



PLANNING & DESIGN REPORT

80 and 82 BASE LINE ROAD WEST, LONDON

PROPOSED ZONING BY-LAW AMENDMENT TO PERMIT AN 8-STOREY RESIDENTIAL BUILDING WITH 77 UNITS

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1 INTRODUCTION

Strik, Baldinelli, Moniz Ltd has been retained by 13759741 Canada Inc. (the “Applicant”) to prepare a Planning and Design Report in support of a Zoning By-law Amendment (ZBA) application for the property municipally identified as 80 and 80 Base Line Road West in southwest London (the “Subject Site”). The purpose of the following Planning and Design Report is to evaluate the proposed ZBA application within the context of existing land use policies and regulations, including the *Provincial Policy Statement (2020)*, *The London Plan* (the “Official Plan”), and the City of London Zoning By-law No. Z-1, as amended.

The Applicant is seeking to amend the Zoning By-law to permit an eight-storey apartment building with 77 dwelling units, 22 vehicle parking spaces, 80 long-term and eight (8) short-term bicycle parking spaces, a building footprint area of 594.2 square metres and a gross floor area of 4,585.5 square metres.

The intent of the development proposal is to replace the existing underutilized site with a contemporary apartment building that will add 77 new dwelling units to the local housing supply while leveraging its location on existing London Transit Commission bus routes within walking distance to the commercial amenities on Wharnccliffe Road South to the east and Commissioners Road West to the south.

2 PRE-APPLICATION CONSULTATION

As part of a site plan consultation process dated November 2, 2023, City of London staff advised that the scale, intensity, and zoning deficiencies associated with the initial proposal for 97 dwelling units warranted a ZBA application. Based on that feedback, a subsequent ZBA pre-application consultation meeting request was submitted and the Record of Consultation dated December 6, 2023 was received. Following those comments, changes were made to the proposal, including reducing the number of dwelling units from 97 to 77. Staff were again consulted, and comments were provided via a memo dated February 2, 2024. These three documents containing staff comments are being included in the submission package for reference.

3 SITE DESCRIPTION AND SURROUNDING CONTEXT

The subject site, hereafter referred to as the “Subject Site”, is a land assembly on the north side of Base Line Road West approximately 400 metres west of the Base Line Road West/Wharnccliffe Road South intersection in the Southcrest Planning District. It is comprised of two legal parcels municipally identified as 80 and 82 Base Line Road West, which can be described as follows based on City of London information:

Parcel 1 (80 Base Line Road West): PT LT 6, PL 29, PART 14 & 15, 33R9334; London/Westminster

Parcel 2 (82 Base Line Road West): PT LT 6, PL 29, PART 11 & 12, 33R9334; London/Westminster

The Subject Site is generally flat and regularly shaped with a total area of 2,024.7 square metres and a frontage of 45.7 metres with respect to Base Line Road West. 80 Base Line Road West is currently improved with a one-storey single detached house and 82 Base Line Road West is improved with a two-storey fourplex with a rear shed. Both parcels have existing driveways connecting to Base Line Road West. The Subject Site is located within the Urban Growth Boundary and municipal servicing is available. It is not regulated by the Upper Thames Regional Conservation Authority and is not located in a Tree Preservation Area. However, there are several existing trees, which are identified on the Tree Preservation Plan included in the submission package.

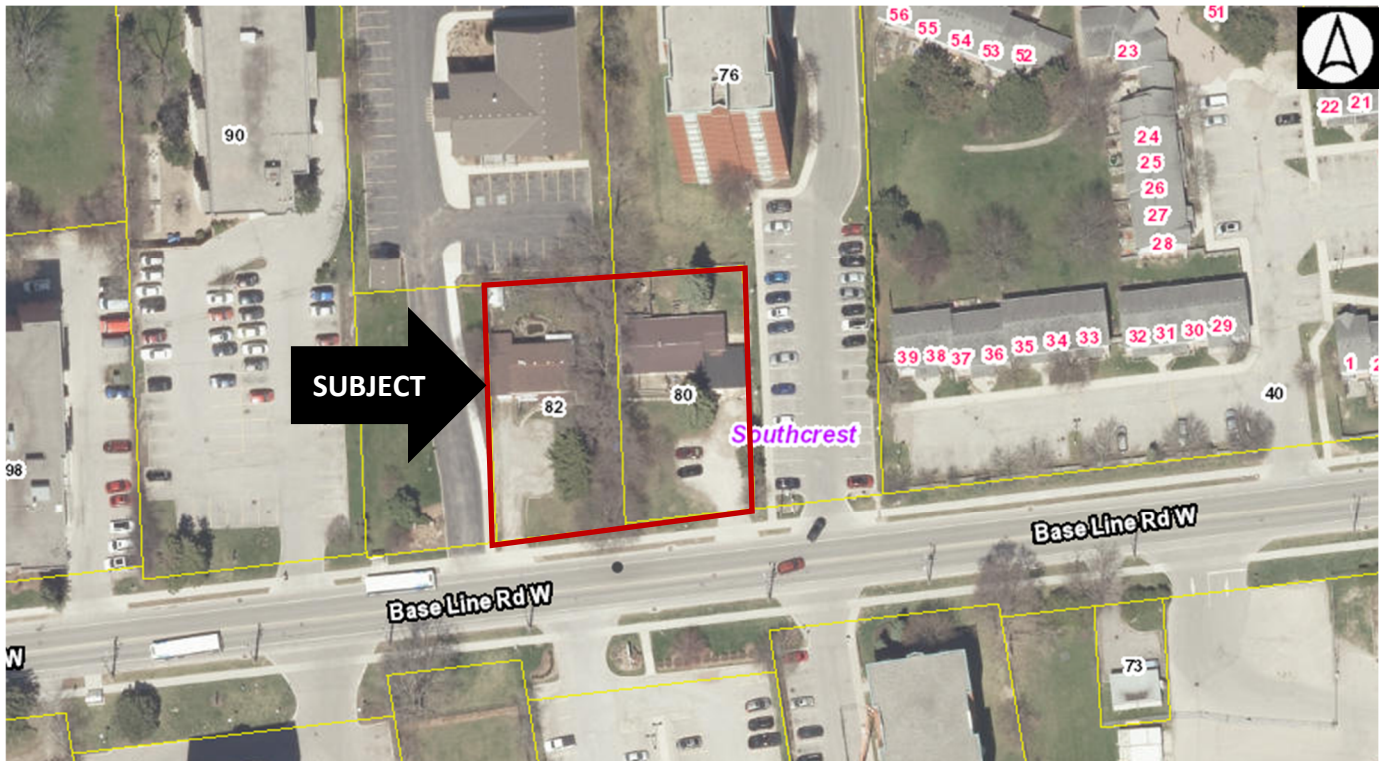


Figure 1. Aerial view of Subject Site (Source: London City Map)

A site visit was conducted on April 30, 2024 and the photos identified as **Figures 2, 3, 5 and 6** were taken.



Figure 2. View of western portion of Subject Site (82 Base Line Rd W)



Figure 3. View of eastern portion of Subject Site (80 Base Line Rd W) with 76 Base Line Rd W in the background

3.1 Surrounding Context

North: high-rise (11-storey) apartment building at 76 Base Line Road West; place of worship; Centre Street further north

South: across from Base Line Road West is a mid-rise apartment building; retirement residence

West: several apartment buildings of varying heights

East: townhouses; high-rise seniors apartment building at 30 Base Line Road West

4 SPATIAL ANALYSIS AND NEIGHBOURHOOD CHARACTER

4.1 Neighbourhood Spatial Analysis

The Subject Site is located in the southeast section of the Southcrest Planning District and the neighbourhood can be characterized as that area generally located within an 800-metre radius of the Subject Site. **Figure 4** shows land uses and notable features within 400 metre and 800 metre radii of the Subject Site, including the street classifications of Base Line Road West, Wharnccliffe Road South and Commissioners Road West per Map 3 of the Official Plan. The two radii represent walking distances of approximately 5 and 10 minutes, respectively.

The Subject Site is conveniently located close to many amenities within a 10-minute walking distance, including a major grocery store (Gary's NoFrills) at the commercial plaza located at the southeast corner of Base Line Road and Wharnccliffe Road South. Several parks within walking distance offer recreational opportunities and a public school – Arthur Ford Public School - providing kindergarten-grade 8 education is located 1.1-1.3 kilometres from the Subject Site, depending on the route and model of travel. It is expected that the existing dedicated bike lanes along Base Line Road West and Commissioners Road West will be extended and better connected in the future, as envisioned on Map 4 of the Official Plan. The largest employer in the neighbourhood is Enbridge Gas, which has an administrative office at 109 Commissioners Road West. Other commercial/retail employment opportunities exist within the businesses fronting the Wharnccliffe Road South and Commissioners Road West corridors.

For those trips not within walking or cycling distance, Base Line Road West, which is classified as a Neighbourhood Connector on Map 3 of the Official Plan, has one vehicle lane in each direction along with dedicated bicycle lanes and sidewalks on both sides of the street. There are several London Transit Commission (LTC) transit stops within walking distance of the Subject Site. There are two bus stops west of the Subject Site on Base Line Road West - Route 15 provides regularly scheduled Monday-Friday service offering connections to downtown and Westmount Shopping Centre while Route 56 is a "community" route offering connections to grocery stores (e.g., Food Basics and Giant Tiger) and White Oaks Mall. Commissioners Road West is classified as a Civic Boulevard on Map 3 of the Official Plan and is approximately 600 m from the Subject Site and offers Transit Route 12 – Downtown Wharnccliffe at Wonderland, and Route 24 – Talbot Village-Summerside. Finally, Wharnccliffe Road West, approximately 500 east of the Subject Site,

is also classified as a Civic Boulevard – LTC offers Route 12 and Route 93 – White Oaks Mall – Masonville, including a direct connection along Western Road to Western University campus buildings.

Figure 4. Existing Context Regional Space Analysis (400 m and 800 m) (Source: Google Maps)



As **Figure 4** also illustrates, the stretch of Base Line Road West close to the Subject Site is a medium to high density residential area comprised of existing townhouse complexes and multi-residential buildings ranging from four storeys to 11 storeys in height. Examples of existing high-rise buildings include the 11-storey building at the rear of the Subject Site (see **Figure 5**) and the newer 10-storey building on the south side of Base Line Road West municipally identified as 101 Base Line Road West (see **Figure 6**). The site municipally identified as 129-131 Base Line Road has also received zoning approval for a 14-storey, 176-unit apartment building in June 2023. These developments reflect that this stretch of Base Line Road West, as well as a section of Commissioners Road West west of Wharnccliffe Road South, is subject to the “High Density Residential Overlay (from the 1989 Official Plan)” identified on Map 2 of the Official Plan, which acknowledges properties previously zoned for high density (see **Figure 7**). Please see section 7 of this report for a detailed planning framework analysis.



Figure 5. Example of older existing high-density residential (76 Base Line Rd W)



Figure 6. Example of newer existing high-density residential (101 Base Line Rd W)

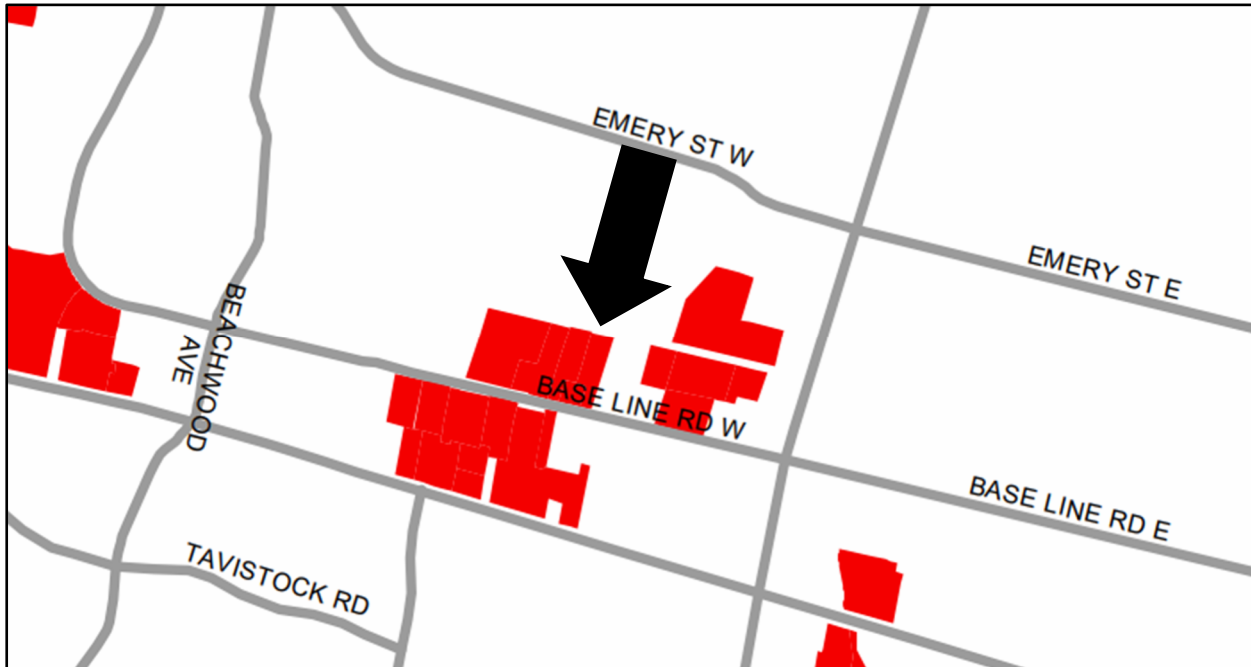


Figure 7. Extract of High Density Residential Overlay (from 1989 Official Plan) (Source: Map 2, *The London Plan*)

4.2 Site-Specific Spatial Analysis

The two existing houses represent an underutilization of the Subject Site within the existing high-density residential context. This provides an opportunity for redevelopment and intensification. Site-specific development considerations are illustrated in **Figure 8** below. The only public interface is Base Line Road West to the south of the Subject Site. The south side of Base Line Road West opposite the Subject Site features existing high-rise and mid-rise residential buildings. The northwest property limit of the Subject Site interfaces with a one-storey institutional building (i.e., church) and its surface parking lot, which is accessed from Base Line Road West. There is a separation distance of approximately 22 metres between the northern limit of the Subject Site and the church building. The northeast portion of the Subject Site interfaces with an 11-storey high-rise residential building oriented perpendicularly to Base Line Road West with surface parking located to the north and south of this building – access to this property is also being provided from Base Line Road West. Located on a deep through lot, this building has a large front yard depth of approximately 47 metres with respect to Base Line Road West.

The western property limit of the Subject Site directly interfaces with the driveway of the church building noted above – further to the west in a straight-line direction is the parking lot associated with the property municipally identified as 90 Base Line Road West, which has a low-rise apartment building set back even further from Base Line Road West at approximately 62 metres. Assuming a straight-line west direction, the closest apartment building to the western limits of the Subject Site is located 76 metres away at 98 Base Line Road West.

Finally, the eastern property limit of the Subject Site directly abuts the driveway providing access to the 11-storey apartment building noted above and municipally identified as 76 Base Line Road West. Further to the east is a two-storey condominium townhouse block parallel to Base Line Road West fronting an internal drive aisle and parking area. As noted on the drawing titled “Cross Sections” prepared by SBM Ltd., there is a 30.18 metre separation distance between the eastern property line of the Subject Site and the existing townhouse block.

Considering the above description, the most important site design considerations for any high-rise development on the Subject Site are lot characteristics combined with the proximity to the existing 11-storey building to the north, which is located approximately 14 metres from the northeast property limit. This distance, combined with the narrow depth of the Subject Site, means that any proposed high-rise building must be oriented parallel to Base Line Road West with a limited front yard depth. This contrasts with many of the surrounding lots fronting Base Line Road West, which are deep and essentially demand perpendicular building orientations to maximize the development potential of these lots. This type of perpendicular building orientation in relation to Base Line Road West has also resulted in some lots with large front yard depths with surface parking in the front yard and clearly visible from the street, which does not reflect contemporary urban design standards.

Adequate building separation is important to maximize daylight and minimize shadowing and overlook. The surrounding predominant medium and high-density development, particularly west and south of the Subject Site, should minimize any perceived adverse impacts of the proposed high-rise building on existing development. The existing low-rise church is not considered a sensitive use and should not require the separation and buffering typically expected to protect low-rise residential development. With respect to the eastern interface, some units (e.g. # 56) within the existing townhouse complex are already located within 15 metres of the existing high-rise building at 76 Base Line Road West. Section 5 of this report will provide more information about the relationship between the proposed building and existing development, including summarizing the results of the required shadow study.

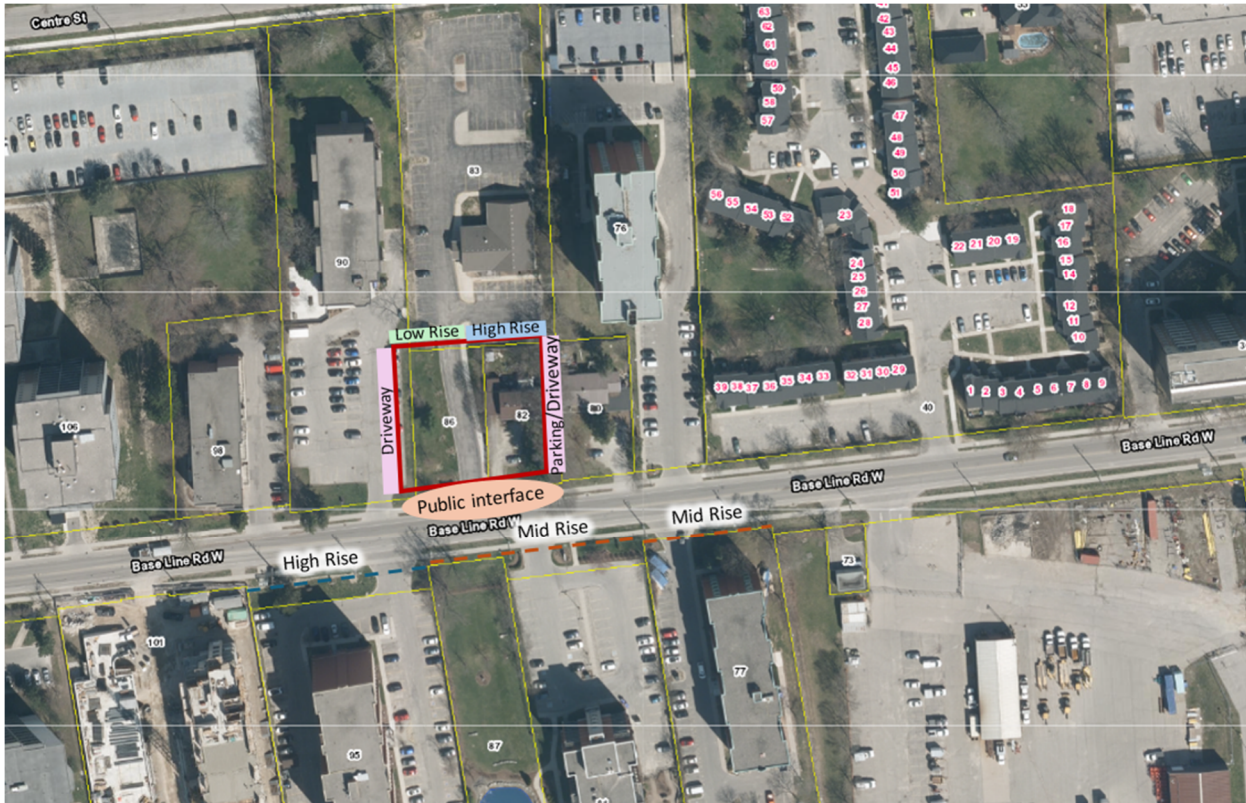


Figure 8. Development Site-Specific Spatial Analysis (Source: City of London)

5 DEVELOPMENT PROPOSAL

5.1 Development Concept

The ZBA application submission package includes an architectural drawing set stamped by Patrick David Trottier, OAA, comprised of ten drawings: south-east perspective view (A001); south-west perspective view (A002); site plan & zoning chart (SP1); site plan details (SP2); level 1 floor plan (A100); typical floor plan (A101); south elevation (A400); north elevation (A401); east elevation (A402); west elevation (A403).

As illustrated on the site plan drawing, the two existing buildings on the Subject Site are proposed to be demolished and replaced with an eight-storey (30 metre high), 77-unit residential building fronting onto Base Line Road West with a building footprint area of 594.2 square metres, a gross floor area of 4,585.5 square metres and a density of 386 units per hectare. Surface vehicle parking would be provided at the rear of the building in the form of 17 'standard' spaces, three (3) visitor parking spaces and two (2) barrier-free spaces for a total of 22 vehicle parking spaces. Cycling is promoted by the provision of 72 long-term bicycle spaces to be stored on the ground floor and eight (8) short-term spaces to be stored in a bicycle rack located just west of the principal building entrance. The rear surface parking lot would be accessed from Base Line Road West using a partial breezeway located on the eastern portion of the Subject Site that includes a two-way

driveway designed to accommodate both paratransit and general loading requirements. A walkway with a minimum width of 1.5 metres surrounds the building and connects to the existing sidewalk on Base Line Road West. The building entrances have been strategically located to allow for convenient pedestrian access to the building, including a primary entrance located at the centre of the building. A secondary access point is proposed under the breezeway to allow for covered loading space and ease of access from the building to the parking spaces at the rear of the Subject Site.

Some of the dwelling units (i.e., the 1-bedroom and 2-bedroom units) would have private balconies, as illustrated on the south and north elevation drawings. However, all building residents would have convenient access to both common outdoor and indoor amenity spaces. A common outdoor amenity space with an area of 79.6 square metres intended as a social outdoor seating area is proposed west of the proposed building. Indoor common amenity space is also proposed on the ground floor in the form of a community room with an area of 92 square metres and a fitness room with an area of 40.5 square metres.

Due to the narrow lot depth and high-rise building to the rear, the building is proposed to be located close to the street at a minimum front yard depth of 0.96 metres with respect to the new property line after the required 1.96 metre road widening. This will ensure a maximum building to building separation distance of 36.80 metres, as illustrated on the cross-sections drawing. As the proposed building is not exactly parallel to the property line and Base Line Road West, it appears staggered with several different front yard setbacks with the largest front yard setback occurring on the western side (i.e., 3.12 metres). The minimum rear yard depth is proposed to be 21.31 metres, the west interior side yard depth is proposed to be 6.37 metres and the east interior side yard depth would be 4.97 metres. The proposed development will be referred to as the “Development Proposal” hereafter.

5.2 Design Considerations

Conceptual Design Elements

Site Design

- The Subject Site, with its proximity to transit stops and amenities in an established residential area within the urban growth boundary, is an appropriate location for the Development Proposal. Increasing density on underutilized sites within the urban growth boundary is a tenet of contemporary planning.
- The Development Proposal efficiently uses the Subject Site and maintains an appropriate building to building separation distance of 36.8 metres from the existing 11-storey apartment building to the north. As noted on the drawing entitled “Perspective View (Looking North from Base Line Road)”, there would also be a 6.46 metre separation distance between the proposed building and the existing 1-storey church to the northwest. The outdoor amenity area also serves as a transition/buffer

between the proposed building and the existing church building. On the east side, there would be a large 30.18 metre building to building distance to the existing two-storey townhouse complex.

- The proposed mid-rise building frames Base Line Road West (i.e., its long axis is parallel to the streetscape) to allow for direct and convenient access from the public sidewalk to the principal building entrance and it is located as close to the street as possible while avoiding encroachments of footings and canopies. Parking would be located at the rear screened by the building and accessed by a partial breezeway to further enhance the public realm. The building orientation in relation to the public street, limited setback from the street, and rear parking reflects contemporary urban design standards.
- Pedestrian circulation is promoted via a minimum 1.5 metre walkway that connects to Base Line Road West and surrounds the building.
- Bicycle parking spaces are proposed inside and outside the building to encourage cycling.
- Accessibility/barrier-free access is promoted by the provision of three barrier-free parking spaces in the rear parking lot and a driveway that accommodates paratransit vehicles.
- A variety of private and shared amenity spaces are provided inside and outside the building. Common/shared amenity areas include the outdoor space located west of the building, as well as the indoor areas identified on the level 1 floor plan drawing. Furthermore, balconies serve as private amenity areas, as illustrated on the elevation drawings.

Built Form, Massing and Articulation

- The proposed building would be considered a tall mid-rise with a maximum height of 30 metres, which is lower than some of the existing surrounding high-rise buildings, including the 11-storey building directly to the north and significantly lower than the 45 metres permitted by the existing zoning.
- The partial breezeway is a unique feature that has several functions, including breaking up the massing of the building while providing access to the rear parking lot and supporting residential units above.
- The building uses a variety of building materials to address massing, as noted in more detail below. Articulation is provided through architectural elements such as recessed balconies, a glazed curtain wall on the east side above the breezeway, and a recessed vertical section just west of the breezeway identified on the site plan as the stairwell exit.
- The proposed building is divided into three distinct areas:

i) **Base:** the base consists of the ground floor and breezeway which is proposed to be treated with a natural stone finish in a dark colour. The dark colour would have the effect of ‘grounding’ the building while the natural stone would soften/add warmth while drawing attention to the principal building entrance. Strategic plantings would also be used on this level to soften the building façade. The ground floor also features extensive glazing to draw attention to the active uses on that floor and enhance the streetscape.

ii) **Middle:** this central area corresponding to levels 2-6 is proposed to be treated with a dark vertical siding material with white horizontal accents in the recessed balcony areas. The dark colour siding ties into the dark material of the base while using a lighter material reduces the impact on the pedestrian realm. For example, as the renderings indicate, the lighter colour starting at level 6 gives the impression that the building has a step-back at that level. However, no actual step-back is proposed.

iii) **Top:** for levels 7-9 the dark vertical treatment was replaced with a lighter colour to reduce the perceived impact of the building on the streetscape. The lighter colour also gives the impression the building is receding into the skyline.

- Residential space proposed above the base and breezeway is proposed to include 77 residential units to be allocated among studio, 1-bedroom and 2-bedroom units.
- A shadow impact study conducted by SBM Ltd. determined that while there will be some shadow impacts on adjacent properties and the Base Line Road West corridor, all shadow impacts are consistent with the guidelines developed by the City of Waterloo, which are widely accepted by other municipalities and were used as a benchmark since the City of London does not have its own guidelines.
- Building entrances have been strategically located to enhance the pedestrian experience, including a primary entrance at the centre of the building fronting Base Line Road West, and a secondary access under the breezeway to provide a covered loading space. The secondary access, which is also accessible, is connected to the rear parking lot through a wide 3.0-metre-wide walkway.
- Active uses are proposed on the ground floor to create a pedestrian-oriented streetscape and further activate the street. The outdoor shared amenity space to the west provides recreational space while providing for a larger west side yard depth/setback of 6.37 metres.

Character and Image

- The Development Proposal features a functional, contemporary mid-rise building that is compatible with the surrounding neighbourhood with its eclectic mix of low, medium and high-rise residential development within walking distance of amenities and public transit stops.
- Although the Base Line Road West corridor does not have a consistent character and instead features a variety of older and newer buildings reflecting the design standards of their time, the proposed building reflects contemporary urban design standards.

Architectural Treatment

- The proposed building materials and colours are identified on the elevation drawing and will be further refined at the site plan control stage.

Servicing

- All vehicle parking is proposed to be located at the rear of the Subject Site behind the proposed building.
- Long-term bicycle parking is provided in the interior of the building, as indicated on the level 1 floor plan; short-term bicycle parking is proposed to be located behind the landscape feature wall west of the principal building entrance.
- General loading space and paratransit vehicle drop off/pick up location would occur in the same space, as indicated on the site plan.
- Waste management bins are proposed to be stored inside the building on the ground floor, as indicated on the level 1 floor plan, and are to be brought out when required for pick-up, as indicated on the site plan.

6 SUPPORTING STUDIES

The conclusions of the studies/reports requested by City of London staff and identified in the ZBA Record are summarized below – please refer to the original reports included in the ZBA submission package for complete details.

6.1 Servicing Feasibility Study

SBM Ltd. completed a Servicing Feasibility Study dated June 3, 2024 that evaluated the servicing feasibility for the Development Proposal. The report concludes that the existing sewers and watermains have the capacity required to accommodate the anticipated servicing demands. In terms of stormwater management, quantity controls would be implemented to restrict post-development flows to the pre-development rate.

Existing overland flows appear to be generally directed to the south towards Base Line Road West. Stormwater quality controls would also be implemented during detailed design for site plan approval.

6.2 Parking Justification Report

Paradigm Transportation Solutions Limited completed the required Parking Justification Report dated May 9, 2024 to evaluate the vehicle parking associated with the Development Proposal – 22 spaces are proposed while 39 are required. The report justifies the proposed parking by reviewing site-specific characteristics and proposing to reduce demand through Transportation Demand Management (TDM) measures.

6.3 Tree Preservation Plan

A Tree Preservation Plan (TPP) stamped May 29, 2024 was completed by landscape architect Dan Weagant, OALA, CSLA. While existing onsite trees are required to be removed to facilitate the Development Proposal, a landscape plan and appropriate compensation would be provided through the site plan approval process.

7 PLANNING FRAMEWORK

7.1 Provincial Policy Statement, 2020 (PPS)

The Provincial Policy Statement (PPS), issued under section 3 of the Planning Act, provides policy direction on matters of Provincial interest related to land use planning and development to ensure efficient and cost-effective development patterns, healthy communities, and the protection of resources. Any land use planning decision shall be consistent with the (PPS). The Development Proposal is consistent with the PPS and more specifically supports the following policies:

- The Subject Site is in an established residential neighbourhood within the City of London urban growth boundary. It makes efficient use of land and the available municipal services in accordance with policies 1.1.3.1, 1.1.3.2 and 1.1.3.3.
- The Development Proposal would redevelop and appropriately intensify the Subject Site by adding 77 apartment dwelling units to the local housing supply while taking advantage of existing municipal infrastructure, in accordance with policies 1.1.1 a), b), c), d), e), 1.6.6.2.
- The proposed dwelling units would also assist the City of London in its role as a planning authority in conforming with Section 1.4 of the PPS, especially policies 1.4.1 and 1.4.3.

In my opinion, the proposed ZBA is consistent with the policies of the PPS.

7.2 Official Plan

The Subject Site is designated as a Neighbourhoods Place Type fronting a Neighbourhood Connector on Map 1 of the Official Plan”) (see **Figure 9** below). It is also identified on Map 2 as a “High Density Residential Overlay” based on the 1989 Official Plan (see **Figure 10** below) so this specific designation supersedes the typical Neighbourhoods Place Type height/storey permissions. Table 8 states that the standard maximum (outside of the Primary Transit Area or PTA) is 12 while the maximum (inside the PTA) is 14 storeys. The Subject Site is located within the PTA and the proposed height of 8 storeys would conform to the maximum. Policy 955_ lists a set of policies for development within the High Density Residential Overlay area. In my opinion, the Development Proposal conforms to these policies.

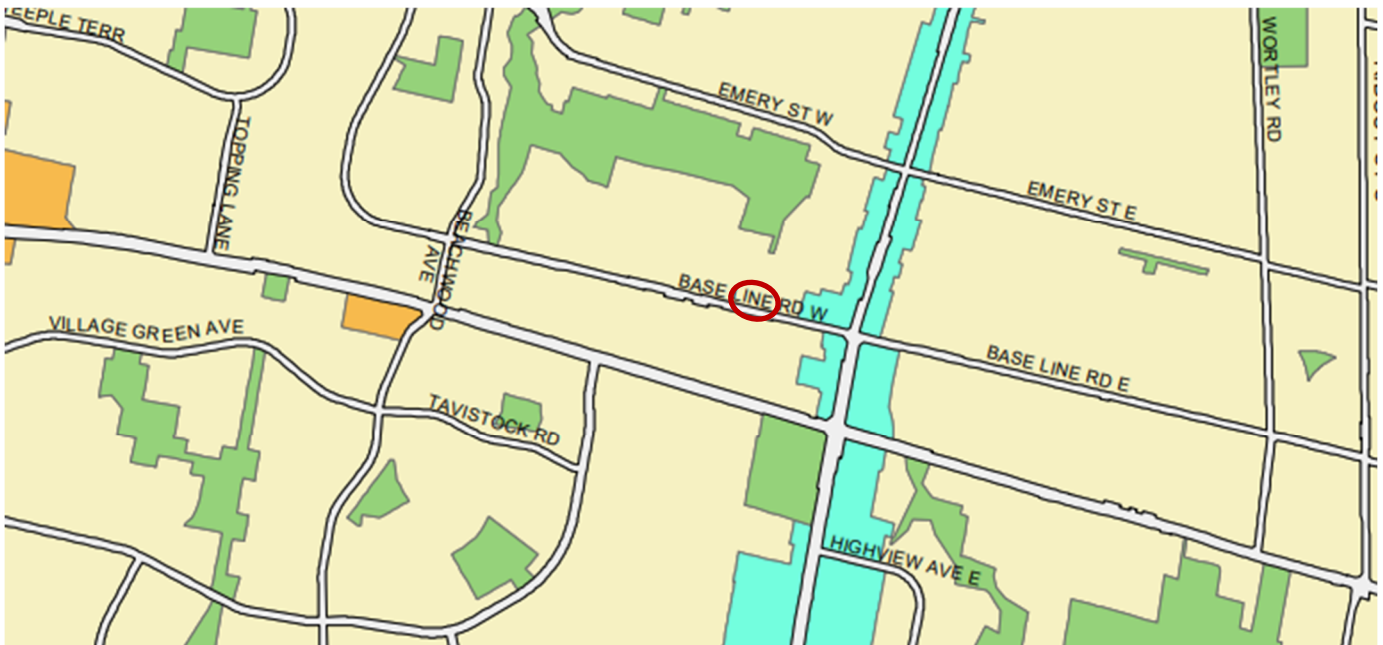


Figure 9. Subject Site (location approximate) land use designation - Neighbourhoods Place Type fronting a Neighbourhood Connector (Source: Official Plan, Map 1)

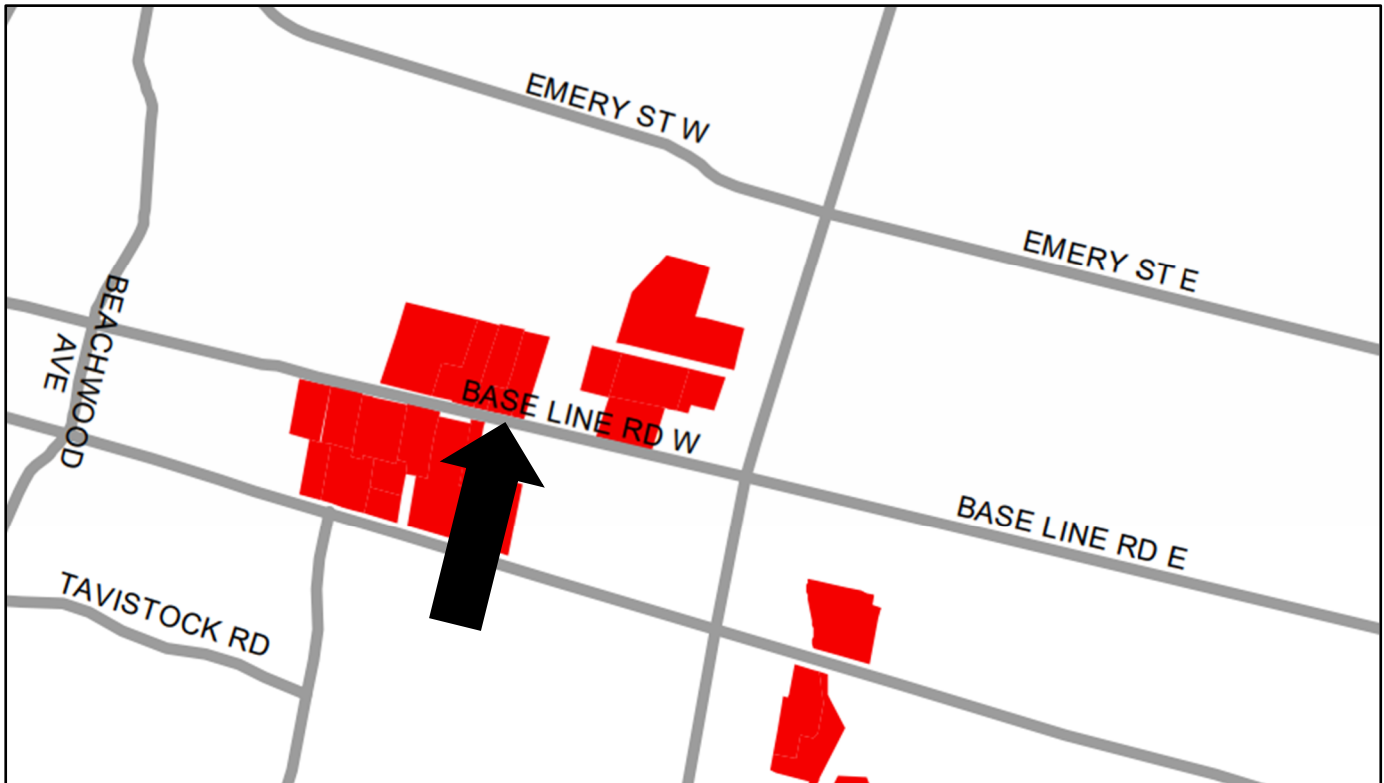


Figure 10. Subject Site – High Density Residential Overlay (from 1989 Official Plan) (Source: Map 2)

Neighbourhoods Place Type Policies

The Neighbourhoods Place Type has policies with respect to use, intensity, and form. The intensity policies are outlined in policy 935 and, in my opinion, the Development Proposal conforms to these policies in light of the existing zoning permissions, which will be discussed below. Policy 936 addresses form within the Neighbourhoods Place Type. With respect to policy 936_4, although the Development Proposal does not feature a mixed-use building, it includes onsite surface parking at the rear of the Subject Site screened behind the building to reduce its visual impact since underground parking is not financially feasible for this project. Furthermore, several other developments on the street feature surface parking so it is characteristic of the Base Line Road West corridor. The only other relevant policy in this section is 936_1, which requests conformity with the City Design or urban design policies of the Official Plan. Further to the design analysis already provided in section 5.2, the section below summarizes how the Development Proposal addresses the City Design policies embedded within the following categories.

Character

The Baseline Road West corridor is characterized by low-density residential development to the west and a mix of medium and high-density residential development in closer proximity to the Subject Site. Although many London residential neighbourhoods have a consistent density of primarily low-density houses perhaps

interspersed with medium density townhouse blocks, this corridor implements the 1989 Official Plan residential overlay and incorporates high-density development. The Development Proposal is an example of infill development and redevelopment that would be compatible with and fit within this existing context. High-density development is also appropriate in a neighbourhood with access to public transit and amenities within walking distance, which this neighbourhood provides. The sidewalks, public transit stops, and some cycling infrastructure promote connectivity to the surrounding area and to the rest of the city.

Street Network

The Development Proposal would have no adverse impact on the existing street network and would not create any new streets. The internal walkway would connect to the existing public sidewalk. Note that although a road widening of 1.442 metres was requested through the SPC consultation process to achieve 11.5 metres from the centreline of Base Line Road West, a site-specific survey identified that a road widening of 1.96 metres is actually required to achieve the desired result.

Streetscapes

Base Line Road West is characterized as a Neighbourhood Connector on Map 3 of the Official Plan, which envisions low to medium volumes of bicycle, transit and vehicle movements and is prioritized for pedestrians. There are no physical changes proposed to the existing Base Line Road West ROW with this application.

Changes are proposed to the Subject Site to facilitate the Development Proposal, as illustrated on the Site Plan, including a new two-way driveway connecting the street to the rear parking lot, as well as wide walkway with a minimum width of 1.5 metres surrounding the building to enable convenient pedestrian circulation. In conformity with policy 228_, the minimum front yard depth of the proposed building and abutting amenity space to the west encourage passive surveillance of the street. The front façade of the building would also be softened with plantings and include a landscape feature wall with a limited height of 1.2 metres to accommodate short-term bicycle parking near the front/primary door. Landscape details will be provided at the site plan control stage with the submission of a landscape plan.

Public Space

The Development Proposal is not proposing any new public spaces, however, a combination of common and individual private amenity spaces are proposed for building residents, as illustrated on the architectural drawings.

Site Layout and Parking

Please review the “Site Design” portion of section 5.2 of this report. In my opinion, the Development Proposal conforms to the relevant policies of these City Design policies. With respect to policy 256_, although there is no consistent street line given the variety of building typologies and lot configurations on Base Line Road West, the proposed building reflects contemporary urban design standards by being oriented parallel to the street with a minimum front yard setback. This also results in a maximum separation distance of 36.8 metres between the proposed building and the existing high-rise building to the rear.

Buildings

Please refer to the “Built Form, Massing and Articulation” portion of section 5.2 of this report. In my opinion, the Development Proposal conforms to the relevant City Design policies. Since this project is subject to site plan control, building and site details will be further refined at this stage of the development review process. Although a rooftop plan was initially requested by City of London urban design staff, since no rooftop amenities are proposed, it was agreed that this drawing can be provided at the site plan control stage.

In conclusion, it is my opinion that the Development Proposal conforms with the relevant City Design policies of the Official Plan. As the Development Proposal is an example of residential intensification in neighbourhoods (i.e., the Neighbourhoods Place Type), policies 937-940 apply. In particular, policies 939_5 and 6 both apply. The Development Proposal can be characterized as both “infill development” and “redevelopment” as the Subject Site is underutilized, and the existing detached houses are proposed to be demolished and replaced with 77 dwelling units. In accordance with policy 940, it is the intent of the Official Plan “support all of these forms of intensification, while ensuring that they are appropriately located and fit well within their neighbourhood.” The Development Proposal is an example of contextually sensitive redevelopment that is designed to fit harmoniously within its existing and planned context.

In addition to the City Design policies referenced above, policy 953 references “additional urban design considerations for residential intensification”. This report addresses the requirement in policy 953_1 for a “Planning and Design Report” for all intensification proposals. As the compatibility and fit of the Developed Proposal with the surrounding neighbourhood has already been addressed in this report, an additional analysis is not necessary. Please review the description and analysis in this report in conjunction with the architectural drawings being submitted. To summarize, it is my opinion that the Development Proposal has adequately addressed the criteria in policy 953_2 and that its proposed intensity is appropriate for the size of the lot such that it can accommodate the requirements in policy 953_3. Justification for the zoning deficiencies will be provided in the zoning analysis below.

8 Zoning Analysis and Proposed Zoning By-law Amendment

The existing zoning for the Subject Site is Residential R9 (R9-7*H45), which permits apartment buildings up to a height of 45 metres. Although the existing zone is not proposed to be changed, the following special provisions would be required to permit the Development Proposal:

Zoning Standard	Required	Proposed	Comments
Front yard depth (m min)	3.0	0.96	This is the proposed minimum front yard setback after the road dedication corresponding to a column of the breezeway, as illustrated on the site plan. As the proposed building is not exactly parallel to the property line and Base Line Road West, it appears staggered with several different front yard setbacks with the largest front yard setback occurring on the western side (i.e., 3.12 metres). The building is intentionally as close to the street as possible to maximise the distance between it and the existing high-rise building to the north and promote pedestrian connectivity by limiting the distance between the street and the front entrance of the building.
Interior side yard depth (m min) 12.0 m		East (4.97)	<p>The depth or setback is the distance between the eastern property line and the eastern footing of the breezeway. While a 12-metre side yard setback does allow for increased landscaping versus the proposed 4.97 metres, this required setback is reflective of more suburban development on large lots or even the “tower in the park” aesthetic, which has been deemed to be problematic. Contemporary Infill development in urban areas tends to occur on more constrained lots where it is challenging to provide such large setbacks while respecting municipal design standards (e.g., paratransit layby areas) and ensuring that a project is financially feasible.</p> <p>Regarding buffering and transition, there is adequate separation distances between the proposed development and existing development, as has been argued in this report and illustrated by the drawings supporting the application.</p>

			Furthermore, the 1.8 metre landscape strip complies with the 1.5 metre width required by the City of London Site Plan Control By-law. A landscape plan showing the proposed plantings would be provided at the site plan control stage.
		West (6.37)	See comments above. Furthermore, the amenity space will likely include plantings, as will be shown on the landscape plan to be provided at the site plan control stage
Interior side yard encroachment (m max)	3.0	4.87	This is being requested since it appears that the amenity space structure on the west side of the building will be categorized as an encroachment subject to section 4.27 of the Zoning By-law since it is attached to the building. This is a minor deficiency to permit an important shared outdoor amenity space area.
Density (units/ha max)	150	386	Although it is acknowledged that there is a large numerical discrepancy between the permitted and the proposed density, it appears that the Zoning By-law, which implements the 1989 Official Plan, was envisioning a tall high-rise with large units for this site given that the permitted height of 45 metres is significantly higher than the proposed 30 metres. Contemporary market economics have led the applicants to propose smaller units in a mid-rise form, given the site constraints. These smaller units also have the advantage of being more affordable and helping the City of London achieve its housing objectives more quickly than fewer, larger units. So it is important to evaluate the proposal in its entirety and not focus on one or two metrics.
Vehicle parking spaces	39	22	As noted in this report, the Subject Site is within walking distance of amenities (e.g., grocery store) and public transit stops. Long-term and short-term bicycle parking spaces in compliance with the Zoning By-law are also being proposed to reduce car dependency. For further justification, please review the Parking Justification Report included in the submission package.

In summary, the Development Proposal generally meets the intent and purpose of the Zoning By-law and the proposed special provisions are appropriate for the reasons outlined above.

9 CLOSING

Given the above, the proposed ZBA to develop the Subject Site should be supported for the following reasons:

- The Development Proposal would add 77 residential units on a site that is well-suited for increased density as it is located within walking distance to transit stops and daily amenities. The proposed mid-rise building is compatible with the eclectic mix of building typologies and architectural styles within the Base Line Road West corridor and reflects contemporary urban design standards in being located to the street.
- The Development Proposal is consistent with the PPS and conforms to the relevant Official Plan policies.
- The existing zone is being retained and the special provisions to implement the proposed site-specific zoning are appropriate given contemporary standards for infill development in urban areas.

For the reasons noted above and throughout this report, the proposed ZBA application represents sound land use planning practice.

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