

Report to Planning and Environment Committee

To: Chair and Members
Planning and Environment Committee

From: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic Development

Subject: Sandhyaji Homes Inc. (c/o Zelinka Priamo Ltd.)
325 Southdale Road East
File Number: Z-9794, Ward 12

Date: Public Participation Meeting on: December 3, 2024

Recommendation

That, on the recommendation of the Director, Planning and Development, the following action be taken with respect to the application of Sandhyaji Homes Inc. (c/o Zelinka Priamo Ltd.) relating to the property located at 325 Southdale Road East, the proposed by-law attached hereto as Appendix "A" **BE INTRODUCED** at the Municipal Council meeting December 17, 2024, to amend Zoning By-law No. Z.-1, in conformity with the Official Plan, The London Plan, to change the zoning of the subject property **FROM** a Residential R3 (R3-3) Zone, **TO** a Holding Residential R5 Special Provision (h*R5-6(_)) Zone.

IT BEING NOTED, that the above noted amendment is being recommended for the following reasons:

- i) The recommended amendment is consistent with the *Provincial Policy Statement, 2024 (PPS)*, which encourages growth in settlements areas and land use patterns based on densities and a mix of land uses that provide for a range of uses and opportunities for intensification and redevelopment;
- ii) The recommended amendment conforms to The London Plan, including but not limited to Key Directions, City Building policies, and the Neighbourhoods Place Type policies;
- iii) The recommended amendment conforms to the Southwest Area Secondary Plan, including but not limited to the North Longwoods Neighbourhood policies; and
- iv) The recommended amendment would permit an appropriate form of development at an intensity that is appropriate for the site and surrounding neighbourhood.

Executive Summary

Summary of Request

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R3 (R3-3) Zone to a Residential R5 Special Provision (R5-6(_)) Zone. Special provisions requested by the applicant include: a minimum front yard setback of 1.6 metres (post-road widening), whereas a minimum of 8.0 metres is required; a minimum lot frontage of 22.4 metres, whereas a minimum of 30.0 metres is required; and an interior side yard setback of 5.0 metres, whereas a minimum of 5.6 metres is required.

Purpose and the Effect of Recommended Action

Staff are recommending approval of the requested Zoning By-law amendment and special provisions, with the exception of the requested special provision for a reduced front yard setback of 1.6 metres. Instead, Staff are recommending a special provision for a minimum front yard setback of 10.0 metres and a maximum front yard setback of 12.0 metres from the existing front lot line. A holding provision is also recommended to ensure that development will not occur until adequate servicing can be demonstrated by the applicant (h-holding provision).

The recommended action will permit a 3.5 storey, **10-unit** stacked townhouse development.

Linkage to the Corporate Strategic Plan

This recommendation will contribute to the advancement of Municipal Council's 2023-2027 Strategic Plan in the following ways:

- **Wellbeing and Safety**, by promoting neighbourhood planning and design that creates safe, accessible, diverse, walkable, healthy, and connected communities.
- **Housing and Homelessness**, by ensuring London's growth and development is well-planned and considers use, intensity, and form.
- **Housing and Homelessness**, by supporting faster/streamlined approvals and increasing the supply of housing with a focus on achieving intensification targets.

Analysis

1.0 Background Information

1.1 Previous Reports Related to this Matter

December 9, 2002 – O-6402 – 315-349 Southdale Road East & 3026-3025 White Oak Road.

October 29, 2008 – O-7507 – 315-349 Southdale Road East & 3026-3025 White Oak Road.

1.2 Planning History

In May 2002, an application was made for the property at 315 Southdale Road East to change the Official Plan designation of this vacant corner lot from Low Density Residential to Multi-Family, High Density Residential and to change the zoning from a single-family residential zone to a convenience commercial zone. Staff recommended refusal of the application. During the application review process, other property owners within this area requested that the City consider the land use designation of their lands immediately south and east of the subject property. As a result, the application for 315 Southdale Road was referred back to Staff for a comprehensive review of the land use designations of the entire area. On February 16, 2004, Municipal Council resolved that no further action be taken with respect to the application initiated by the City of London to amend the Official Plan relating to these properties at the southeast corner of Southdale Road and White Oak Road.

Subsequently, the Council decision was appealed to the Ontario Municipal Board (OMB) for the entire southeast quadrant of the Southdale Road – White Oak Road intersection. The OMB dismissed the appeals, therefore approving the Official Plan amendment to permit the development of townhouse units.

1.3 Property Description and Location

The subject lands are located on the south side of Southdale Road East between White Oak Road and Jalna Boulevard, in the White Oak Planning District. The subject lands have an area of approximately 0.19 hectares (0.49 acres), frontage of 22.4 metres along Southdale Road East, and depth of 89.3 metres. The property is currently occupied by a single detached dwelling with an accessory structure in the rear yard. Vehicular access is provided from an existing driveway connecting to Southdale Road East.

The subject lands are within an urban neighbourhood consisting primarily of low density and medium density residential uses in the form of single detached dwellings, duplexes and townhouses. The neighbouring property to the west contains a single detached dwelling, and abutting to the south is a vacant lot zoned for a Neighbourhood Facility. Directly to the east are 2.5 storey-stacked townhouse developments and across Southdale Road to the north is a Place of Worship. Broader surrounding land uses include a mix of low and medium density residential uses, commercial uses along the Wharncliffe Road and Southdale Road corridors, schools and open space.

Southdale Road East is classified as a Civic Boulevard on Map 3 – Street Classifications of The London Plan. The road segment between White Oak Road and Jalna Boulevard has a traffic volume of approximately 24,000 vehicles per day. Southdale Road East has public sidewalks on both sides of the street, and a signalized pedestrian crossover is located approximately 70 metres to the west at the intersection of White Oak Road/Homeview Road and Southdale Road East. Finally, approximately 40 metres to the east is an LTC stop serviced by LTC transit route 10.

Site Statistics:

- Current Land Use: Residential
- Frontage: 22.4 metres (73.4 ft)
- Depth: 89.3 metres (292.9 ft)
- Area: 0.19 hectares (0.49 acres)
- Shape: Regular (rectangle)
- Located within the Built Area Boundary: Yes
- Located within the Primary Transit Area: Yes

Surrounding Land Uses:

- North: Residential & Neighbourhood Facility
- East: Residential
- South: Vacant
- West: Residential

Existing Planning Information:

- The London Plan Place Type: Neighbourhoods Place Type on a Civic Boulevard (Southdale Road East)
- Existing Special Policies: Southwest Area Secondary Plan – Medium Density Residential
- Existing Zoning: Residential R3 (R3-3) Zone



Figure 1- Aerial Photo of 325 Southdale Road East and surrounding lands.



Figure 2 - Streetview of 325 Southdale Road East (view looking south)

2.0 Discussion and Considerations

2.1 Development Proposal

In September 2024, the City accepted a complete Zoning By-law Amendment to permit a 3.5-storey, 10-unit, stacked townhouse block with a maximum density of 50 units per hectare. The proposed average unit size is approximately 120 m². A common outdoor amenity space is proposed to the rear of the building, connected to the internal sidewalk for convenient access for future residents. Vehicular access is provided from Southdale Road East, leading to the rear-yard parking area with a total of 11 spaces including one (1) Type A accessible parking space and one (1) visitor parking space. Pedestrian connections are provided with an internal sidewalk leading to entrances, amenity areas and public sidewalk.

The proposed development includes the following features:

- Land use: residential
- Form: stacked townhouse development
- Height: 3.5 storeys (11.3 metres)
- Residential units: 10 units
- Density: 50 units per hectare
- Gross floor area: 1,198 m²
- Building coverage: 15%
- Parking spaces: 11 surface parking spaces
- Bicycle parking spaces: 0
- Landscape open space: 49%
- Functional amenity space: 380m²

The conceptual site plans are shown below as Figure 3. Building renderings and elevations are shown in Figures 4 and 5 below.

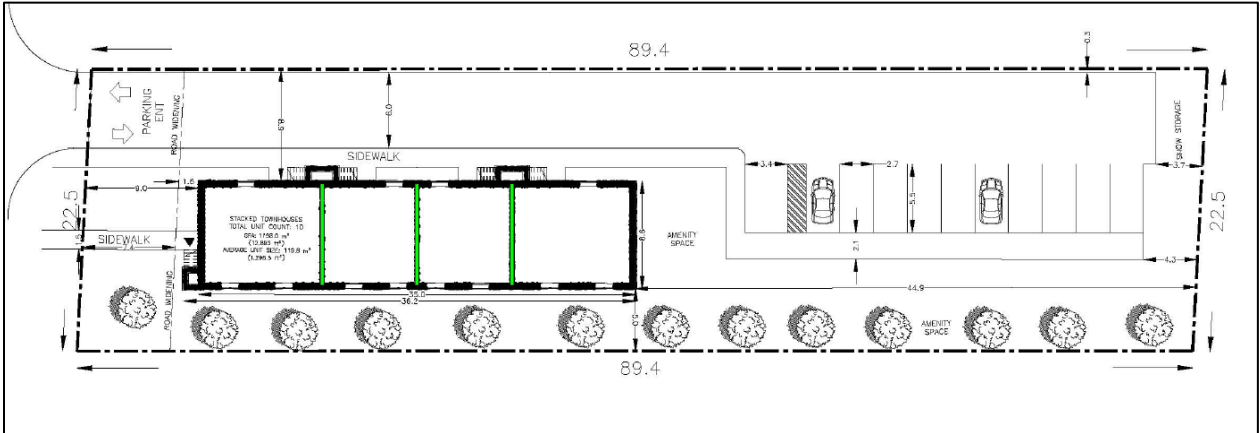


Figure 3 - Conceptual Site Plan (Received September 2024)



Figure 4 – Front Elevation (Received September 2024)

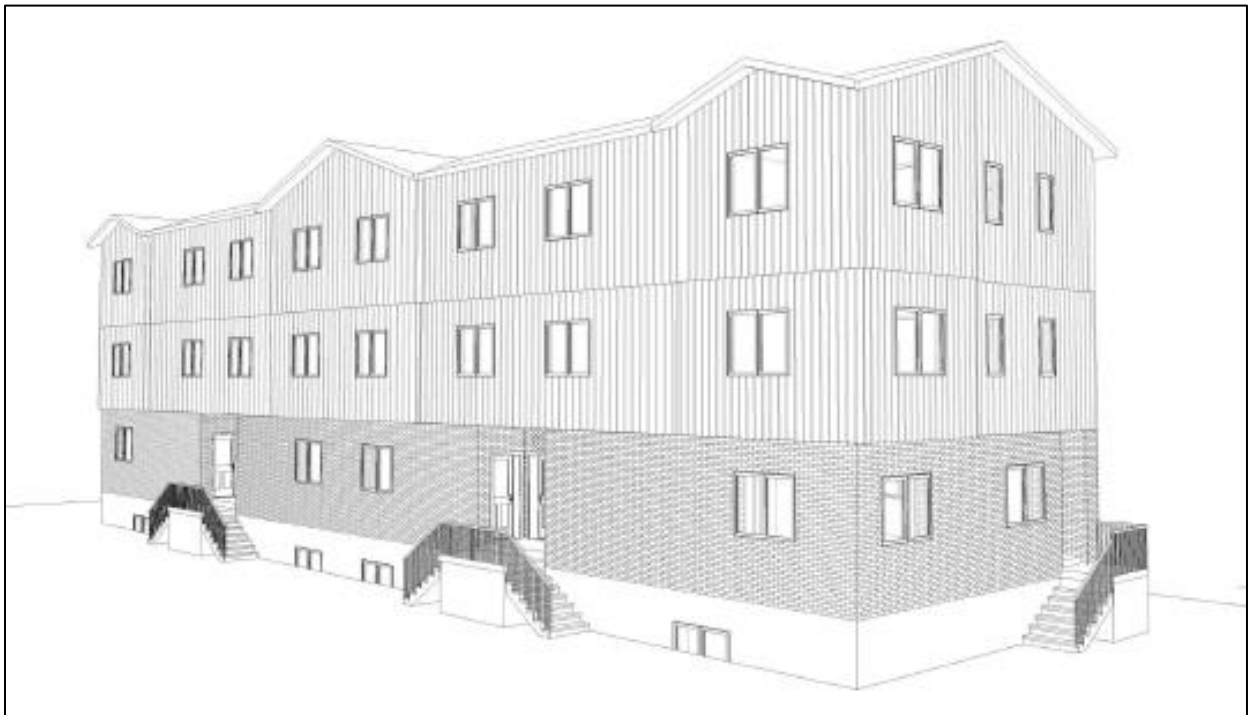


Figure 5 – Visualisation Proposed Development (Received September 2024)

Additional plans and drawings of the development proposal are provided in Appendix “B”.

2.2 Requested Amendment(s)

The applicant has requested an amendment to the Zoning Bylaw Z.-1 to rezone the property from a Residential R3-3 Zone to a Residential R5 Special Provision (R5-6) Zone.

The following table summarizes the special provisions that have been proposed by the applicant and those that are being recommended by staff.

Regulation (R5-6)	Required	Proposed	Recommended
Lot Frontage (Minimum)	30.0 metres	22.4 metres	22.4 metres
Front Yard Setback (Minimum) – from ultimate road-allowance	8.0 metres	1.6 metres	3.0 metres
Interior Side Yard Setback – West (Minimum)	5.6 metres	5.0 metres	5.0 metres

The following table summarizes the special provisions that are being recommended by staff:

Regulation (R5-6)	Required	Proposed	Recommended
Front Yard Setback (Minimum) – from front lot line existing on	8.0 metres	9.0 metres	10.0 metres

Regulation (R5-6)	Required	Proposed	Recommended
the date of passing of this by-law			
Front Yard Setback (Maximum) – from front lot line existing on the date of passing of this by-law	N/A	N/A	12.0 metres
Yards where Parking is Permitted	N/A	N/A	Parking shall not be permitted in the front yard

2.3 Internal and Agency Comments

The application and [associated materials](#) were circulated for internal comments and public agencies to review. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Key issues identified by staff and agencies included:

- As adequate servicing needs to be demonstrated, a general h-holding provision will be required.
- Provide a minimum front yard setback of 3m from the ultimate road widening of Southdale Road East.
- Provide a minimum drive aisle setback from the property line to the east for a landscape buffer and screening to mitigate potential negative impact on the adjacent property.

Detailed internal and agency comments are included in Appendix “D” of this report.

2.4 Public Engagement

On October 1, 2024, Notice of Application was sent to 214 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on October 10, 2024. A “Planning Application” sign was also placed on the site.

There were no responses received during the public consultation period.

2.5 Policy Context

The Planning Act and the Provincial Planning Statement, 2024

The Provincial planning policy framework is established through the *Planning Act* (Section 3) and the *Provincial Planning Statement, 2024 (PPS)*. The *Planning Act* requires that all municipal land use decisions affecting planning matters shall be consistent with the *PPS*. The mechanism for implementing Provincial policies is through the Official Plan, The London Plan. Through the preparation, adoption and subsequent Ontario Land Tribunal (OLT) approval of The London Plan, the City of London has established the local policy framework for the implementation of the Provincial planning policy framework. As such, matters of provincial interest are reviewed and discussed in The London Plan analysis below.

As the application for a Zoning By-law amendment complies with The London Plan, it is staff’s opinion that the application is consistent with the *Planning Act* and the *PPS*.

The London Plan, 2016

The London Plan (TLP) includes evaluation criteria for all planning and development applications with respect to use, intensity and form, as well as with consideration of the following (TLP 1577-1579):

1. Consistency with the Provincial Policy Statement and all applicable legislation.
2. Conformity with the Our City, Our Strategy, City Building, and Environmental

policies.

3. Conformity with the Place Type policies.
4. Consideration of applicable guideline documents.
5. The availability of municipal services.
6. Potential impacts on adjacent and nearby properties in the area and the degree to which such impacts can be managed and mitigated.
7. The degree to which the proposal fits within its existing and planned context.

Staff are of the opinion that all the above criteria have been satisfied.

Southwest Area Secondary Plan

The *Southwest Area Secondary Plan (SWAP)* has been reviewed in its entirety and it is staff's opinion that the proposed Zoning By-law amendment is consistent with it. The subject lands are designated Medium Density Residential pursuant to Schedule 13 (North Longwoods Residential Neighbourhood Land Use Designations) of the *SWAP*.

3.0 Financial Impact/Considerations

3.1 Financial Impact

There are no direct municipal financial expenditures associated with this application.

4.0 Key Issues and Considerations

4.1 Land Use

The proposed residential use is supported by the policies of the *PPS* and align with the vision of the Neighbourhoods Place Type by contributing to a diversity and mix of different housing types, intensities, and forms (TLP, 918_2). Further, the proposed residential use aligns with Key Direction #5 – Building a mixed-use compact city, by planning for infill and intensification of various types and forms to take advantage of existing services and facilities and to reduce our need to grow outward (TLP 59_4), and to ensure a mix of housing types within our neighbourhoods so that they are complete and support aging in place (TLP 59_5).

The Neighbourhoods Place Type contemplates a range of residential uses, dependent upon the street classification on which the property has frontage. As set out in Table 10 – Range of Permitted Uses in Neighbourhoods Place Type, the range of permitted uses along a Civic Boulevard include stacked townhouses (TLP Table 10). As such, the proposed stacked townhouse dwellings are a permitted use in conformity with the Neighbourhoods Place Type policies of The London Plan.

Southwest Area Secondary Plan

The subject site is located in the North Longwoods Residential Neighbourhood of *SWAP* and are designated Medium Density Residential (*SWAP* Schedule 13 – North Longwoods Residential Neighbourhood Land Use Designations). Permitted uses in the Medium Density Residential designation include multiple-attached dwellings, low-rise apartment buildings, rooming and boarding houses, emergency care facilities, converted dwellings, small-scale nursing homes, rest homes, and homes for the aged.

Staff are satisfied the proposed stacked townhouse development is supported by the policies of the *PPS 2024* and are a permitted use in the Medium Density Residential Designation of *SWAP* (11.1.ii).

4.2 Intensity

The proposed residential intensity is consistent with the policies of the *PPS 2024* that encourage all types of residential intensification, including redevelopment which results in a net increase in residential uses (*PPS* 2.2.1.2), compact form (*PPS* 2.4.1.3.c), and an appropriate mix of housing options and densities (*PPS* 2.3.1.3). Table 11 of The London Plan provides a range of permitted heights in the Neighbourhoods Place Type

based on street classification (TLP Table 11 – Range of Permitted Heights in Neighbourhoods Place Type). As the subject site has frontage on a Civic Boulevard, the minimum permitted height is 2 storeys, and the standard maximum height is 4 storeys.

The proposed residential intensity will facilitate an appropriate scale of development, that is compatible with the existing neighbourhood character and can be accommodated on a parcel that is of sufficient size to support their use. The redevelopment of the parcel will facilitate the efficient use of land, and is consistent with the residential intensification policies of The London Plan that encourage infill development on vacant or underutilized lots (TLP 939_5).

Southwest Area Secondary Plan

SWAP provides that the Medium Density Residential designation in the North Longwoods Neighbourhood shall have a maximum density of 75 units per hectare and building heights shall not exceed four storeys (SWAP 11.1.iii) a). The proposed Residential R5 (R5-6) Zone applies a maximum density of 50 units per hectare, which is within the 75 units per hectare contemplated by SWAP.

Based on the above, the proposed 3.5-storey stacked townhouse development is supported by the policies of the PPS 2024, contemplated in the Neighbourhoods Place Type in The London Plan, and in keeping with the intensity policies for the Medium Density Residential designation of the North Longwoods Neighbourhood in SWAP.

4.3 Form

The proposed built form on the subject site is supported by the policies of the PPS 2024 and is consistent with the Neighbourhoods Place Type policies and the City Design policies of The London Plan by facilitating an appropriate form and scale of residential intensification that is compatible with the existing neighbourhood character (TLP 953_2). Specifically, the proposed built form supports infill development of a vacant and underutilized lot, a mix of housing types to support ageing in place and affordability and is designed to be a good fit and compatible within the surrounding context (TLP 193).

The proposed built form consists of a 3.5-storey stacked townhouse development, oriented perpendicular to Southdale Road East. The proposed built form and massing of the stacked townhouse development have consideration for the surrounding land uses and is appropriate in scale compared to the surrounding neighbourhood character (TLP 953_2). Access to the subject lands will be provided through a two-way driveway off Southdale Road East, promoting connectivity and safe movement for pedestrians, cyclists, and motorists (TLP 255_). Parking for the proposed stacked townhouse development will be located behind the proposed development, visually screening the surface parking from the street, encouraging a pedestrian oriented streetscape (TLP 936_4).

The proposed stacked townhouse development is supported by the policies of the PPS 2024, contemplated in the Neighbourhoods Place Type, and in keeping with the City Design policies of The London Plan.

Southwest Area Secondary Plan

The Urban Design policies in Section 3.9 of SWAP provide that all development shall be designed in a form that is compact, pedestrian-oriented and transit friendly (SWAP 3.9.1a)). Further, building densities and land uses located along transit routes are to be designed to support transit and the users of those services, which is the case for the proposed stacked townhouse development located in proximity to a public transit stop. Based on the above, the proposed stacked townhouse development is in keeping with the Urban Design policies and the built form policies of the North Longwoods Neighbourhood of SWAP.

4.4 Zoning

The applicant has requested an amendment to Zoning By-law Z.-1 to rezone the subject site from a Residential R3 (R3-3) Zone to a Residential R5 Special Provision (R5-6() Zone. The applicant has requested the following special provisions as part of the application.

Frontage – The applicant is requesting a special provision to permit a frontage of 22.4 metres, whereas 30.0 metres would be the minimum frontage required for the R5-6 zone variation. The reduced frontage is an existing condition, and is sufficient to accommodate the vehicular and pedestrian access, appropriate setbacks and the development as a whole. Staff are supportive of the special provision for reduced frontage.

West Interior Side Yard Setback – The applicant is requesting a special provision to permit a west side yard setback of 5.0 metres, whereas 5.6 metres is the minimum required for the R5-6 zone variation in conjunction with the proposed building height of 11.3 metres. The reduced setback accommodates sufficient space to allow for privacy for abutting properties and future residents, landscaping and fencing. The abutting lot to the west (315-317 Southdale Road), contains several trees and vegetation along the lot line that provide natural privacy screening for the proposed development. Staff are supportive of the special provision for a reduced west interior side yard setback.

Staff are recommending the following special provisions as part of the application.

Front Yard Setback – The applicant is requesting a special provision to permit a reduced front yard setback of 1.6 metres from the ultimate road allowance, whereas 8.0 metres is the minimum front yard setback for the R5-6 zone variation. Since this development includes ten (10) residential units, Section 41.1.2 of *The Planning Act* provides that Site Plan Control will not be required for this development. As such, there is no mechanism to obtain a road-widening dedication, and the front yard depth will be measured from the front lot line as existing on the date of passing this by-law.

The proposed development is set back 9.0 metres from the existing front lot line, as shown by the red line in Figure 6 below. Staff are not supportive of the 9.0 metre setback, as it would not sufficiently reinforce the prevailing street wall with 331 Southdale Road to the east. Additionally, there is sufficient space at the rear of the site to accommodate a slightly larger front yard setback to better align with the existing development to the east. On this basis, staff are recommending a *minimum* front yard setback of 10.0 metres from the existing front lot line along Southdale Road East, and a *maximum* front yard setback of 12.0 metres from the existing front lot line to maintain the street line, as shown by the blue line in Figure 6 below.



Figure 6 – Aerial view street line 325 Southdale Road East

The recommended front yard setback special provisions are consistent with the City Design policies in *The London Plan* that buildings should be sited so that they maintain and reinforce the planned street line and reinforce the prevailing street wall of existing buildings (TLP 256_ & 259_).

Parking – Staff are recommending an additional special provision to ensure that parking shall not be permitted in the front yard. The proposed development provides all parking in the rear yard, in line with the City Building Policies of *The London Plan* to strategically locate and screen parking areas and locate surface parking in the rear yard (TLP 272_). The purpose of this special provision is to ensure all parking remains in the rear yard as currently proposed.

4.5 Holding Provision

Per Bill 23, the proposed development consisting of 10 residential units is not subject to Site Plan Control. As the applicant is required to demonstrate that adequate servicing can be provided for the proposed development, a general h-holding provision is being recommended. All servicing considerations shall be addressed at the servicing permit stage.

Conclusion

The applicant has requested an amendment to the Zoning By-law Z.-1 to rezone the property from a Residential R3 (R3-3) Zone to a Holding Residential R5 Special Provision (R5-6(_)) Zone. Staff are recommending approval of the requested Zoning By-law amendment with additional special provisions and a holding provision to ensure adequate servicing is available. Staff are recommending refusal of the reduced front yard setback of 1.6 metres.

The recommended action is consistent with the PPS 2024, conforms to The London Plan and will permit a 3.5-storey, 10-unit stacked townhouse development.

Prepared by: Isaac de Ceuster,
Planner, Planning Implementation

Reviewed by: Catherine Maton, MCIP, RPP
Manager, Planning Implementation

Recommended by: Heather McNeely, MCIP, RPP
Director, Planning and Development

Submitted by: Scott Mathers, MPA, P.Eng.
Deputy City Manager, Planning and Economic
Development

Copy:
Britt O'Hagan, Manager, Current Development
Mike Corby, Manager, Site Plans
Brent Lambert, Manager, Development Engineering

Appendix A – Zoning By-law Amendment

Bill No. (number to be inserted by Clerk's Office)
2024

By-law No. Z.-1-

A by-law to amend By-law No. Z.-1 to
rezone an area of land located at 325
Southdale Road East

WHEREAS this amendment to the Zoning By-law Z.-1 conforms to the Official Plan;

NOW THEREFORE the Municipal Council of The Corporation of the City of London enacts as follows:

1. Schedule "A" to By-law No. Z.-1 is amended by changing the zoning applicable to lands located at 325 Southdale Road East, as shown on the attached map from a Residential R3 (R3-3) Zone to a Holding Residential R5 Special Provision (h*R5-6(_)) Zone.

2. Section Number 9.4 of the Residential R5 Zone is amended by adding the following Special Provisions:

R5-6(_) 325 Southdale Road East

a. Regulations

- i) Lot Frontage (minimum) – 22.4 metres
 - ii) Front Yard Depth (minimum) – 10.0 metres as measured from front lot line existing on the date of passing this by-law
 - iii) Front Yard Depth (maximum) – 12.0 metres as measured from front lot line existing on the date of passing this by-law
 - iv) West Interior Side Yard Depth (minimum) – 5.0 metres
 - v) Parking shall not be permitted in the front yard.
3. This Amendment shall come into effect in accordance with Section 34 of the *Planning Act, R.S.O. 1990, c. P13*, either upon the date of the passage of this by-law or as otherwise provided by the said section.

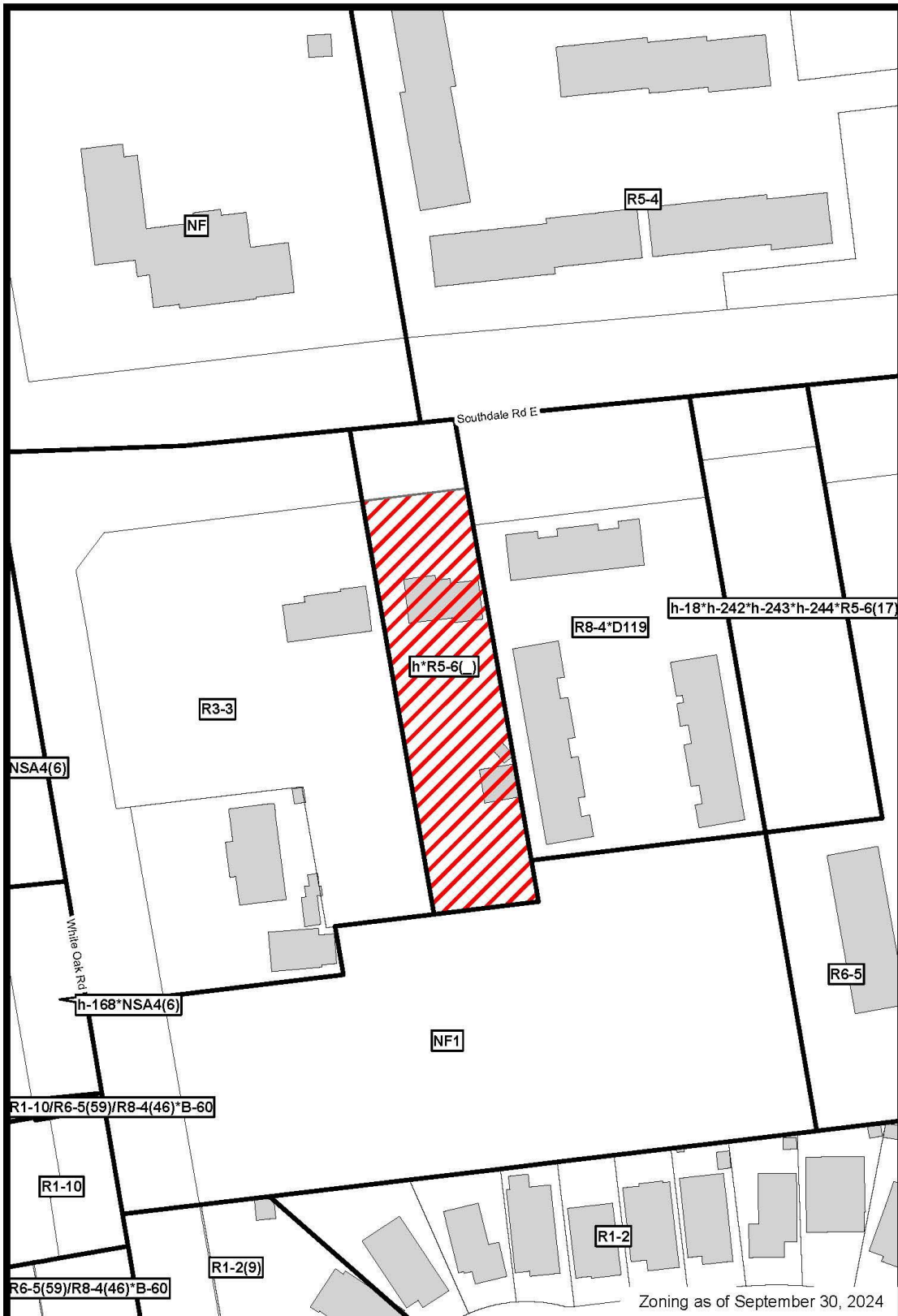
PASSED in Open Council on December 17, 2024 subject to the provisions of PART VI.1 of the *Municipal Act, 2001*.

Josh Morgan
Mayor


Michael Schulthess
City Clerk

First Reading – December 17, 2024
Second Reading – December 17, 2024
Third Reading – December 17, 2024

AMENDMENT TO SCHEDULE "A" (BY-LAW NO. Z.-1)




File Number: Z-9794
Planner: IDC
Date Prepared: 2024/10/25
Technician: JI
By-Law No: Z.-1-

SUBJECT SITE 

1:1,000

0 5 10 20 30 40 Meters



Appendix B - Site and Development Summary

A. Site Information and Context

Site Statistics

Current Land Use	Residential
Frontage	22.4 metres (73.4 feet)
Depth	89.3 metres (292.9 feet)
Area	0.19 hectares (0.49 acres)
Shape	Regular (rectangle)
Within Built Area Boundary	Yes
Within Primary Transit Area	Yes

Surrounding Land Uses

North	Residential & Neighbourhood Facility
East	Residential
South	Vacant
West	Residential

Proximity to Nearest Amenities

Major Intersection	White Oak Road & Southdale Road East, 70 metres
Dedicated cycling infrastructure	White Oak Road, 70 metres
London Transit stop	Southdale Road East, 40 metres
Public open space	Devon Park, 400 metres
Commercial area/use	White Oak Road & Southdale Road East, 70 metres
Food store	Berries Market, 450 metres
Community/recreation amenity	Earl Nichols Arena, 350 metres

B. Planning Information and Request

Current Planning Information

Current Place Type	Neighbourhoods Place Type on a Civic Boulevard (Southdale Rd E)
Current Special Policies	Southwest Area Secondary Plan – Medium Density Residential
Current Zoning	Residential R3 (R3-3) Zone

Requested Designation and Zone

Requested Place Type	N/A
Requested Special Policies	N/A
Requested Zoning	Residential R5 (R5-6)

Requested Special Provisions

Regulation (R5-6)	Required	Proposed
Lot Frontage (Minimum)	30.0 metres	22.4 metres
Front Yard Setback (Minimum) – from ultimate road allowance	8.0 metres	1.6 metres
Interior Side Yard Setback – West (Minimum)	5.6 metres	5.0 metres

C. Development Proposal Summary

Development Overview

A 3.5 storey, stacked townhouse development containing 10 residential units.
--

Proposal Statistics

Land use	Residential
Form	Stacked townhouses
Height	3.5 storeys (11.3 metres)
Residential units	10 units
Density	50 units per hectare
Gross floor area	1,198 m ²
Building coverage	15%
Landscape open space	49%
Functional amenity space	380 m ²
New use being added to the local community	Yes

Mobility

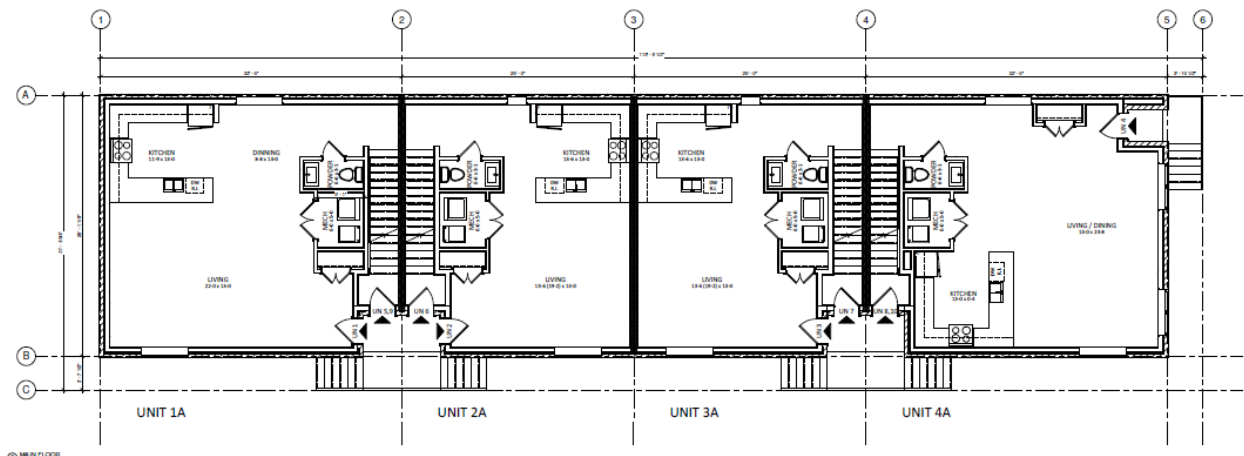
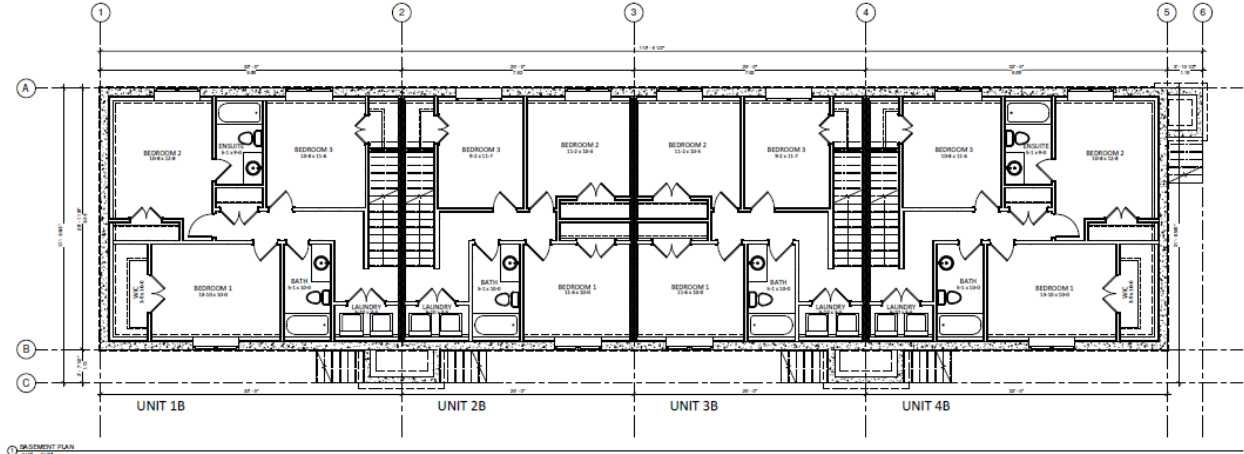
Parking spaces	11 surface parking spaces
Vehicle parking ratio	1.0 per unit (plus 1 visitor space)
New electric vehicles charging stations	0
Secured bike parking spaces	0
Secured bike parking ratio	N/A
Completes gaps in the public sidewalk	N/A
Connection from the site to a public sidewalk	Yes
Connection from the site to a multi-use path	N/A

Environment

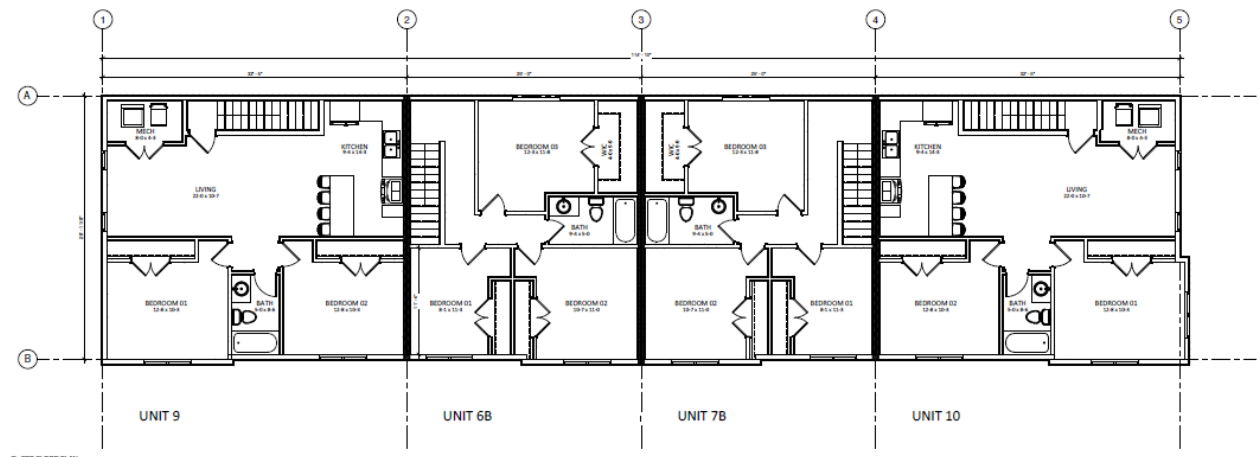
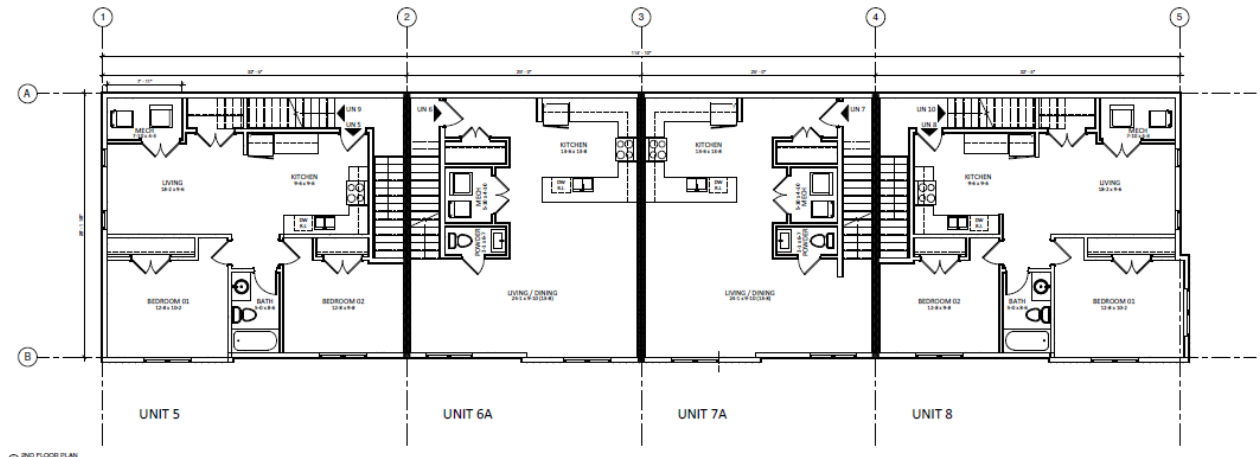
Tree removals	Approximately 6
Tree plantings	Unknown
Tree Protection Area	No
Loss of natural heritage features	N/A
Species at Risk Habitat loss	N/A
Minimum Environmental Management Guideline buffer met	N/A
Existing structures repurposed or reused	No
Green building features	Unknown

Appendix C – Additional Plans and Drawings

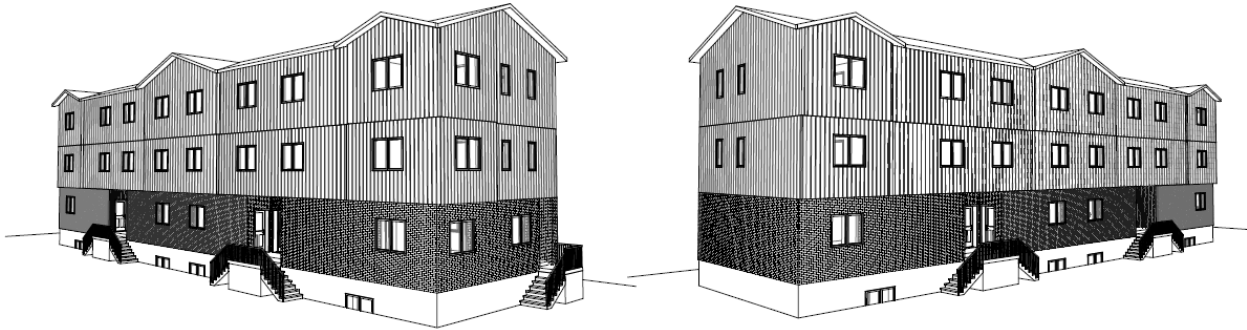
Proposed Development 325 Southdale Road East – Basement & Main Floor Plan



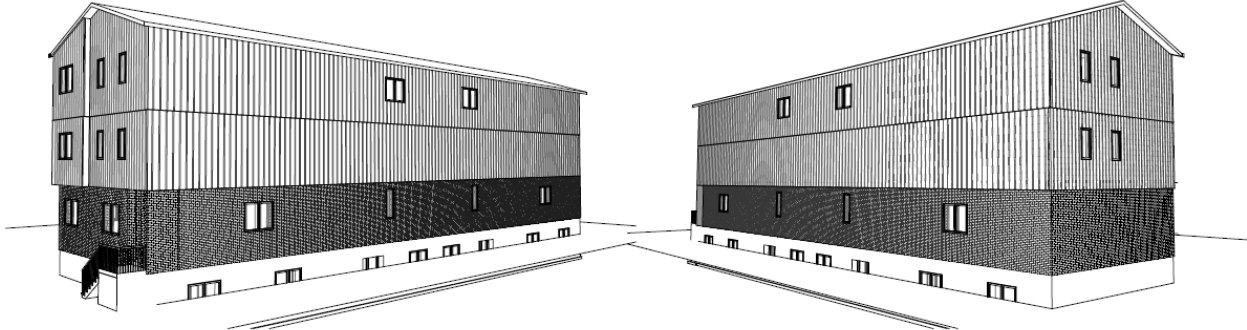
Proposed Development 325 Southdale Road East – 2nd & 3rd Floor Plan



Proposed Development 325 Southdale Road East – 3D Views



FRONT LEFT FRONT RIGHT



REAR LEFT REAR RIGHT

Zoning Excerpt Map 325 Southdale Road East



COUNCIL APPROVED ZONING FOR THE SUBJECT SITE: R3-3

1) **LEGEND FOR ZONING BY-LAW Z-1**

- | | |
|---|-----------------------------------|
| R1 - SINGLE DETACHED DWELLINGS | RF - REGIONAL FACILITY |
| R2 - SINGLE AND TWO UNIT DWELLINGS | CF - COMMUNITY FACILITY |
| R3 - SINGLE TO FOUR UNIT DWELLINGS | NF - NEIGHBOURHOOD FACILITY |
| R4 - STREET TOWNHOUSE | HER - HERITAGE |
| R5 - CLUSTER TOWNHOUSE | DC - DAY CARE |
| R6 - CLUSTER HOUSING ALL FORMS | OS - OPEN SPACE |
| R7 - SENIOR'S HOUSING | CR - COMMERCIAL RECREATION |
| R8 - MEDIUM DENSITY/LOW RISE APTS. | ER - ENVIRONMENTAL REVIEW |
| R9 - MEDIUM TO HIGH DENSITY APTS. | OB - OFFICE BUSINESS PARK |
| R10 - HIGH DENSITY APARTMENTS | LI - LIGHT INDUSTRIAL |
| R11 - LODGING HOUSE | GI - GENERAL INDUSTRIAL |
| DA - DOWNTOWN AREA | HI - HEAVY INDUSTRIAL |
| RSA - REGIONAL SHOPPING AREA | EX - RESOURCE EXTRACTIVE |
| CSA - COMMUNITY SHOPPING AREA | UR - URBAN RESERVE |
| NSA - NEIGHBOURHOOD SHOPPING AREA | AG - AGRICULTURAL |
| BDC - BUSINESS DISTRICT COMMERCIAL | AGC - AGRICULTURAL COMMERCIAL |
| AC - ARTERIAL COMMERCIAL | RRC - RURAL SETTLEMENT COMMERCIAL |
| HS - HIGHWAY SERVICE COMMERCIAL | TGS - TEMPORARY GARDEN SUITE |
| RSC - RESTRICTED SERVICE COMMERCIAL | RT - RAIL TRANSPORTATION |
| CC - CONVENIENCE COMMERCIAL | |
| SS - AUTOMOBILE SERVICE STATION | "h" - HOLDING SYMBOL |
| ASA - ASSOCIATED SHOPPING AREA COMMERCIAL | "D" - DENSITY SYMBOL |
| OR - OFFICE/RESIDENTIAL | "H" - HEIGHT SYMBOL |
| OC - OFFICE CONVERSION | "B" - BONUS SYMBOL |
| RO - RESTRICTED OFFICE | "T" - TEMPORARY USE SYMBOL |
| OF - OFFICE | |

CITY OF LONDON

PLANNING SERVICES / DEVELOPMENT SERVICES

**ZONING
BY-LAW NO. Z-1
SCHEDULE A**



THIS MAP IS AN UNOFFICIAL EXTRACT FROM THE ZONING BY-LAW WITH ADDED NOTATIONS

FILE NO:

Z-9794

IDC

MAP PREPARED:

2024/10/15

JL

1:1,500

0 5 10 20 30 40 Meters

The London Plan – Map 1 Place Types

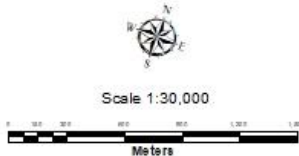


Legend

Downtown	Future Community Growth	Environmental Review
Transit Village	Heavy Industrial	Farmland
Shopping Area	Light Industrial	Rural Neighbourhood
Rapid Transit Corridor	Future Industrial Growth	Waste Management Resource Recovery Area
Urban Corridor	Commercial Industrial	Urban Growth Boundary
Main Street	Institutional	
Neighbourhood	Green Space	

This is an excerpt from Planning & Development's working consolidation of Map 1 - Place Types of the London Plan, with added notations.

CITY OF LONDON
Official Plan
LONDON PLAN MAP 1
- PLACE TYPES -
 PREPARED BY: Planning & Development



File Number: Z-9794
Planner: IDC
Technician: JI
Date: 2024/10/15

Appendix D – Internal and Agency Comments

Urban Design

Urban Design is generally supportive of the proposed development provided that the building achieves a high-quality, street-oriented design.

The proposed principal unit entrance and window openings on the north facade fronting Southdale Road East with the direct walkway connection to the public sidewalk are supported and should be carried forward.

Matters for Zoning

1. **Provide a minimum front yard setback of 3m from the ultimate road widening of Southdale Road East** to encourage street orientation and reinforce the existing/envisioned street line along the Civic Boulevard. TLP 256, 259
2. **Provide a minimum interior side yard setback** to accommodate access and maintenance and not hinder redevelopment of the adjacent property to the west. TLP 253, 252.
3. **Provide a minimum drive aisle setback from the property line to the east** for allowing a landscape buffer and screening to mitigate potential negative impact on the adjacent property. TLP 253
4. **Orient the built form to Southdale Road East with principal entrances and a similar number of windows as is found on the east elevation** to promote accessibility, wayfinding and offer passive surveillance. TLP 285, 286, 228 SWASP 20.5.3.9(iii)(d)

This application would not proceed through the Site Plan Approval process given the current unit count, however, **Urban Design recommends the following site design matters be considered:**

1. **Provide additional window openings on all floors of the rear façade** to offer passive surveillance on the outdoor amenity space to the south. TLP 228
2. **Prohibit any garbage storage area from being located along Southdale Road East frontage** to avoid any negative visual impact on the users or detracting from pedestrian connections. TLP 266

Site Plan

Site Plan is not required.

Heritage

Thanks for circulating this report and clearance letter to me. I have reviewed and can confirm that the archaeological requirements for Z-9794 have been met.

Ecology

This e-mail is to confirm that there are currently no ecological planning issues related to this property and/or associated study requirements.

Major issues identified

- None

Ecology – complete application requirements

- None

Parks Planning and Design

1. Major Issues
 - a. None.
2. Matters for OPA/ZBA
 - a. None.

3. Matters for Site Plan

- a. Parkland dedication has not been taken for this site. It is to be noted that the applicant, as a condition of building permit, will be required to provide parkland dedication in the form of cash-in-lieu pursuant to By-law CP-25.

Landscape Architecture

It is the property owner's responsibility to ensure that the new development does not result in adverse effects to neighboring properties. Staff recommends the property owner undertake a Tree Protection Plan (TPP) prior to any construction on the property and that any recommendation from the TPP are implemented prior to and during construction to protect trees. Should any tree fail within and outside of site and fall damaging property or injuring people this could become a civil matter between impacted property owners.

City Bylaws and Provincial Legislation pertaining to trees:

The City of London Tree Protection Bylaw protects trees with a diameter of 50+ cm growing on private property. Permits would be required to remove **on-site** trees with diameters +50cm at breast height. <https://london.ca/by-laws/consolidated-tree-protection-law> contact Forestry Dispatcher at trees@london.ca with details of your request. Any person who contravenes any provision of this By-law is guilty of an offence and if convicted is liable to a minimum fine of \$500.00 and a maximum fine of \$100,000.00.

The City of London Boulevard Tree Protection Bylaws protects **all trees located on City of London Boulevards** (including their root zones). To request the removal or to apply for consent to injure the roots of the City trees, contact Forestry Dispatcher at trees@london.ca with details of your request. Any person who contravenes any provision of this By-law is guilty of an offence and if convicted is liable to a minimum fine of \$500.00 and a maximum fine of \$100,000.00.

Boundary trees are protected by the province's Forestry Act 1998, c. 18, Sched. I, s. 21, boundary trees can't be removed without written consent from co-owner because any tree whose trunk is growing on the boundary between adjoining lands is the common property of the owners of the adjoining lands. Injury or removal of a boundary tree without consent is guilty of an offence under this Act. The legal definition of a tree trunk is everything from the root-collar (at the base) to where the first branch appears.

Engineering

- As per Bill 23, the proposed development is considered 10 units or less and is not subject to a siteplan application. As the applicant is required to demonstrate adequate servicing can provided for the proposed development, **a general holding provision will be requested as part of zoning approval**. All servicing considerations shall be addressed at the servicing permit stage.
- As part of a complete servicing permit application, the applicant will be required to provide the following:
 - Servicing Plans
 - Grading Plans
 - Restoration, Sediment & Erosion control plans
 - Stormwater Management Report and applicable hydrogeological studies
 - Sanitary Servicing Brief
 - Water Servicing Brief

The following items are to be addressed at the servicing permit stage:

Wastewater:

- The municipal sanitary sewer to service the site is the 200mm diameter sewer on Southdale Road East.
- City Plan No. 16795 shows related information to the municipal sewer and PDC(s).

- As part of a complete application, the applicant will be required to have their consulting engineer submit a sanitary servicing report, which will include how servicing would be provided, intended population build out and peak flow of the proposed development.

Water:

- Water is available for the subject site via the municipal 300mm high-level watermain on Southdale Road East.
- As part of a complete application, the applicant will be required to have their consulting engineer submit a water servicing report that details domestic and fire flows as well as water quality turnover.

Stormwater:

- As per attached 5892, the site is not tributary to the existing 250mm storm sewer on Southdale Road East which only conveys road runoff and the original design of the sewers did not account for this proposed development. Therefore, the consultant is to provide a SWM brief indicating how the site is proposed to be serviced (e.g. on-site controls, LID, etc.).
- As per attached as-con T18505-02, the site (at C=0.65) is tributary to the existing storm sewer system on White Oak Road. The consultant is to investigate options for conveyance of storm servicing to White Oak Road, noting that sewer extensions, servicing easements, and a sewer capacity analysis (design sheet) to demonstrate available capacity in the system (if connecting to 200mm storm sewer on White Oak Road) all may be required in support of the servicing strategy.
- Alternatively, as per the Drainage By-Law, section 5.2, where no storm sewer is accessible the applicant shall provide a dry well or storm water retention system which is certified by a Professional Engineer to the satisfaction of the City Engineer. Any proposed LID solution should be supported by a Geotechnical Report and/or hydrogeological investigations prepared with focus on the type of soil, its infiltration rate, hydraulic conductivity (under field saturated conditions), and seasonal high ground water elevation. The report(s) should include geotechnical and hydrogeological recommendations of any preferred/suitable LID solution and rationale about the following points:
- The owner shall be required to comply with the MECP and City's applicable Acts, Regulations, Standards, Specifications and Requirements including Drainage By-Law and acts (WM-4), to ensure that the post-development storm/drainage discharges from the subject lands will not cause any adverse effects to adjacent lands, all to the specifications of the City Engineer.
- The subject lands are located in the Dingman Subwatershed. The Owner shall provide a Storm/Drainage Servicing Report demonstrating compliance with the SWM criteria and environmental targets identified in the Dingman Subwatershed Study that may include but not be limited to, quantity/quality control (80% TSS), erosion, stream morphology, etc.
- So long as the stormwater servicing is provided via the White Oak Road storm sewer system, and the proposed development does not exceed 29 at grade parking spaces, quality control will not be required to be undertaken as it is provided by the downstream Cleardale Ravine.
- The Owner agrees to promote the implementation of SWM Best Management Practices (BMP's) within the plan, including Low Impact Development (LID) where possible, to the satisfaction of the City Engineer.
- The owner is required to provide a lot grading plan for stormwater flows and major overland flows on site, ensuring that stormwater flows are self-contained and that grading can safely convey up to the 250 year storm event, all to be designed by a Professional Engineer for review.
- The Owner shall allow for conveyance of overland flows from external drainage areas that naturally drain by topography through the subject lands.
- Stormwater run-off from the subject lands shall not cause any adverse effects to adjacent or downstream lands.

- An erosion/sediment control plan that will identify all erosion and sediment control measures for the subject site shall be prepared to the specification and satisfaction of the City Engineer and shall be in accordance with City of London and MECP (formerly MOECC) standards and requirements. This plan is to include measures to be used during all phases of construction. These measures shall be identified in the Storm/Drainage Servicing Report

London Hydro

- Servicing the above proposal should present no foreseeable problems. Any new and/or relocation of existing infrastructure will be at the applicant's expense, maintaining safe clearances from L.H. infrastructure is mandatory. A blanket easement will be required. **Note:** Transformation lead times are minimum 16 weeks. Contact Engineering Dept. to confirm requirements & availability.
- London Hydro has no objection to this proposal or possible official plan and/or zoning amendment. However, London Hydro will require a blanket easement.

Appendix E – Public Engagement

On October 1, 2024, Notice of Application was sent to 214 property owners and residents in the surrounding area. Notice of Application was also published in the *Public Notices and Bidding Opportunities* section of *The Londoner* on October 10, 2024. A “Planning Application” sign was also placed on the site.

There were no responses received during the public consultation period. Comments received were considered in the review of this application and are addressed in Section 4.0 of this report.

Nature of Liaison: 325 Southdale Road East – The purpose and effect of this Zoning amendment is to permit a 3.5-storey stacked townhouse development with a total of 10 residential units, and a total maximum density of 50 units per hectare. Possible change to Zoning By-law Z.-1 FROM a Residential R3 (R3-3) zone TO a Residential R5 Special Provision (R5-6(_)) Zone. Requested special provisions for front yard setback, lot frontage, and westerly interior side yard setback. The City may also contemplate alternative zoning such as a different base zone, additional permitted uses, additional special provisions and/or the use of holding provisions. File Z-9794. Planner: Isaac de Ceuster