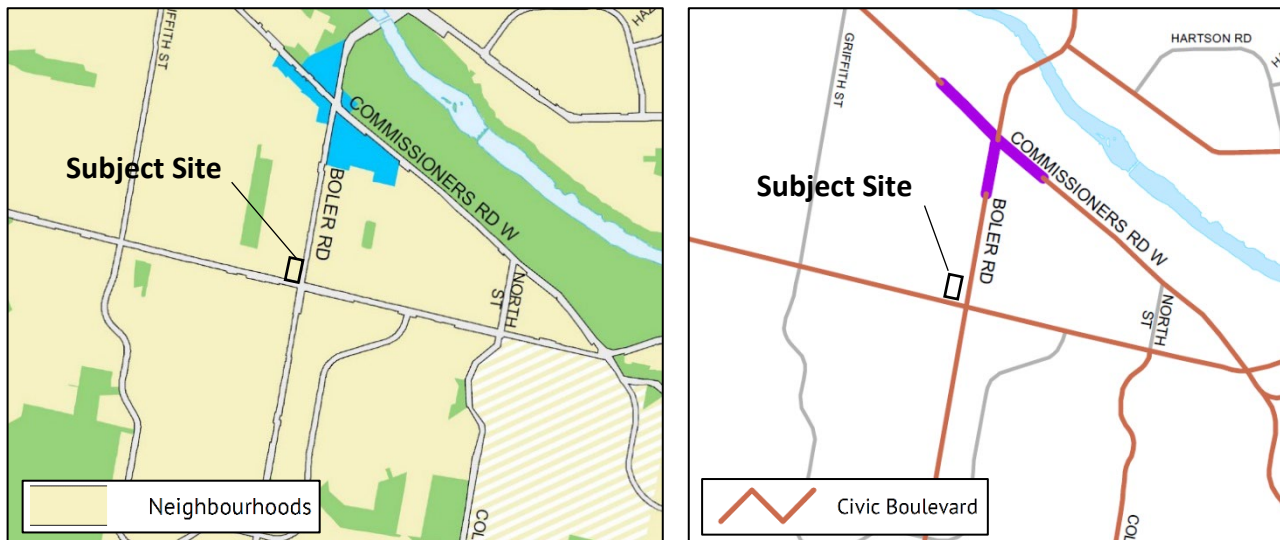
*2. Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized servicing systems and decentralized servicing systems.*

**Response:** The use of existing municipal infrastructure (i.e., sewage, water, and stormwater) is being optimized through the proposed development along Boler Road and Byron Baseline Road corridors.

Based on the above, that the proposed Zoning By-Law Amendment would be consistent with the policies of the 2024 Provincial Planning Statement.

## **9.2 THE LONDON PLAN**

The London Plan designates the Subject Site within the “Neighbourhoods” Place Type, per Map 1, and fronting onto two “Civic Boulevards”, per Map 3 (**Figure 11**). Tables 10 and 11 of the London Plan outline the range of permitted uses and intensities for sites located within the “Neighbourhoods” Place Type and fronting onto “Civic Boulevards”, which includes “Low Rise Apartments” up to 4-storeys, or an upper limit of 6-storeys through a site specific zoning bylaw.



**Figure 11.** The London Plan: Map 1 – Place Types (Left); Map 3 – Street Classifications (Right)

### Neighbourhoods Place Type - Vision

The overall objectives for new residential development designated under the Neighbourhoods Place Type are outlined in the following objectives listed of Policy 916:

1. *A strong neighbourhood character, sense of place and identity.*
2. *Attractive streetscapes, buildings, and public spaces.*
3. *A diversity of housing choices allowing for affordability and giving people the opportunity to remain in their neighbourhoods as they age if they choose to do so.*
4. *Well-connected neighbourhoods, from place to place within the neighbourhood and to other locations in the city such as the downtown.*
5. *Lots of safe, comfortable, convenient, and attractive alternatives for mobility.*
6. *Easy access to daily goods and services within walking distance.*
7. *Employment opportunities close to where we live.*
8. *Parks, pathways, and recreational opportunities that strengthen community identity and serve as connectors and gathering places.*

**Response:** The proposed development would satisfy the above key elements and objectives, as follows:

- The Subject Site is located in a mature community that has an established sense of identity and character. The proposed development is well-designed and located at a prominent intersection in the community and would establish noticeable “focal point”.

- The proposed building is located at the street edge and would re-animate the street edge with enhanced landscaping, boulevards improvements, windows/entrances oriented to the street.
- The proposed development provides for a housing type (apartments) that is not widely available in the immediate community, providing a wider range of housing choice in this neighbourhood for all household types.
- The Subject Site is located at the intersection of two arterials, with bus transit services and in a location that is proximal to commercial shopping amenities.
- The existing neighbourhood is walkable, with access to open space, schools, and commercial shopping amenities within 10 minute walk from the Subject Site.

### Intensification

*79\_ The London Plan places an emphasis on growing “inward and upward” to achieve a compact form of development. This should not be interpreted to mean that greenfield forms of development will not be permitted, but rather there will be a greater emphasis on encouraging and supporting growth within the existing built-up area of the city.*

*80\_ Residential intensification will play a large role in achieving our goals for growing “inward and upward”. Intensification will be supported, subject to the policies of this Plan, in the following forms:*

*4. Infill development of vacant and underutilized lots.*

*84\_ Intensification may occur in all of the Place Types that allow for residential uses.*

*937\_ Residential intensification is fundamentally important to achieve the vision and key directions of The London Plan. Intensification within existing neighbourhoods will be encouraged to help realize our vision for aging in place, diversity of built form, affordability, vibrancy, and the effective use of land in neighbourhoods. Such intensification should add value to neighbourhoods by adding to their planned and existing character, quality and sustainability....*

**Response:** Regarding intensification, The London Plan encourages “inward and upward” growth to achieve compact built form within existing built-up areas. The Subject Site is designated within the Neighbourhood Place Type, proposes a compact built form, and located in an existing built-up area in the city of London. The proposed residential intensification would fulfil these objectives and policies by transforming three single detached dwellings into a mid-rise apartment building that is situated to reinforce the residential community, and offer an alternative built form/tenure and appropriate height transition/setback/stepbacks to accommodate solar access and privacy.



## City Design

The London Plan includes a City Design chapter that outlines urban design policies that are applied to all new developments in the Neighbourhoods Place Type (Policy 936). The overarching City Design objectives are listed in Policy 193:

*193\_ In all of the planning and development we do and the initiatives we take as a municipality, we will design for and foster:*

- 1. A well-designed built form throughout the city.*
- 2. Development that is designed to be a good fit and compatible within its context.*
- 3. A high-quality, distinctive and memorable city image.*
- 4. Development that supports a positive pedestrian environment.*
- 5. A built form that is supportive of all types of active mobility and universal accessibility.*
- 6. High-quality public spaces that are safe, accessible, attractive and vibrant.*
- 7. A mix of housing types to support ageing in place and affordability.*
- 8. Sustainably designed development that is resilient to long-term change.*
- 9. Healthy, diverse and vibrant neighbourhoods that promote a sense of place and character.*

**Response:** The components of this proposal have been designed to achieve the applicable listed objectives in the following manner. The proposed development would be:

- architecturally designed to feature a high-quality building using high-quality materials, lighting and colour, and designed to minimize massing and promote a human-scale development that is reflective of the surrounding commercial, institutional and residential development.
- a maximum of 6-storeys to not overwhelm the pedestrian environment, with a significant stepback from the north property limit to achieve appropriate transition in building height.
- compatible with existing residential land uses by being located furthest from shared property boundaries to reduce overlook or shadow onto the adjacent properties.
- distinctive along the street edge, contributing to the creation of a consistent streetwall and activating the public realm.
- supportive of walkable and alternative transportation modes by linking the site via walkways to the sidewalk network and providing bicycle facilities.
- designed in accordance with Ontario Building Code and Zoning Bylaw regulations that ensure the capability for universal accessibility.
- contributing to the supply of attainable housing types within an establishing community that offers housing choice, affordability, and opportunities to “age in place”.

## Character

*197\_ The built form will be designed to have a sense of place and character consistent with the planned vision of the place type, by using such things as topography, street patterns, lotting patterns, streetscapes, public spaces, landscapes, site layout, buildings, materials and cultural heritage.*

*202\_ Buildings and public spaces at key entry points into neighbourhoods will be designed to help establish a neighbourhood's character and identity*

*252\_ The site layout of new development should be designed to respond to its context and the existing and planned character of the surrounding area.*

*253\_ Site layout should be designed to minimize and mitigate impacts on adjacent properties.*

**Response:** The Subject Site is located at the intersection of two arterials (“Civic Boulevards”) at the heart of the Byron community. The London Plan Neighbourhoods Place Type allows for increased density and built form at these intersections and ‘gateway’ features. At the northeast, southeast and southwest corners of this intersection are large lot developments including a community church, office building and community shopping center. The proposed development is compatible with the existing context of the intersection, that is characterized by larger lots and large format buildings. However, the proposed development would be of enhanced architectural design, locating the building forward to the street, and with vehicular parking to the rear of the building, thereby providing a more visually attractive corner node for this prominent intersection.

## Streetscapes

*222A\_ The proportion of building and street frontages used for garages and driveways should be minimized to allow for street trees, provide for on-street parking and support pedestrian and cycling-oriented streetscapes.*

*236\_ All streets, and the associated infrastructure, should be designed to include space for appropriately sized street trees with tree canopy coverage that will provide for pleasant pedestrian enhanced aesthetics, afford cooling to adjacent buildings, improve air quality, and offer habitat for urban wildlife.*

*235\_ Landscaping should be used to define spaces, highlight prominent features and landmarks, add visual interest, define pedestrian areas, delineate public and private spaces, add comfort and improve health, offer visual screening, and improve the aesthetic quality of neighbourhoods.*

**Response:** The three existing driveways on Boler Road would be removed and reconfigured to allow for two new entrances, located furthest from the intersection at the edges of the property limits to allow for safe intersection operations, reduce pedestrian/vehicle conflicts, and improve the streetscape. The street edges would include walkways and landscaped frontage leading to principal building entrances. The proposed development would also

be subject to significant road dedication, allowing for potential expansion of the paved portion of the road, but also wider sidewalks, street trees and landscaping, seating, and/or improved transit facilities.

### Site Layout

*253\_ Site layout should be designed to minimize and mitigate impacts on adjacent properties.*

*255\_ Site layout will promote connectivity and safe movement for pedestrians, cyclists, and motorists between, and within, sites.*

*256\_ Buildings should be sited so that they maintain and reinforce the prevailing street wall or street line of existing buildings. Where a streetscape has not been built out, buildings should be sited with regard for the planned street wall or street line.*

*259\_ Buildings should be sited with minimal setbacks from public streets and public spaces to create an inviting, active and comfortable pedestrian environment.*

*268\_ Sites shall be designed to provide a direct, comfortable and safe connection from the principle building entrance to the public sidewalk.*

*269\_ Buildings should be sited to minimize the visual exposure of parking areas to the street.*

**Response:** The proposed building would be brought forward to the street, as much as possible, reducing the “front” and “external side” yard setbacks to activate the street edge and maximize building separation from the surrounding lots. Walkways would be provided on-site to connect the public street to building entrances and parking.

### Parking

*270\_ The location, configuration, and size of parking areas will be designed to support the planned vision of the place type and enhance the experience of pedestrians, transit-users, cyclists, and drivers.*

*271\_ The Zoning By-law will establish automobile parking standards, ensuring that excessive amounts of parking are not required. Requirements may be lower within those place types and parts of the city that have high accessibility to transit or that are close to employment areas, office areas, institutions and other uses that generate high levels of attraction.*

*272\_ The impact of parking facilities on the public realm will be minimized by strategically locating and screening these parking areas. Surface parking should be located in the rear yard or interior side yard.*

*273\_ Parking structures should be integrated into the design of buildings to ensure the public realm is not negatively affected. Structured parking will be screened.*

*280\_ Adequate bicycle parking facilities will be required for all development to encourage cycling as a viable form of transportation. Covered and secure forms of bicycle parking should be incorporated into multi-unit residential, large-scale commercial or retail, institutional, and recreational developments.*

**Response:** On-site parking facilities are proposed behind the building and within the ground level, providing 63 parking spaces. In support of transit and alternative modes of transport, the site is located directly across the street from bus transit facilities and on-street bicycle lane (Byron Base Line ROW), would provide ample bicycle storage internal to the building, and is connected via sidewalk network to several parks, trails and amenities to serve the residents.

### Buildings

*285\_ To support pedestrian activity and safety, large expanses of blank wall will not be permitted along the street edge.*

*286\_ Buildings should be designed to achieve human-scale relationships that are comfortable for pedestrians.*

*287\_ Within the context of the relevant place type policies, the height of buildings should have a proportional relationship to the width of the abutting public right-of-way to achieve a sense of enclosure.*

*290\_ Buildings located on corner sites should address the corner through building massing, location of entrances, and architectural elements.*

*291\_ Principal building entrances and transparent windows should be located to face the public right-of-way and public spaces, to reinforce the public realm, establish an active frontage and provide for convenient pedestrian access.*

*298\_ Design measures relating to building height, scale and massing should be used to provide a transition between development of significantly different intensities, considering the existing and planned context.*

**Response:** The proposed building would enhance the street edge as well as the pedestrian experience along the two civic boulevards. This would be accomplished by using architectural features to break-up the massing of the building, provide transitional building height, and using a combination of materials and lighting to direct sightlines and orient pedestrians to the southeast edge facing the intersection.

In relation to the abutting streets, both Byron Baseline Road and Boler Road are classified as Civic Boulevards. A total right-of-way width of 36m from property line to property line, is planned for this area, which may include bicycle facilities, sidewalks, boulevards trees and street furniture among other infrastructure matters. The proposed building would be less than the roadway width planned for this intersection, representing less than a 45 degree plane from property line and representing a human-scale form of development with activated ground floor.

In terms of height, scale and massing, the proposed development is consistent with the intended built form envisioned within The London Plan. This is particularly true for a development located at the intersection of two civic boulevards opposite community shopping amenities, employment uses. The adjacent lands are presently single detached homes; however under The London Plan, these lands are also designated Neighbourhoods and permitted for more intensive forms of development – consistent with the Subject Site –that could be rezoned and redeveloped in the future.

The intent of the “Neighbourhoods” place type is to permit a range of residential land uses and intensities that are compatible in form and offer a variety of housing choice, particularly with higher intensity along higher-order streets. The intent and approach of this place type is:

*“to balance neighbourhood stability and predictability with the goals of creating neighbourhoods that allow for different housing types, an appropriate mix of uses, affordability, aging in place, and vibrant, interesting communities” (Policy 919.6)*

The London Plan lists the permitted uses for sites within the “Neighbourhoods Place Type” with frontage onto two “Civic Boulevards”, to include Low Rise Apartments (Table 10) up to 6 storeys in height (Table 11).

### Materials

*301\_ A diversity of materials should be used in the design of buildings to visually break up massing, reduce visual bulk and add interest to the building design.*

*302\_ Materials should be selected for their scale, texture, quality, durability, and consistency within their context.*

**Response:** Emphasis has been placed on building design using variations in materials (e.g., brick, steel, etc.), building articulation, glazing and canopies/pergolas. An architectural package has been submitted with the application (under separate cover) to provide conceptual illustrations and material selections that are envisioned for this project.

### Housing

*506\_ Subject to the City Structure Plan and Residential Intensification policies in the Neighbourhoods Place Type chapter of this Plan, infill and intensification in a variety of forms, including secondary dwelling units, will be supported to increase the supply of housing in areas where infrastructure, transit, and other public services are available and accessible.*

*508\_ Abundant opportunities will be provided for the development of a broad range and mix of housing types and densities in conformity with the City Structure Plan and the Urban Place Type policies of this Plan.*

**Response:** The proposed development represents an infill and intensification opportunity that would increase the housing supply in a desirable neighbourhood, that is supported by existing municipal infrastructure, transit availability, and access to services, amenities, parks and schools.

The proposed development would provide an additional housing type in a predominantly low-rise, single-detached neighbourhood. The proposed six-storey apartment building will introduce a compact residential land use permitted under the “Neighbourhoods” Place Type, where intensification is appropriate.

Consistent with the intent of The London Plan, the proposed development and associated Zoning By-law Amendment seeks to permit an appropriate built form with a height of 6 storeys with site specific provisions to ensure the built form would not be unsuitable and consistent with good architecture and urban design principles.

#### Neighbourhoods Place Type

*918\_ We will realize our vision for the Neighbourhoods Place Type by implementing the following in all the planning we do and the public works we undertake:*

- 1. Through the review of all planning and development applications, neighbourhoods will be designed to create and enhance a strong neighbourhood character, sense of place and identity.*
- 2. Neighbourhoods will be planned for diversity and mix and should avoid the broad segregation of different housing types, intensities, and forms.*
- 3. Affordable housing will be planned for, and integrated into, all neighbourhoods.*
- 13. Intensification will respect existing neighbourhood character and offer a level of certainty, while providing for strategic ways to accommodate development to improve our environment, support local businesses, enhance our physical and social health, and create dynamic, lively, and engaging places to live.*

**Response:** The proposed development would contribute to the housing stock, by creating 62 apartment dwelling units that integrate a more attainable housing form of development, enhances and animates the public realm, and increases foot traffic and viability of commercial business in close proximity.

#### Residential Intensification

*940\_ It is an important strategy of this Plan to support all of these forms of intensification, while ensuring that they are appropriately located and fit well within their neighbourhood.*

**Response:** The proposed building constitutes a form of redevelopment and infill development of three single detached lots. The proposed development is suitable for the location, being the intersection of two Civic

Boulevards, within 100m of community shopping amenities and transit services, and within 200m from community facilities and public parks.

The proposed development is designed to fit within the existing neighbourhood and contemporary in appearance. While the proposed building height is greater than the existing buildings north and west of the site, specific architectural strategies and treatment would promote compatibility and “fit” within the existing streetscape, effectively reducing the perceived height and massing of the building. A landscaping scheme would be used to minimize impacts and improve privacy for adjacent properties, such as using vertical elements for year-round screening. The proposed building would generally “fit” within the neighbourhood achieving a compatible intensification (further described in the following review of Policy 953).

#### Urban Design Considerations for Residential Intensification

*953\_ The City Design policies of this Plan will apply to all intensification proposals. In addition, the following design policies will apply:*

*2. Compatibility and fit, from a form perspective, will be evaluated based on such matters as:*

*a. Site layout within the context of the surrounding neighbourhood, considering such things as access points, driveways, landscaping, amenity areas, building location, and parking.*

*b. Building and main entrance orientation.*

*c. Building line and setback from the street.*

*d. Character and features of the neighbourhood.*

*e. Height transitions with adjacent development.*

*f. Massing appropriate to the scale of the surrounding neighbourhood.*

*3. The intensity of the proposed development will be appropriate for the size of the lot such that it can accommodate such things as driveways, adequate parking in appropriate locations, landscaped open space, outdoor residential amenity area, adequate buffering and setbacks, and garbage storage areas.*

**Response:** The proposed site layout and building have been carefully designed and would achieve a compatible intensification project by incorporating the following considerations:

- Building: the proposed would front onto two civic boulevards, locating parking to the rear of the site and within the ground level of the building. Landscaping and screening would be used to improve screening of parking areas from the public realm.

- Building and main entrance orientation: The building’s principal entrance would be oriented to Boler Road with secondary access to Byron Baseline Road, with walkway access from the sidewalk network.
- Building line and setback from the street: Given the extensive road dedication required along both fronting streets, the proposed building would be setback and be aligned with existing building frontages of Boler Road and Byron Baseline Road, reaffirming the street wall along both corridors.
- Character and features of the neighbourhood: the Subject Lands are located at a prominent intersection that is characterized by the four corners that establish a “gateway” into the Boler community. Each of the four corners are characterized with surface parking in front of larger-format buildings (church, office, and shopping centre). In this context, the proposed building would be an improvement to the streetscape and would help frame the intersection by emphasizing the gateway location, providing an enclosure of the public realm and placing parking to the rear of the building.
- Height and Massing: The proposed development is consistent with the proposed R8-4 provisions for lot coverage and landscape area percentages. The proposed building, from a scale perspective, is consistent with the envisioned built form of the London Plan, which permits 4-6 storeys in height, and has been architecturally designed with elements, building stepback and articulation to reduce the perceived massing and scale from abutting lots and the public realm.
- Parking/Access: Two access into the site - one from each road frontage – are proposed to improve access to/from the site in proximity to the intersection. Parking has been located at the rear of the site with through access allowing larger vehicles to manoeuvre through the site and options for ingress/egress.
- Maximum Building Height

- *1638\_ The maximum height in the applicable Place Type may include a standard maximum and upper maximum height. Zoning on individual sites may be permitted up to the standard maximum height. Applications to exceed the standard maximum height will be reviewed on a site-specific basis and will not require an amendment to this Plan. Heights exceeding the upper maximum will require an amendment to this Plan.*
- *1640\_ In order to provide certainty and to ensure that the features required to mitigate the impacts of the additional height and densities are provided, a site-specific zoning by-law amendment will be required to exceed the standard maximum height. Through the amendment process the community, City Council and other stakeholders can be assured that measures will be implemented to mitigate any impacts of additional height or density.*



- *1641\_ Increases in building height above the Standard Maximum may be permitted where the resulting intensity and form of the proposed development represents good planning within its context.*

**Response:** As described in the above listed responses to The London Plan policies for Urban Design and development under the “Neighbourhoods” Place Type and Residential Intensification sections, it has been evidenced that the proposed development would take great strides to fit within the community, establish a sense of place and focal point, and enhance the streetscape with its presence. The proposed development would consolidate three lots to accommodate the larger development and would meet the established parameters for additional height and density.

Based on the above, the proposed development and Zoning By-Law Amendment conforms to the policies and general intent of The London Plan and the applicable “Neighbourhoods” Place Type designation.

## **10 PROPOSED ZONING BYLAW AMENDMENT**

The site is currently zoned Residential (R1-8) Zone variation, which only permit single detached dwellings; therefore a Zoning By-law Amendment is being requested to create a site specific zone for the subject site to permit the proposed apartment building use under the Residential (R8-4) Zone variation. **Table 1** provides a comparative analysis of the regulations of the proposed R8-4 Zone as it relates to the proposed development per **Table 1. Zoning Data Table.**

**Table 1.** Zoning Data Table Comparison

Zoning Regulations	R8-4 Zone	Provided	Conforms?
Lot Area (m <sup>2</sup> ) Minimum	1,000	4,464	✓
Lot Frontage (m) Minimum	30	46.2	✓
Front Yard Depth (m) Minimum (South)	8 (6m + 1m per 10m of building height above the first 3m)	1.5*	✗
Exterior Yard Depth (m) Minimum (East)	8 (6m + 1m per 10m of building height above the first 3m)	1.5*	✗
Internal Yard Depth (m) Minimum (West)	8 (1.2m per 3m of building height above the first 3m)	22.0	✓
Rear Side Yard Depth (m) Minimum (North)	8 (1.2m per 3m of building height above the first 3m)	13.9	✓

Zoning Regulations	R8-4 Zone	Provided	Conforms?
Landscaped Open Space (%) Minimum	30	40	✓
Lot Coverage (%) Maximum	40	24	✓
Height (m) Minimum	13	20*	✗
Density (UPH) Maximum	75	140*	✗
Vehicle Parking	31 (0.5/ du)	63 (1.0/ du)	✓
Parking Setback (SPC Bylaw)			
- To Street Line	3.0	0.5*	✗
- To Property Line	1.5	1.5	✓
Bicycle Storage			
- Long Term	56 (0.9 / du)	46 (0.73 / du)	✗
- Short Term	7 (0.1 / du)	32 (0.5 / du)	✓
Encroachment (m)			
- Front and External Side Yard	3	0.0*	✗

\* Special Provision required.

## 10.1 REQUESTED SPECIAL PROVISIONS

Based on the above zoning data comparison, the regulations of the R8-4 zone require special provisions to permit the proposed development, including additional permitted commercial uses, as follows:

### R8-4(\*\*) 415 Boler Road

#### a) Regulations

- |       |  |                       |
|-------|--|-----------------------|
| i.    | Front Yard Setback (Minimum)                   | 1.5 metres            |
| ii.   | Exterior Yard Depth (Minimum)                  | 1.5 metres            |
| iii.  | Building Stepback from the North Property Line | 8 metres              |
| iv.   | (Minimum)                                      |                       |
| v.    | Height (Maximum)                               | 20 metres             |
| vi.   | Density (Maximum)                              | 140 units per hectare |
| vii.  | Bicycle Parking – Long Term (Minimum)          | 46 spaces             |
| viii. | Parking Setback from Street Line               | 0.5 metres            |
| ix.   | Encroachments for Canopies, Balconies, and     | 0 metres              |

- Architectural Features within Front and External Lot Line
- x. Accessory structures such as arbors/ pergolas, seating, and screening shall be permitted within the required Front and External Side Yards.

### 10.1.1 Rationale for Special Provisions:

**Front and Exterior Yard Setbacks:** The application intends to permit reduced front and exterior yard setbacks along Boler Road and Byron Baseline Road. This is intended to provide buildings frontages along the public realm creating activity, interest and building a sense of place; to locate parking and other “back of house” activities to the interior of the site, screening them from view of the public realm; and to maximize the building setback from adjacent properties to the north and west.

The proposed building would be located in alignment with the existing buildings on both street frontages (Boler Road and Byron Baseline Road), essentially preserving and re-establishing the street wall along both frontages. The proposed reductions to the Front and External Yard setbacks are not anticipated to have any impact on the surrounding landowners or the public realm.

**Height and Density:** This application is seeking special provisions to increase the maximum height and density of the R8-4 Zone, to construct a 6-storey (20m) apartment building, permitted under the upper maximum building height limit of the “Neighbourhoods” Place Type of The London Plan. An increase to the maximum density is also requested from 75 Units per hectare (UPH) to 140 UPH.

The site location is appropriate for the proposed intensification based on its location at a prominent intersection and proximity to community shopping, institutional uses, parks, and transit facilities. These features provide the essential elements for a “complete community” to live, work, shop and play within a short commuting distance all of which aid in reduction of vehicle trips, increased foot traffic and viability for retail and transit facilities. These elements all promote the increased utilization of existing infrastructure.

Design considerations have been taken to place the building with smaller setbacks along the street frontages, leaving larger setback distances for the adjacent low rise buildings. This also helps to limit ‘overlook or shadowing’ on abutting lands low rise residential uses, if they will be at all present, from the mid-rise building during the summer months. Therefore, the proposed height and density being requested is appropriate.

**Vehicle and Bicycle Parking:** Under Section 6.2, Schedule 1 of the Site Plan Control Bylaw, off-street parking area are required to be setback a minimum of 3m from the street line. This setback is intended to allow for landscaping to screen vehicles from the abutting street. The development proposal would locate the parking area up to a minimum of 0.5 m; however additional screening would be provided in the form of trellis and pergolas designed to reflect the architectural elements and landscaping features of the ground floor of the building (**Figure 10**).

A request to reduce the minimum long-term bicycle parking ratio has been requested to be reduced from 0.9 per unit to 0.73 per unit, resulting in a reduction of 10 spaces from 56 spaces to 46 spaces. To offset the reduced long-term bicycle parking spaces and continue to encourage the alternative modes of transportation, 25 additional short-term bicycle parking spaces would be provided near the front entrance along Boler Road.

**Encroachments and Accessory Structures:** Under Section 4.27 of the Zoning Bylaw, encroachments of architectural features such as canopies and balconies are restricted. For apartment buildings, balconies may encroach 1.5 metres, provided the projection is no closer than 3.0 to the lot line. Additionally, Section 4 of the Zoning Bylaw prohibits accessory structures to be located within the Front or External Side Yards. As the building frontage is encouraged to animate the pedestrian realm, landscape features including canopies, pergolas and seating areas would be proposed between the building and the two street frontages. This exception would allow for a creative use of space and the activation of the streetscape, encouraging pedestrian use.

### 10.2 REQUESTED ZONING BYLAW MAP AMENDMENT

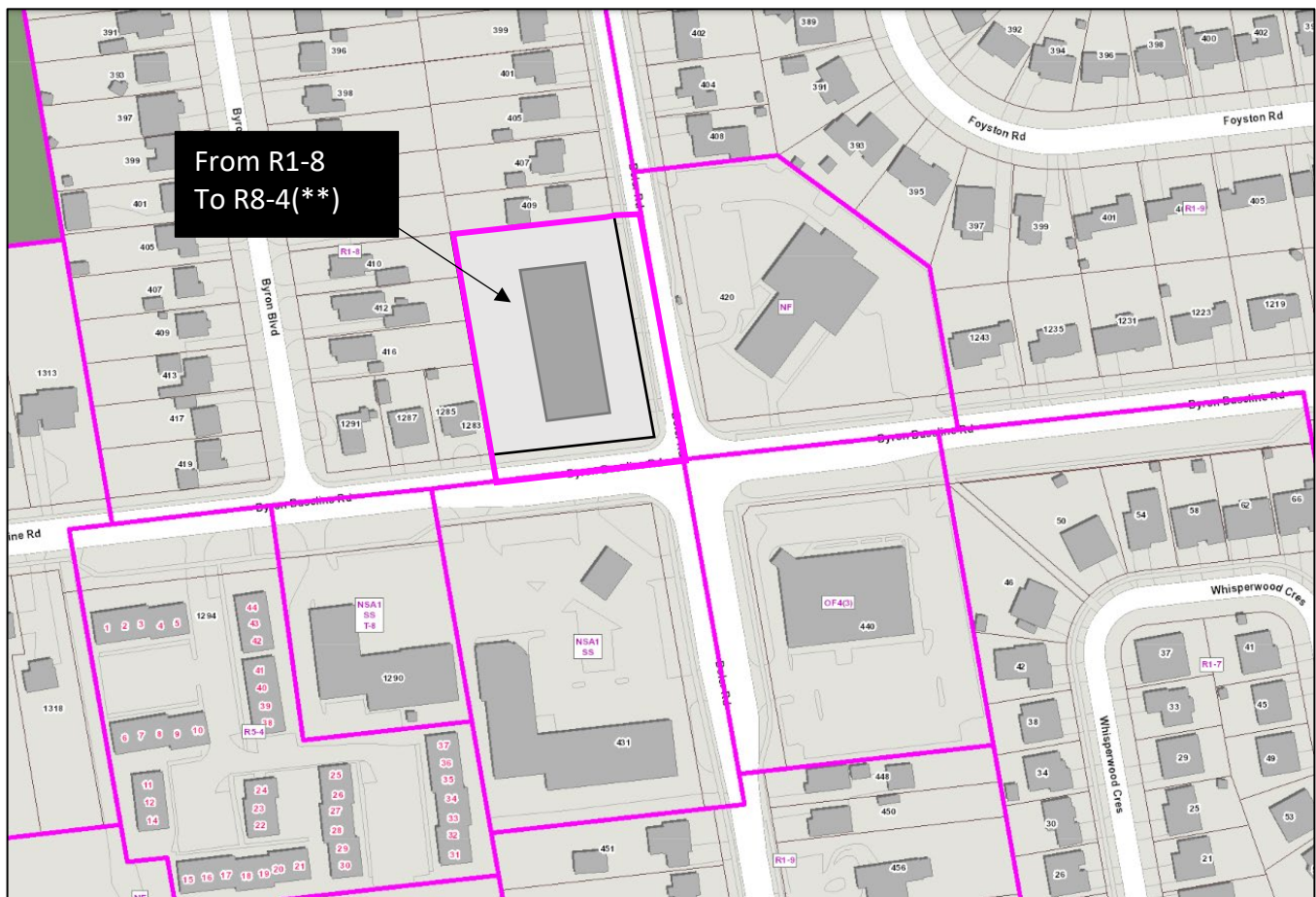


Figure 12. Proposed Map Change to Zoning Bylaw Amendment

## 11 **CLOSING**

The proposed Zoning By-Law Amendments is intended to permit a 6-storey residential apartment building with 62 units on the subject lands is a form of intensified development that represents good planning for the following reasons:

- The proposal is consistent with the policies of the Provincial Planning Statement, 2024, which promotes healthy, liveable and safe communities by accommodating an appropriate intensification of uses within the urban growth boundary on full municipal services;
- The proposed redevelopment conforms to the policies of The London Plan, including urban design guidelines and intensification policies for lands within the Neighbourhoods Place Type;
- The proposed Zoning By-Law Amendment application would permit an appropriate built form (apartments) that would improve housing choice and the supply of attainable units to meet housing demands within the Byron community;
- The proposed development is of a height, scale and density that will result in a use and built form that is representative of intensification that is appropriate along two civic boulevards planned for increased volumes of traffic and intensity;
- The proposal promotes cost-effective development patterns by providing a compact, efficient form of development that will utilize existing and planned services; and
- The proposed development has been designed to be sensitive to the adjacent low density residential uses by implementing suitable building setbacks/stepback.

For the reasons noted above and throughout this report, the proposed Zoning By-Law Amendment application represents sound land use planning practice.

### **Strik, Baldinelli, Moniz Ltd.**

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