



London
CANADA

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Chapter 20

Area-Specific Streetscape Standards

Design Specifications & Requirements Manual

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City of London

Design Specifications and Requirements Manual

The design information contained in this manual is intended to provide guidance beyond legislative and standard design practices for use in the City of London (the City). There will be site specific situations where the design will depart from these practices as it is not possible nor is it the intention of the City to anticipate every situation. The City intends to review and revise the Manual from time to time. The City also acknowledges that other references such as the **'Standard Contract Documents for Municipal Construction Projects'** are to be used in conjunction with this manual. The 2012 update of this manual incorporates design information from the City's former 'Subdivision & Development Guide Manual' to provide consistent and current design information for development projects.

The City of London maintains its right to accept or refuse any design submissions and requires an acceptable design for any given circumstance.

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20 Area-Specific Streetscape Standards

20.1 Downtown Streetscape Standards

20.1.1 Introduction

20.1.1.1 Purpose

The purpose of these standards is to provide direction to projects that involve modifications to the rights-of-way within the downtown area, with the goal of creating a consistent streetscape treatment that is unique to the downtown.

20.1.1.2 Location

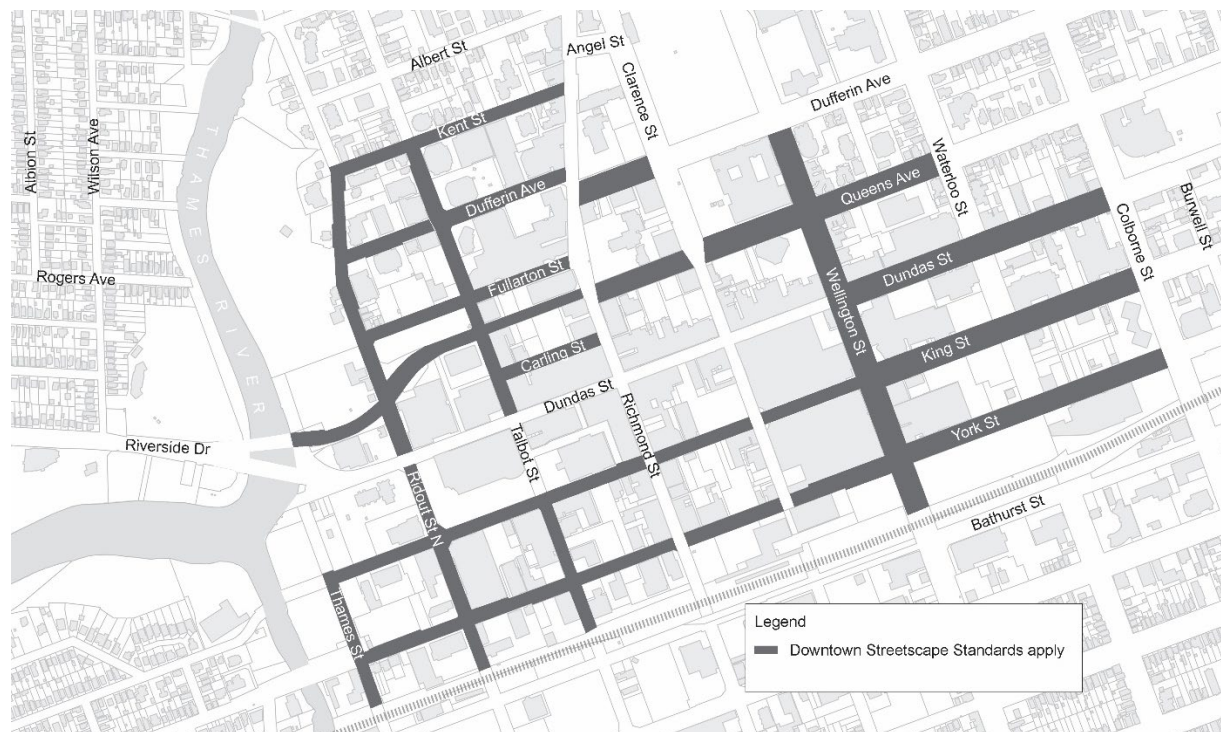
The Downtown Streetscape Standards apply to the following street segments, generally described as:

- York Street, Thames Street to Colborne Street
- Thames Street
- King Street, Thames Street to Colborne Street
- Dundas Street, Wellington Street to Colborne Street
- Carling Street
- Queens Avenue, Thames River to Waterloo Street
- Fullarton Street, Ridout Street North to Richmond Street
- Dufferin Avenue, Ridout Street North to Clarence Street
- Kent Street, Ridout Street North to Richmond Street
- Ridout Street North, train tracks to Kent Street
- Talbot Street, train tracks to King Street
- Talbot Street, Dundas Street to Kent Street
- Wellington Street, train tracks to Dufferin Avenue

These street segments are more precisely identified in **Figure 20.1**. Where minor discrepancies exist between the above description and **Figure 20.1**, **Figure 20.1** takes precedence.

It is important to note that there are street segments within the general downtown area that are excluded from the Downtown Streetscape Standards as other policies direct the streetscape design in these locations.

Figure 20.1: Downtown Streetscape Standard Location Map

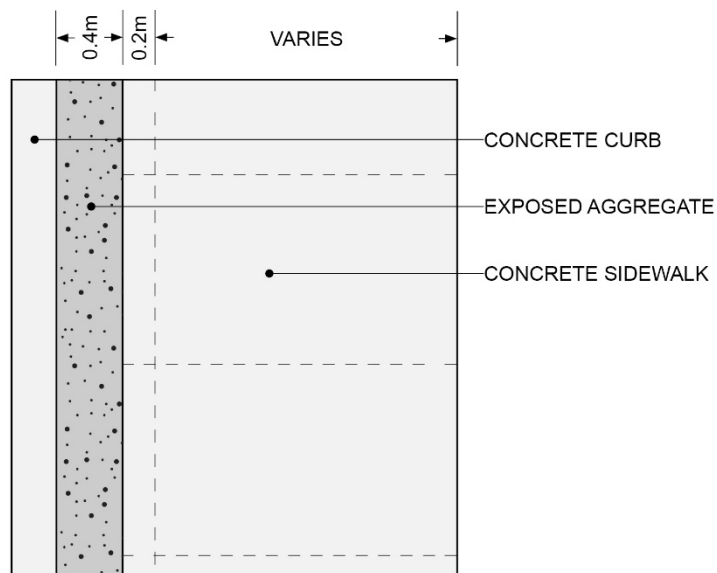


20.1.2 Surface Treatment

20.1.2.1 Standard Surface Treatment

The standard surface treatment for mid-block locations where there is not sufficient room to accommodate street trees within the sidewalk and boulevard area includes a 0.40 metre exposed aggregate band adjacent to the curb edge and a saw cut in the concrete sidewalk parallel to, and 0.20 metres from, the exposed aggregate band. This treatment is illustrated in **Figure 20.2**.

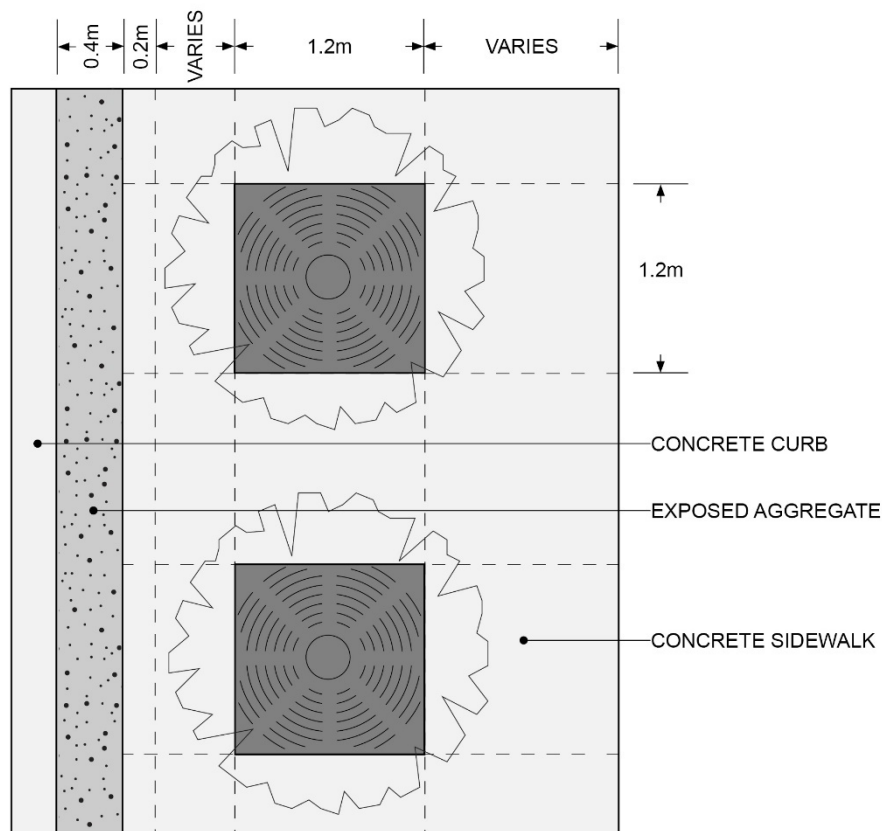
Figure 20.2: Downtown Standard Surface Treatment



20.1.2.2 Standard Surface Treatment with Street Trees

The standard surface treatment described in **Section 20.1.2.1** applies, with the addition of saw cuts in the concrete that correspond with the placement of the street tree grates. These additional saw cuts extend parallel with all four sides of the street tree grates. The location of street trees and tree grates within the right-of-way will be determined by the context. This standard treatment is illustrated in **Figure 20.3**.

Figure 20.3: Downtown Standard Surface Treatment with Street Trees

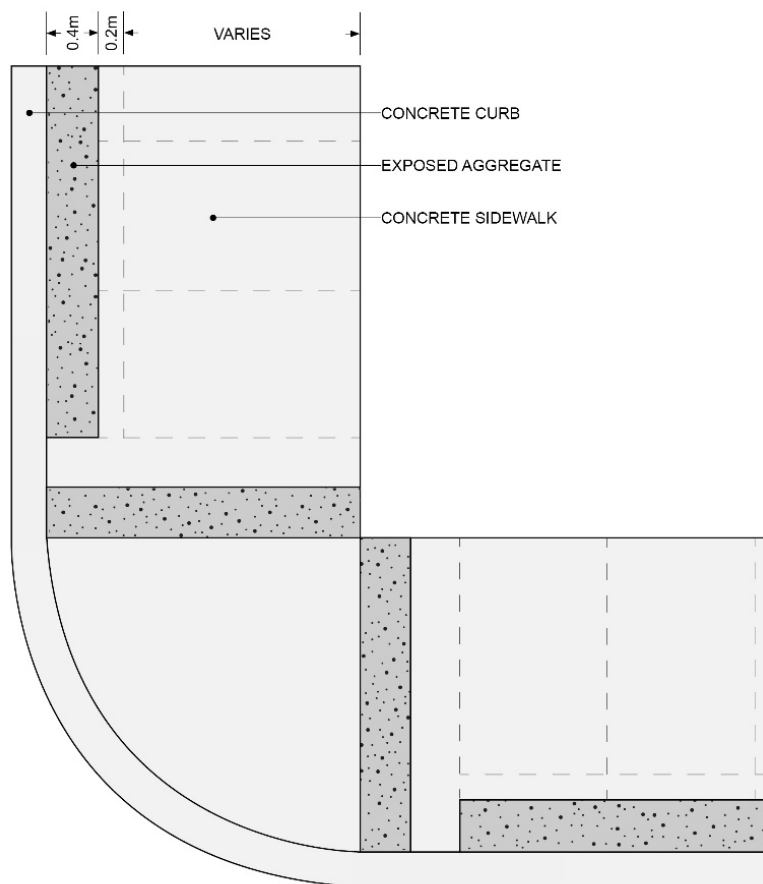


20.1.2.3 Standard Surface Treatment at Corners

The standard surface treatment at corners includes two 0.40-metre exposed aggregate bands, one at each end of the curb radius, perpendicular to the curb edge with two additional saw cuts in the concrete sidewalk that are parallel to, and 0.40 metres from, the exposed aggregate bands.

This standard treatment is illustrated in **Figure 20.4**.

Figure 20.4: Downtown Standard Surface Treatment at Corners



20.1.2.4 Curb Engraving

Street names are to be engraved at each intersection on the curb, with the following specifications:

- Front Style: Century Gothic, bold.
- Lettering size: point size 400 (100mm height)
- Ensure engravings are centred on curb vertically and centered within crosswalk markings and tactile plates.
- Ensure lettering follows curve of curb and is consistent width from edge of front curb (25mm).
- Engraving in concrete curb shall be done using sandblasting with very fine particle size.
- Engraved letters shall be to a depth of 6mm or equal to the width of the engraving, whichever is less.
- Finish engraved letters with black colour fill.

- Gaps between letters to be no less than 20mm.
- Spaces between word to be no less than 100mm.

20.1.2.5 Sod Boulevard Treatment

In instances where there is an existing grass boulevard that is essential to the heritage character of the streetscape, as identified by the *Downtown London Heritage Conservation District Plan*, this grass boulevard should be preserved or maintained. Application of the sod boulevard treatment will be determined within the context of the work being completed. In these instances, a grass strip may replace the curbside exposed aggregate band in the surface treatments described in **Sections 20.1. 2.1, 20.1.2.2, and 20.1.2.3**, and may vary in width dependent on the context.

20.1.3 Enhanced Crosswalk Treatment

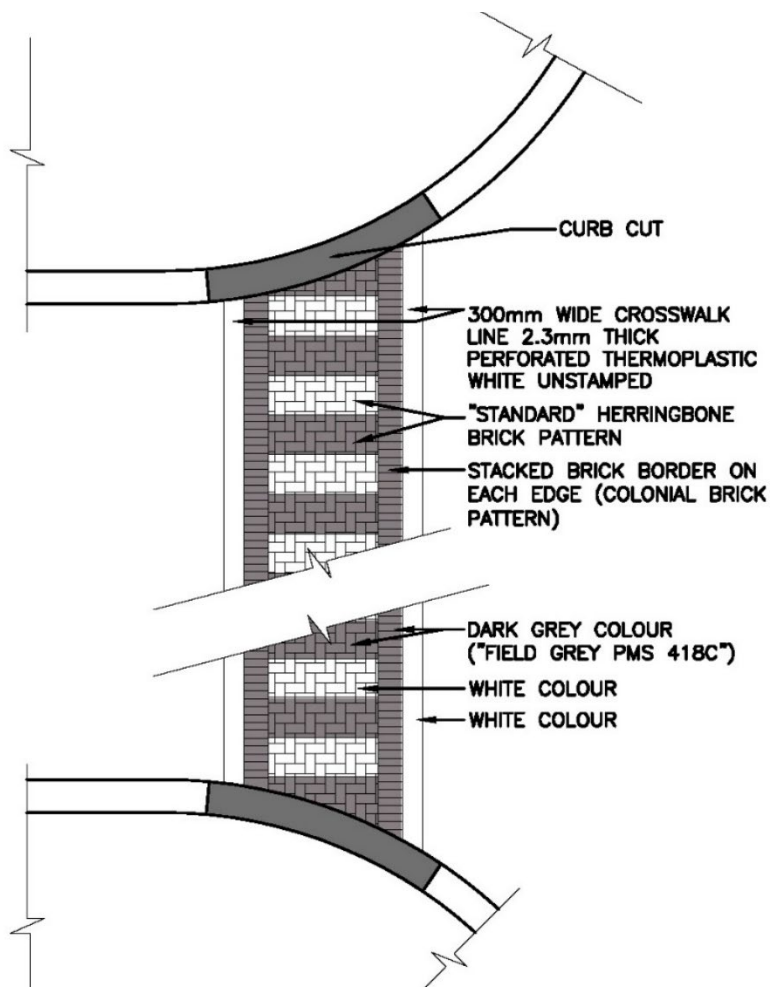
20.1.3.1 Location of Enhanced Crosswalk Treatment

The enhanced crosswalk treatment is to be applied at intersections in close proximity to rapid transit stations, as well as any intersection within the area where the Downtown Streetscape Standards apply as determined by the City of London.

20.1.3.2 Enhanced Crosswalk Treatment Details

The enhanced crosswalk treatment is a stamped asphalt treatment (TrafficPatternXD HUBS Surface Systems) or approved equal. The pattern is “Standard Herringbone” with a “Stacked Brick Border”. The primary crosswalk colour is “Field Grey” (PMS 418C) with white transverse and lateral markings. Details are provided in **Figure 20.5**.

Figure 20.5: Downtown Enhanced Crosswalk Treatment



20.1.4 Street Furniture

20.1.4.1 Bicycle Parking

The bike rack standard is the TeMo bike rack in black powder coat. Bike racks can be placed as singles or in groups.

20.1.4.2 Street Lighting

The street light standard is the Lumec Urbanscape LED Post-Top in black powder coat.

20.1.4.3 Tree Guards

The tree guard standard is the GreenBlue Urban Thames Tree Guard typically in the 1.2 metres by 1.2 metres option in black powder coat. Please also see **Section 12.12** diagram D-35A.

20.1.4.4 Tree Grates

The tree grate standard is the GreenBlue Urban Thames Tree Grate in fine-textured black powder coat.

20.1.4.5 Garbage Receptacles

The garbage receptacle standard is the J. A-STEELWORKS 2-3-Bin-Assy garbage receptacle in black powder coat.

20.1.4.6 Approved Equal

The street furniture described in **Section 20.1.4** are the approved streetscape elements for the Downtown Streetscape Standards area. If circumstances arise where the approved street furniture element cannot be implemented, such as due to product discontinuation, an equal product may be implemented subject to the approval of the City.

20.2 Richmond Street Streetscape Standards

20.2.1 Introduction

20.2.1.1 Purpose

The purpose of these standards is to provide direction to projects that involve modifications to the rights-of-way along Richmond Street, with the goal of creating a consistent streetscape treatment that is unique to Richmond Street.

20.2.1.2 Location

The Richmond Street Streetscape standards apply to Richmond Street between Oxford Street and York Street.

This street segment is more precisely identified in Figure 20.6. Where minor discrepancies exist between the above description and Figure 20.6, Figure 20.6 takes precedence.

Figure 20.6: Richmond Streetscape Standard Location Map

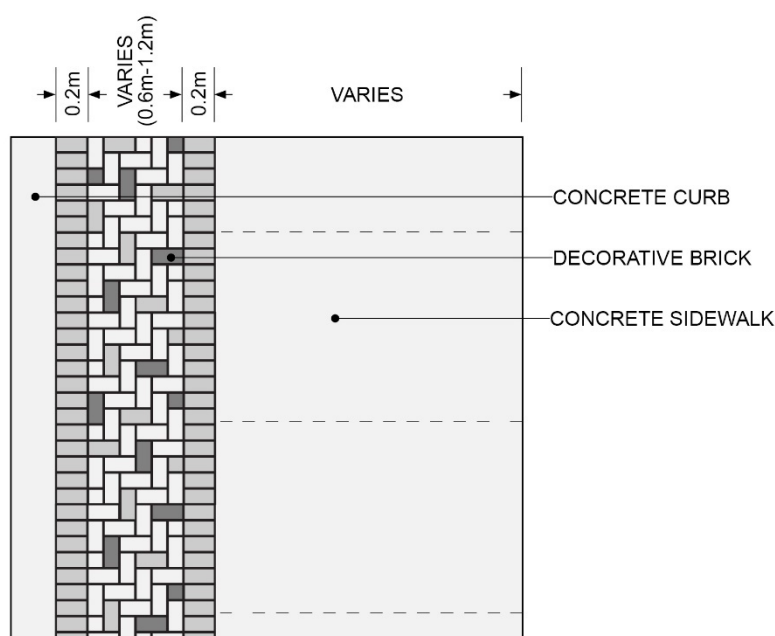


20.2.2 Surface Treatment

20.2.2.1 Standard Surface Treatment

The standard surface treatment includes a decorative brick detail (see **Section 20.2.2.4**) along the curb edge. The brick detail includes a soldier course border with a standard herringbone brick pattern between. The herringbone brick pattern detail varies in width between a minimum of 0.6 meters to a maximum of 1.2 metres and is dependent on the space available. The standard surface treatment is illustrated in **Figure 20.7**.

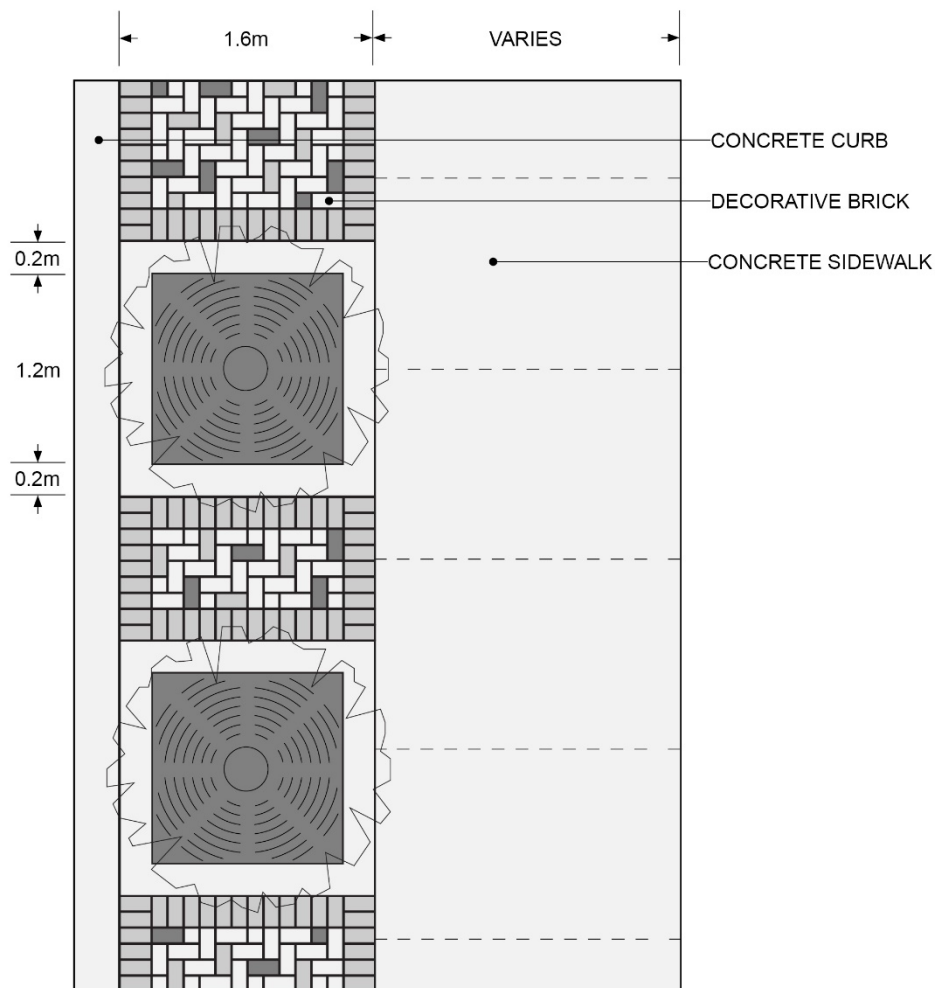
Figure 20.7: Richmond Street Standard Surface Treatment



20.2.2.2 Standard Surface Treatment with Street Trees

In instances where street trees can be accommodated, the decorative brick detail (see **Section 20.2.2.4**) is 1.6 meters wide along the curb edge and a 0.2 metre concrete border is applied around the tree grate. This standard treatment is illustrated in **Figure 20.8**.

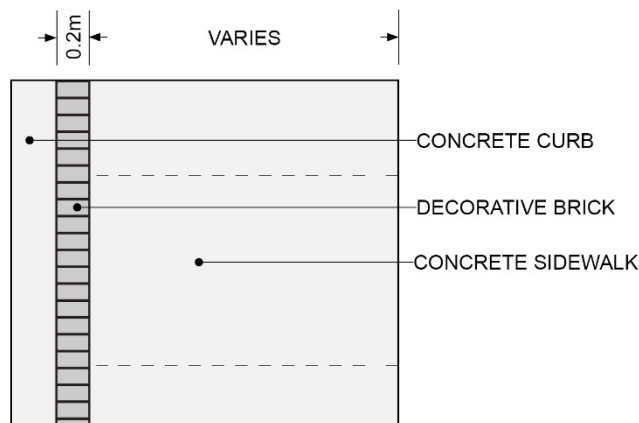
Figure 20.8: Richmond Street Standard Surface Treatment with Street Trees



20.2.2.3 Minimum Standard Surface Treatment

The minimum standard surface treatment should only be used where space is constrained and where the treatment in **Section 20.2.2.1** and **20.2.2.2** cannot be accommodated due to the specific context. The minimum standard surface treatment includes a single soldier course of brick adjacent to the curb (**see Section 20.2.2.4**) and is illustrated in **Figure 20.9**.

Figure 20.9: Richmond Street Minimum Standard Surface Treatment

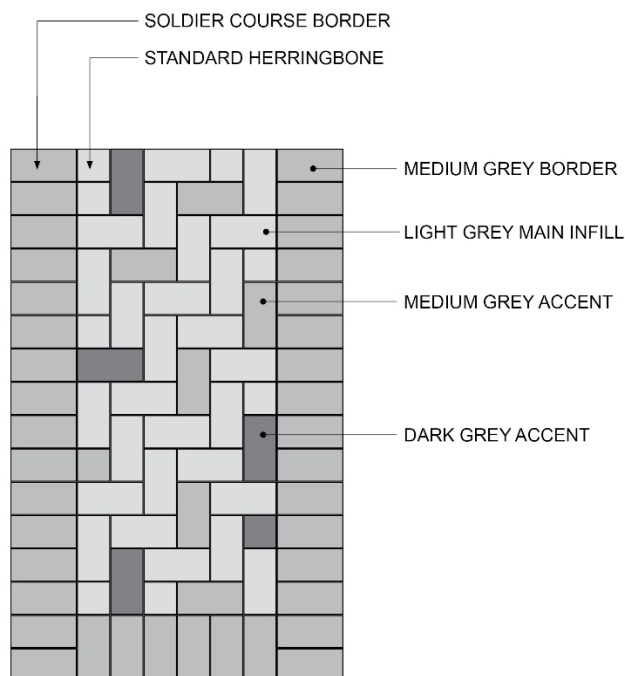


20.2.2.4 Decorative Brick Detail

The application of the decorative brick detail applies to the standard surface treatments as described in **Section 20.2.2.1**, **20.2.2.2** and **20.2.2.3**, as appropriate. Where a soldier course brick detail is used, the bricks should be medium grey. Pavers are to be installed on top of a concrete base in all instances.

Where a standard herringbone pattern is used, as described in **Section 20.2.2.1** and **20.2.2.2**, the main infill should be light grey and make up approximately 80 percent of the pattern. The accent bricks should be medium grey and dark grey and each make up approximately 10 percent of the bricks used within the standard herringbone pattern. This standard brick detail is illustrated in **Figure 20.10**.

Figure 20.10: Richmond Street Decorative Brick Detail



20.2.2.5 Curb Engraving

Street names are to be engraved at each intersection on the curb, with the following specifications:

- Front Style: Century Gothic, bold.
- Lettering size: point size 400 (100mm height)
- Ensure engravings are centred on curb vertically and centered within crosswalk markings and tactile plates.
- Ensure lettering follows curve of curb and is consistent width from edge of front curb (25mm).
- Engraving in concrete curb shall be done using sandblasting with very fine particle size.
- Engraved letters shall be to a depth of 6mm or equal to the width of the engraving, whichever is less.
- Finish engraved letters with black colour fill.
- Gaps between letters to be no less than 20mm.
- Spaces between word to be no less than 100mm.

20.2.3 Street Furniture

20.2.3.1 Bicycle Parking

The bike rack standard is the TeMo bike rack in black powder coat. Bike racks can be placed as singles or in groups.

20.2.3.2 Street Lighting

The street light standard is the Lumec Urbanscape LED Post-Top in black powder coat.

20.2.3.3 Tree Guards

The tree guard standard is the GreenBlue Urban Thames Tree Guard typically in the 1.2 metres by 1.2 metres option in black powder coat. Please see **Section 12.12** diagram D-35A.

20.2.3.4 Tree Grates

The tree grate standard is the GreenBlue Urban Thames Tree Grate in fine-textured black powder coat.

20.2.3.5 Approved Equal

The street furniture described in **Section 20.2.3** are the approved streetscape elements for the Richmond Street Streetscape Standards area. If circumstances arise where the approved street furniture element cannot be implemented, such as due to product discontinuation, an equal product may be implemented subject to the approval of the City.

20.3 Dundas Street (Old East Village) Streetscape Standards

20.3.1 Introduction

20.3.1.1 Purpose

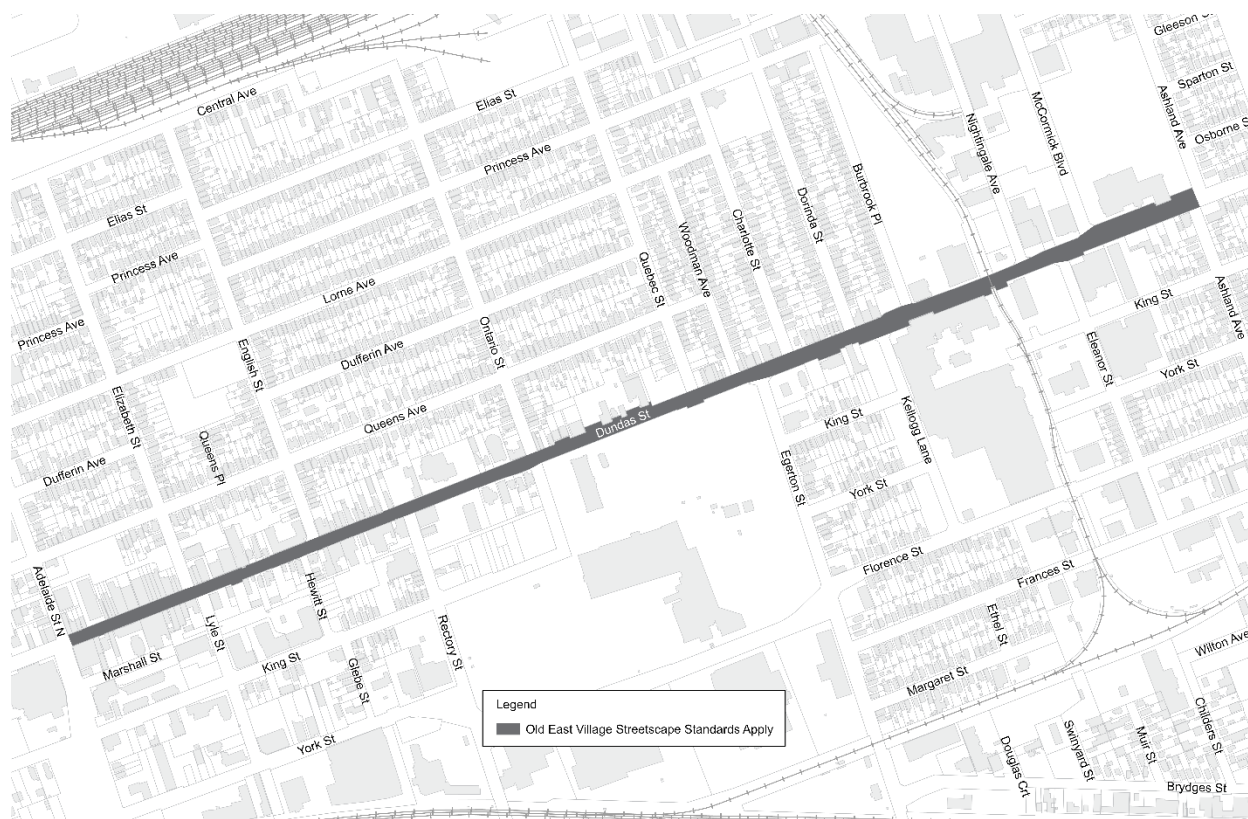
The purpose of these standards is to provide direction to projects that involve modifications to the right-of-way on Dundas Street within Old East Village, continuing east to Ashland Avenue, with the goal of creating a consistent streetscape treatment that is unique to Old East Village.

20.3.1.2 Location

The Old East Village Streetscape Standards shall apply to Dundas Street between Adelaide Street North and Ashland Avenue.

This street segment is more precisely identified in Figure 20.11. Where minor discrepancies exist between the above description and Figure 20.11, Figure 20.11 takes precedence.

Figure 20.11: Old East Village Streetscape Standard Location Map

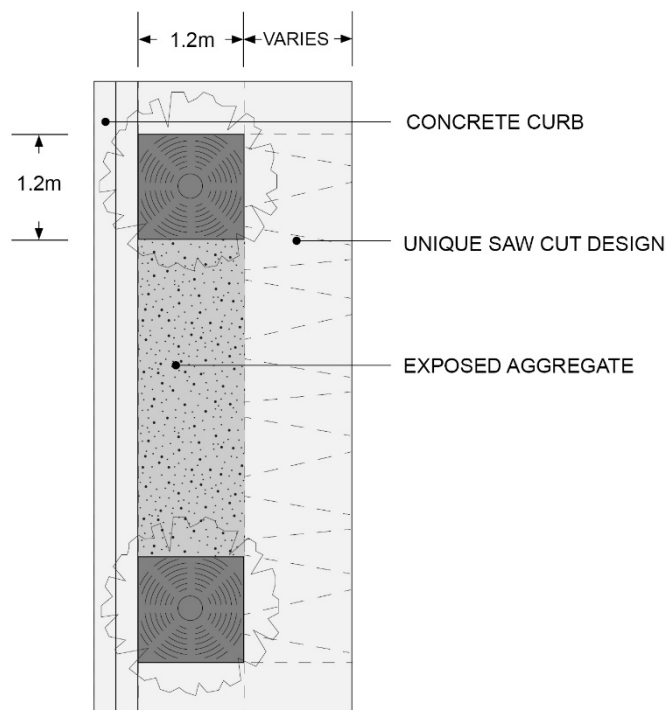


20.3.2 Surface Treatment

20.3.2.1 Standard Surface Treatment

The standard surface treatment for Dundas Street from Adelaide Street North to Ashland Avenue includes an aggregate band between two tree grates with unique saw cut in the concrete sidewalk between the exposed aggregate and tree grates and the building face. The dimensions will vary based on the width of the right-of-way and will be subject to approval by the City. This treatment is illustrated in Figure 20.12.

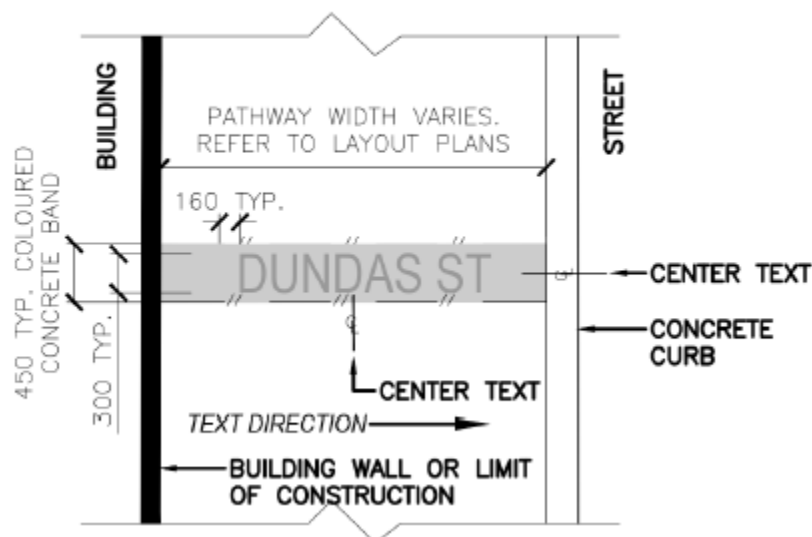
Figure 20.12: Standard Surface Treatment on Dundas Street



20.3.2.2 Street Name Engraving

- Letters shall be located within a coloured concrete band and shall be centered vertically and horizontally within the concrete band.
- Font style: Arial Narrow
- Lettering Size: 300mm typ.
- Depth of Engraving: 1.5mm
- Concrete Band Size: 450mm typ.
- Engraving in concrete curb shall be done using sandblasting with very fine particle size or approved alternative.
- This treatment is further illustrated in Figure 20.13.

Figure 20.13: Street Name Engraving



20.3.3 Street Furniture

20.3.3.1 Bicycle Parking

The bike rack standard is the TeMo bike rack in black powder coat. Bike racks can be placed as singles or in groups.

20.3.3.2 Street Lighting

The street light standard is the Lumec Urbanscape LED Post-Top in Night Blue (RAL 5022).

20.3.3.3 Tree Guard

The tree guard standard is the GreenBlue Urban Thames Tree Guard typically in the 1.2 metres by 1.2 metres option in Black Green (RAL 6012). Please see Section 12.12 diagram D-35A.

20.3.3.4 Tree Grate

The tree grate standard is the GreenBlue Urban Thames Tree Grate in Black Green (RAL 6012).

20.3.3.5 Garbage Receptacles

The garbage receptacle standard is the J. A-STEELWORKS Bin-Assy garbage receptacle in black powder coat.

20.3.3.6 Approved Equal

The street furniture described in Section 20.3.3 are the approved streetscape elements for the Old East Village area. If circumstances arise where the approved street furniture element cannot be implemented, such as due to product discontinuation, an equal product may be implemented subject to the approval of the City.

20.4 City-wide Streetscape Standards

20.4.1 Introduction

20.4.1.1 Purpose

The purpose of these standards is to provide direction to projects that involve modifications to the rights-of-way within the City of London, outside of the specific areas outlined in Section 20.1, 20.2, 20.3 and 20.5 with the goal of creating a consistent streetscape treatment.

20.4.1.2 Location

The City-wide Streetscape Standards apply to all areas of the city outside of the specific areas outlined in Section 20.1, 20.2, 20.3 and 20.5.

20.4.2 Enhanced Crosswalk Treatment

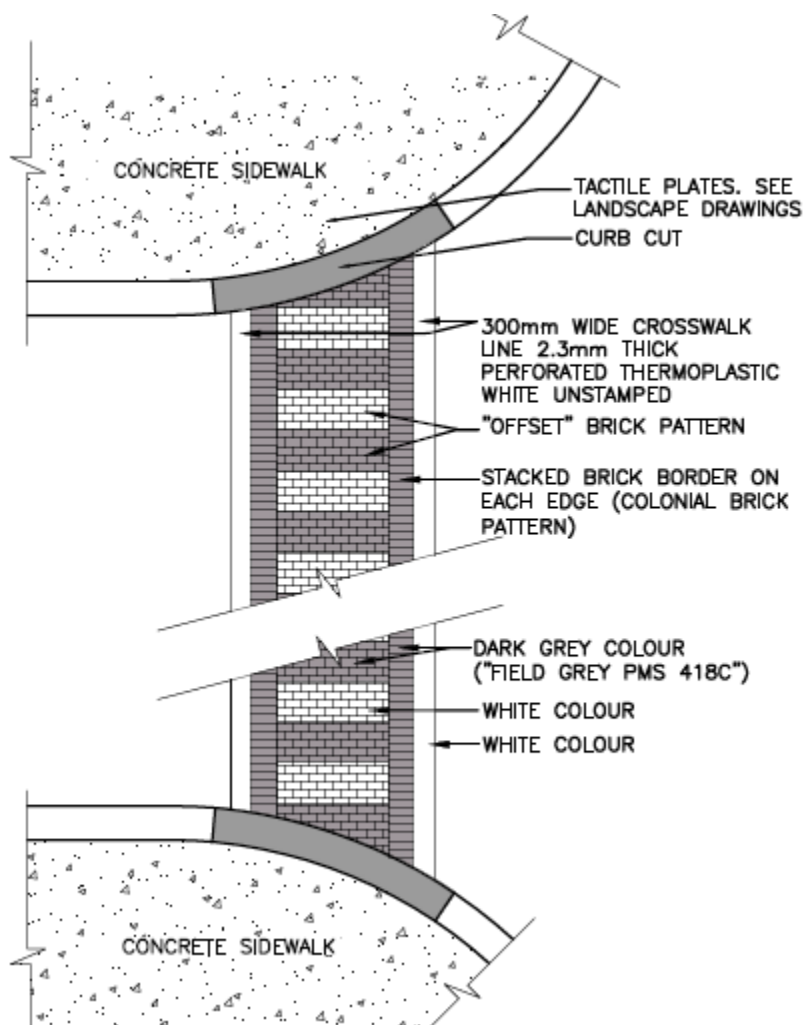
20.4.2.1 Location of Enhanced Crosswalk Treatment

The enhanced crosswalk treatment is to be applied at intersections in close proximity to rapid transit stations, as well as any high-profile intersection deemed appropriate by Urban Design staff in the City.

20.4.2.2 Enhanced Crosswalk Treatment Details

The enhanced crosswalk treatment is a stamped asphalt treatment (TrafficPatternXD HUBS Surface Systems or approved equal). The pattern is “Offset Brick Pattern” with a “Stacked Brick Border”. The primary crosswalk colour is “Field Grey” (PMS 418C) with white transverse and lateral markings. Details are provided in Figure 20.14.

Figure 20.14: Enhanced Crosswalk Treatment



20.4.3 Street Furniture

20.4.3.1 Bicycle Parking

The bike rack standard is the TeMo bike rack in Black Green (RAL 6012). Bike racks can be placed as singles or in groups.

20.4.3.2 Tree Guards

The tree guard standard is the GreenBlue Urban Thames Tree Guard typically in the 1.2 metres by 1.2 metres option in Black Green (RAL 6012). Please see Section 12.12 diagram D-35A.

20.4.3.3 Tree Grates

The tree grate standard is the GreenBlue Urban Thames Tree Grate in Black Green (RAL 6012).

20.4.3.4 Garbage Receptacles

The garbage receptacle standard is the J. A-STEELWORKS Bin-Assy garbage receptacle in Black Green (RAL 6012).

20.4.3.5 Benches

The standard bench is the Maglin 510 bench in Black Green (RAL 6012).

20.4.3.6 Approved Equal

The street furniture described in Section 20.4.3 are the approved City-wide streetscape elements. If circumstances arise where the approved street furniture element cannot be implemented, such as due to product discontinuation, an equal product may be implemented subject to the approval of the City.

20.5 Heritage Conservation Districts Streetscape Standards

20.5.1 Introduction

20.5.1.1 Purpose

The purpose of these standards is to provide direction to projects that involve modifications to the rights-of-way within Heritage Conservation Districts, with the goal of creating a consistent streetscape treatment that is unique to the Heritage Conservation Districts.

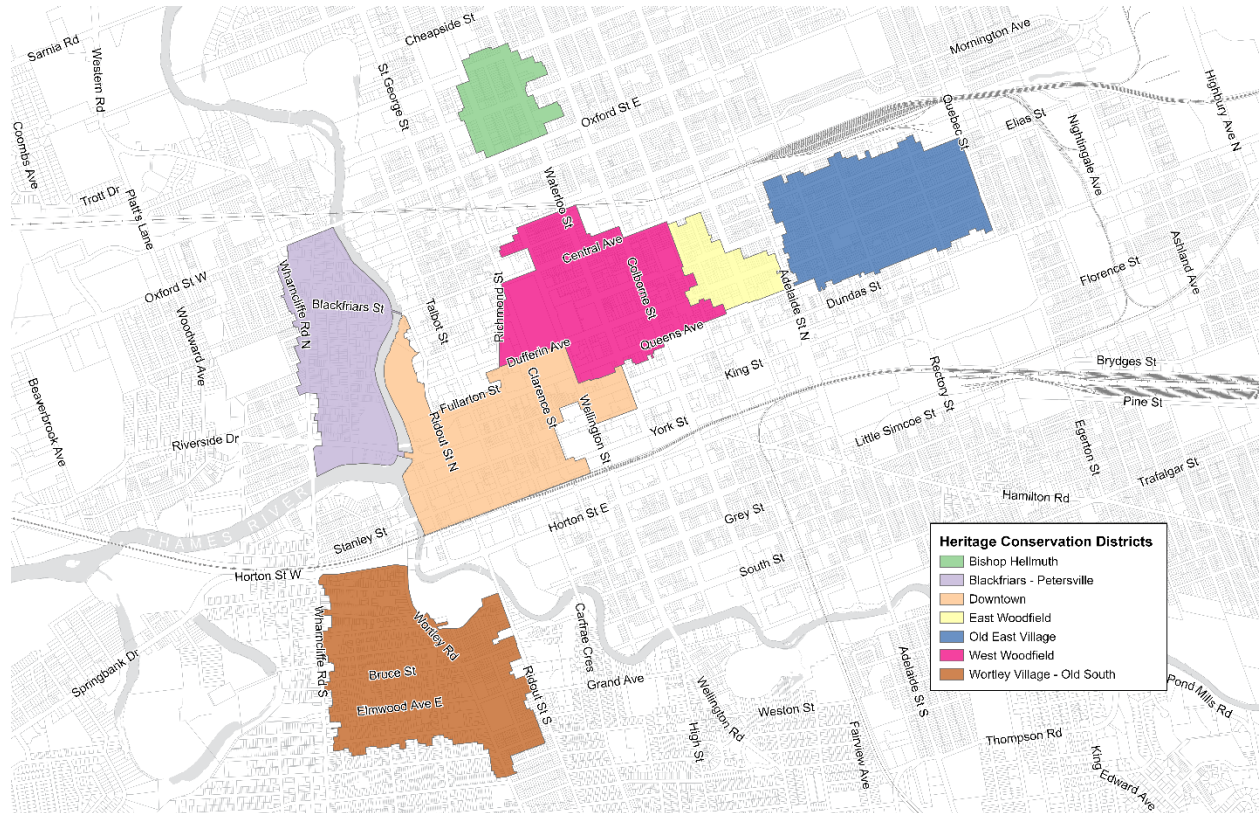
20.5.1.2 Location

The Heritage Conservation Districts Streetscape Standards shall apply to the following Heritage Conservation Districts as identified in Figure 20.15.

- Bishop Hellmuth
- Blackfriars-Petersville
- Downtown
- East Woodfield
- Old East Village
- West Woodfield
- Wortley Village-Old South

The district boundaries and rights-of-way in the Heritage Conservation Districts are more precisely identified in Figure 20.16 – Figure 20.22.

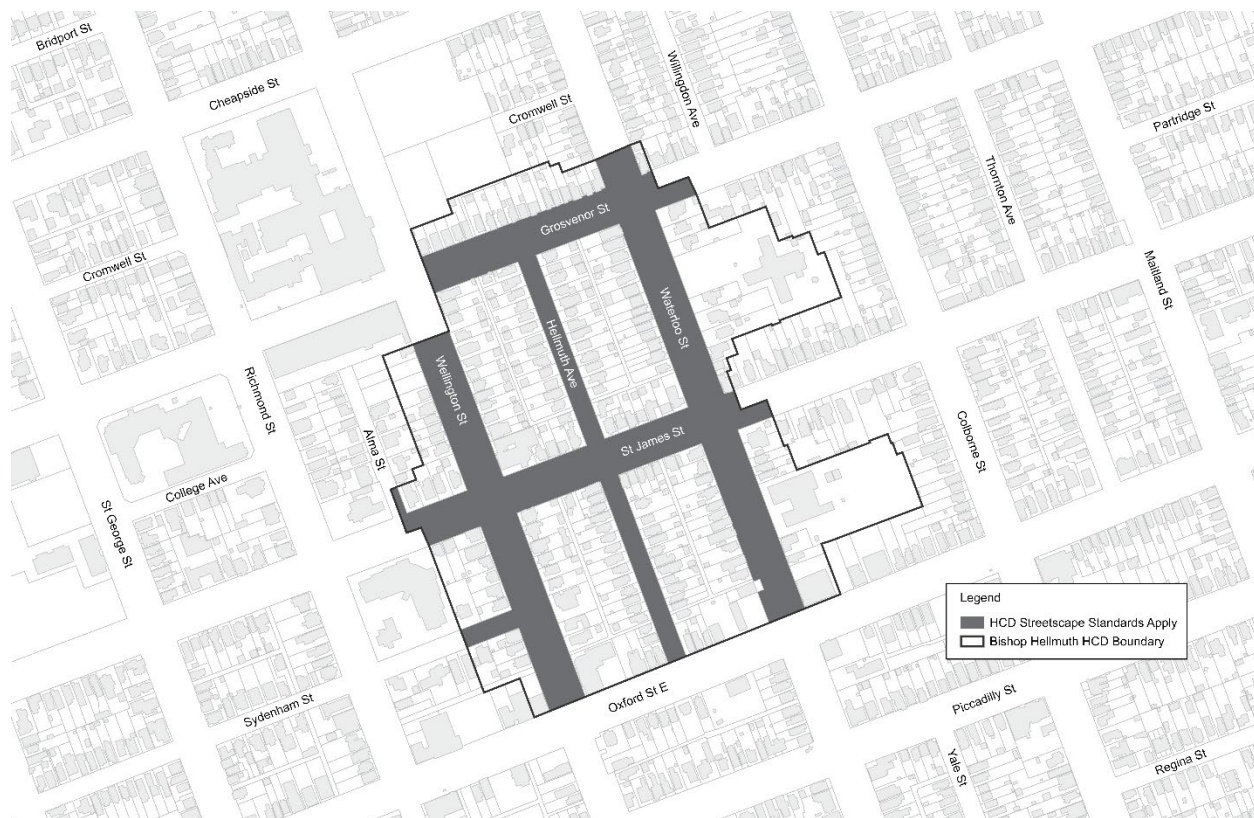
Figure 20.15: Heritage Conservation Districts Streetscape Standards Location Map



20.5.1.3 Bishop Hellmuth

The Heritage Conservation Districts Streetscape Standards shall apply to the Bishop Hellmuth Heritage Conservation District identified in Figure 20.16. Where minor discrepancies exist with Figure 20.15, Figure 20.16 takes precedence.

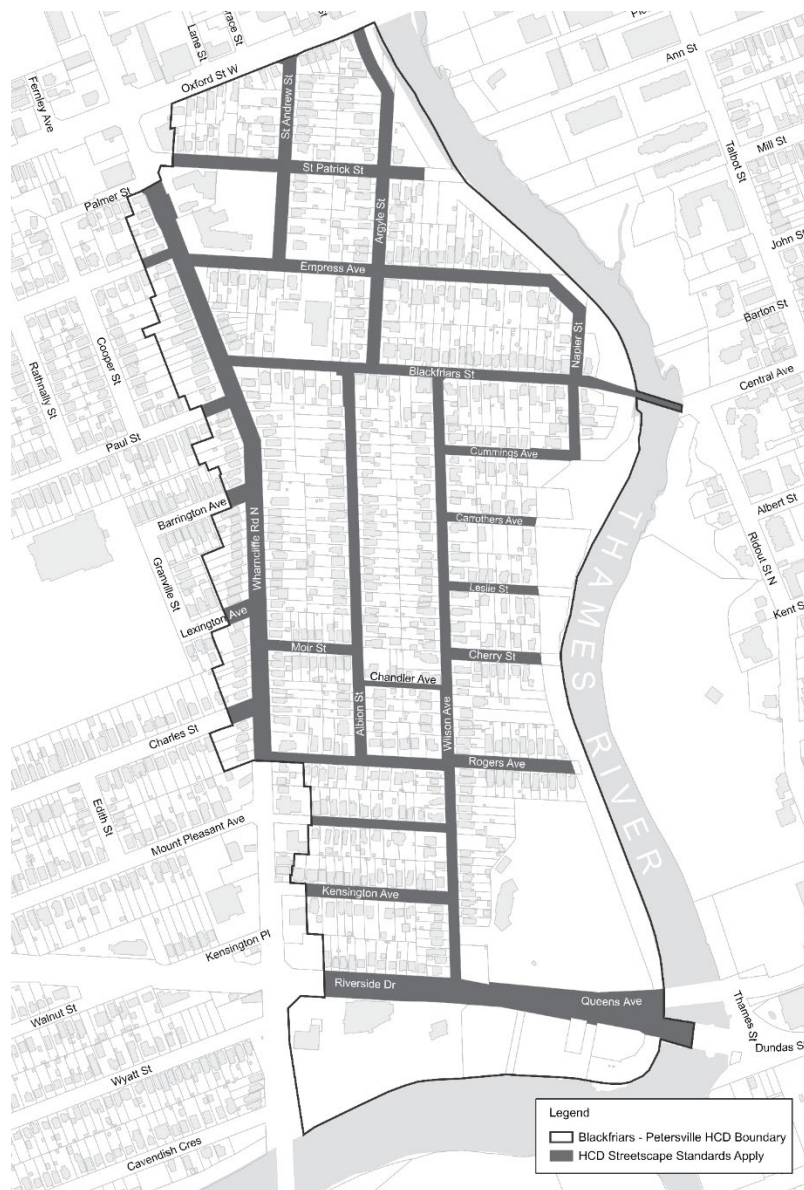
Figure 20.16: Heritage Conservation Districts Streetscape Standards Bishop Hellmuth Location Map



20.5.1.4 Blackfriars - Petersville

The Heritage Conservation Districts Streetscape Standards shall apply to the Blackfriars-Petersville Heritage Conservation District identified in Figure 20.17. Where minor discrepancies exist with Figure 20.15, Figure 20.17 takes precedence.

Figure 20.17: Heritage Conservation Districts Streetscape Standards Blackfriars-Petersville Location Map



20.5.1.5 Downtown

The Heritage Conservation Districts Streetscape Standards shall apply to the Downtown Heritage Conservation District identified in Figure 20.18. Where minor discrepancies exist with Figure 20.15, Figure 20.18 takes precedence.

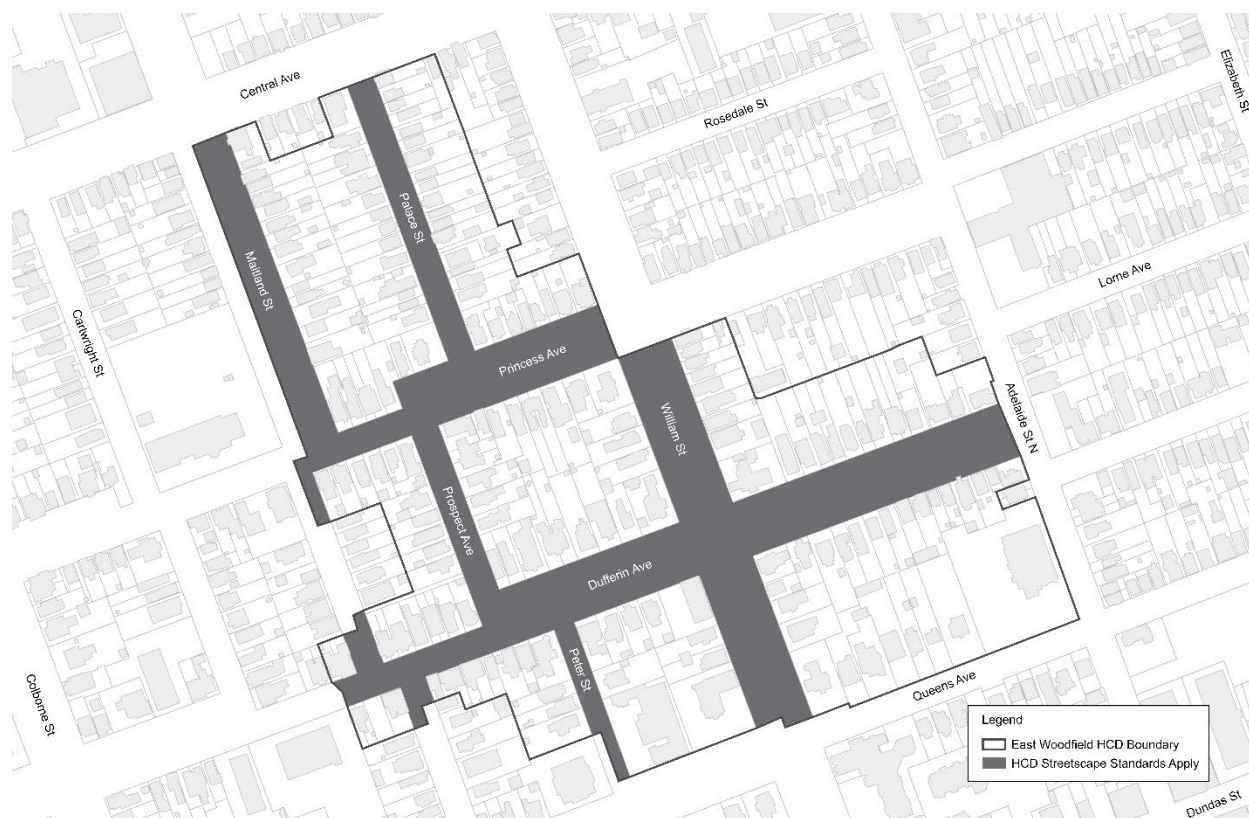
Figure 20.18: Heritage Conservation Districts Streetscape Standards Downtown Location Map



20.5.1.6 East Woodfield

The Heritage Conservation Districts Streetscape Standards shall apply to the East Woodfield Heritage Conservation District identified in Figure 20.19. Where minor discrepancies exist with Figure 20.15, Figure 20.19 takes precedence.

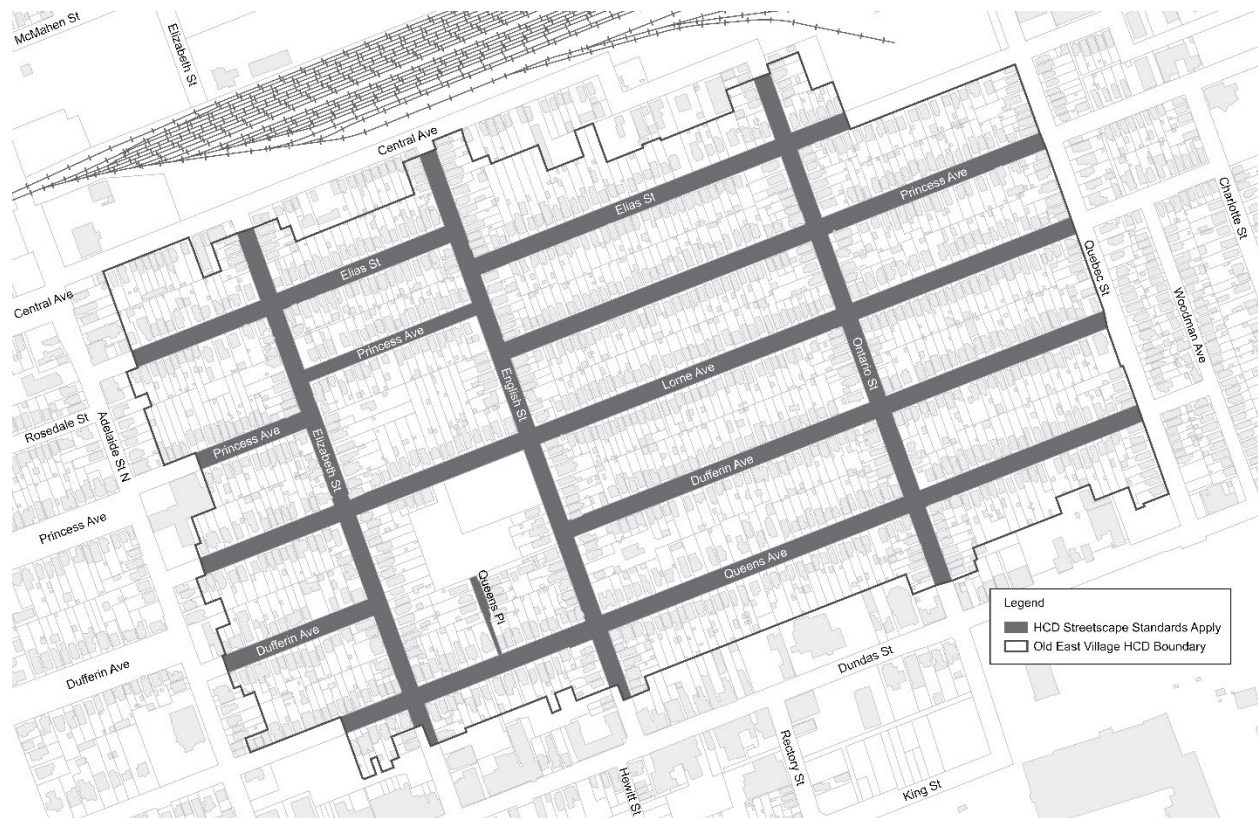
Figure 20.19: Heritage Conservation Districts Streetscape Standards East Woodfield Location Map



20.5.1.7 Old East Village

The Heritage Conservation Districts Streetscape Standards shall apply to the Old East Village Heritage Conservation District identified in Figure 20.20. Where minor discrepancies exist with Figure 20.15, Figure 20.20 takes precedence.

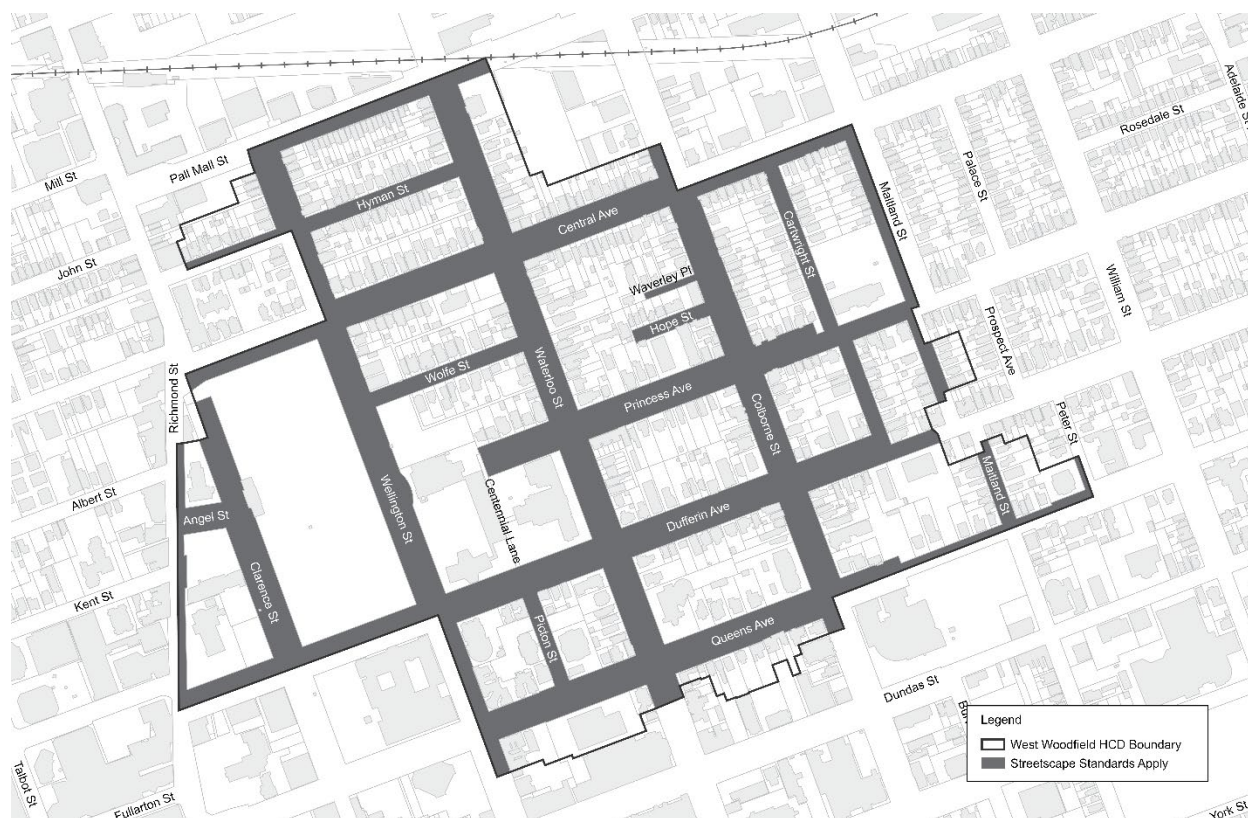
Figure 20.20: Heritage Conservation Districts Streetscape Standards Old East Village Location Map



20.5.1.8 West Woodfield

The Heritage Conservation Districts Streetscape Standards shall apply to the West Woodfield Heritage Conservation District identified in Figure 20.21. Where minor discrepancies exist with Figure 20.15, Figure 20.21 takes precedence.

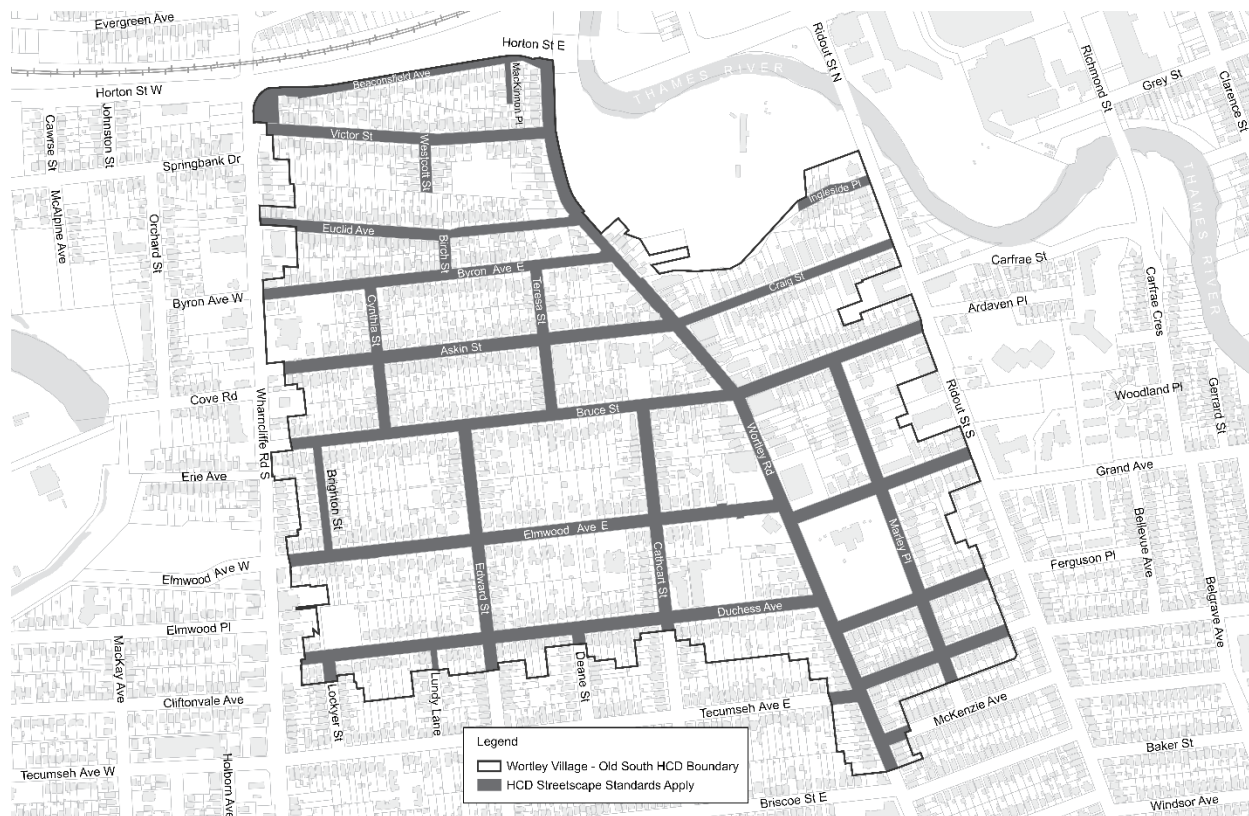
Figure 20.21: Heritage Conservation Districts Streetscape Standards West Woodfield Location Map



20.5.1.9 Wortley Village – Old South

The Heritage Conservation Districts Streetscape Standards shall apply to the Wortley Village – Old South Heritage Conservation District identified in Figure 20.22. Where minor discrepancies exist with Figure 20.15, Figure 20.22 takes precedence.

Figure 20.22: Heritage Conservation Districts Streetscape Standards Wortley Village-Old South Location Map



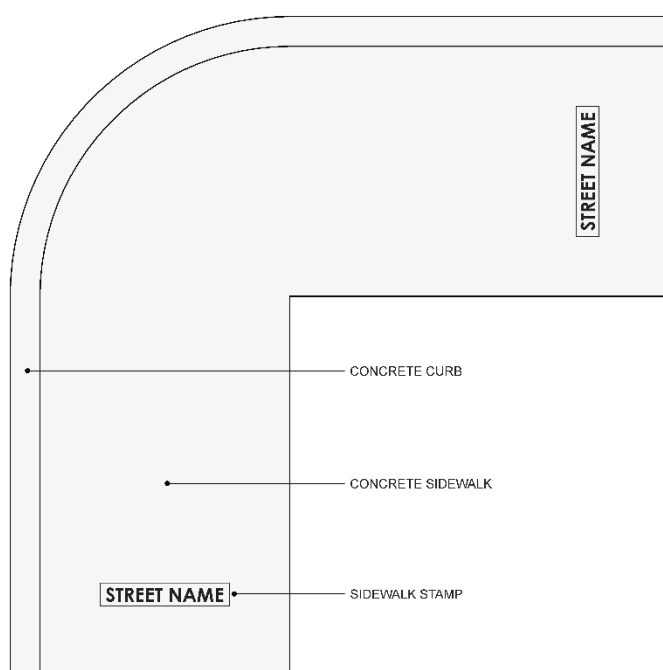
20.5.2 Surface Treatment

20.5.2.1 Sidewalk Street Name Stamping

The standard treatment at corners includes sidewalk street name stamping at each intersection within the Heritage Conservation District Streetscape right-of-way.

This standard treatment is illustrated in Figure 20.23.

Figure 20.23: General Location for Standard Sidewalk Street Name Stamping in Heritage Conservation Districts



Street names are to be stamped at each intersection on the sidewalk, with the following specifications and as illustrated in Figure 20.24 and Figure 20.25:

- Street name sidewalk stamps are to be located within a short distance from the corner (as shown in Figure 20.23) to be determined per project.
- Street names are to be horizontally centred within the sidewalk.
- Street names should appear on one line.
- Lettering sizing is 3" in height.
- Street suffixes should be abbreviated (with a period) as follows:
 - Avenue – Ave.
 - Drive – Dr.
 - Lane – Ln.
 - Place – Pl.

- Road – Rd.
- Street – St.
- Reference Canada Post standard addressing abbreviations for other street suffixes not indicated above.
- Street names with a cardinal direction (e.g. Elmwood Ave. E.) should include an abbreviation with a period (e.g. North – N.)
- Where heritage Conservation District overlap with other streetscape standards, the precise location of street name stamping will be addressed through the detailed design phase

Figure 20.24: Details for Standard Sidewalk Street Name Stamping in Heritage Conservation Districts – Small Width



Figure 20.25: Details for Standard Sidewalk Street Name Stamping in Heritage Conservation Districts – Large Width



20.5.3 Street Furniture

20.5.3.1 Street Lighting

The street lighting standard in Heritage Conservation Districts is the Cooper Lighting Solutions -- STREETWORKS SDL Arm Mount, Springdale.

20.5.3.2 Approved Equal

The street lighting described in Section 20.5.3.1 is the approved lighting standard in Heritage Conservation Districts. If circumstances arise where the approved street lighting standard cannot be implemented, such as due to product discontinuation, an equal product may be implemented subject to the approval of the City.

20.5.3.3 Street Signs

New Heritage Conservation District street signage is to be installed, replacing standard City street signage, subject to the approval of the City.

Existing Heritage Conservation District street signage is to be removed, retained and reinstalled through construction.