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London
CANADA

Chapter 20

Area-Specific Streetscape Standards

Design Specifications & Requirements Manual

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City of London

Design Specifications and Requirements Manual

The design information contained in this manual is intended to provide guidance beyond legislative and standard design practices for use in the City of London (the City). There will be site specific situations where the design will depart from these practices as it is not possible nor is it the intention of the City to anticipate every situation. The City intends to review and revise the Manual from time to time. The City also acknowledges that other references such as the **'Standard Contract Documents for Municipal Construction Projects'** are to be used in conjunction with this manual. The 2012 update of this manual incorporates design information from the City's former 'Subdivision & Development Guide Manual' to provide consistent and current design information for development projects.

The City of London maintains its right to accept or refuse any design submissions and requires an acceptable design for any given circumstance.

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20 Area-Specific Streetscape Standards

20.1 Downtown Streetscape Standards

20.1.1 Introduction

20.1.1.1 Purpose

The purpose of these standards is to provide direction to large- and small-scale projects that involve modifications to the rights-of-way within the downtown area, with the goal of creating a consistent streetscape treatment that is unique to the downtown.

20.1.1.2 Location

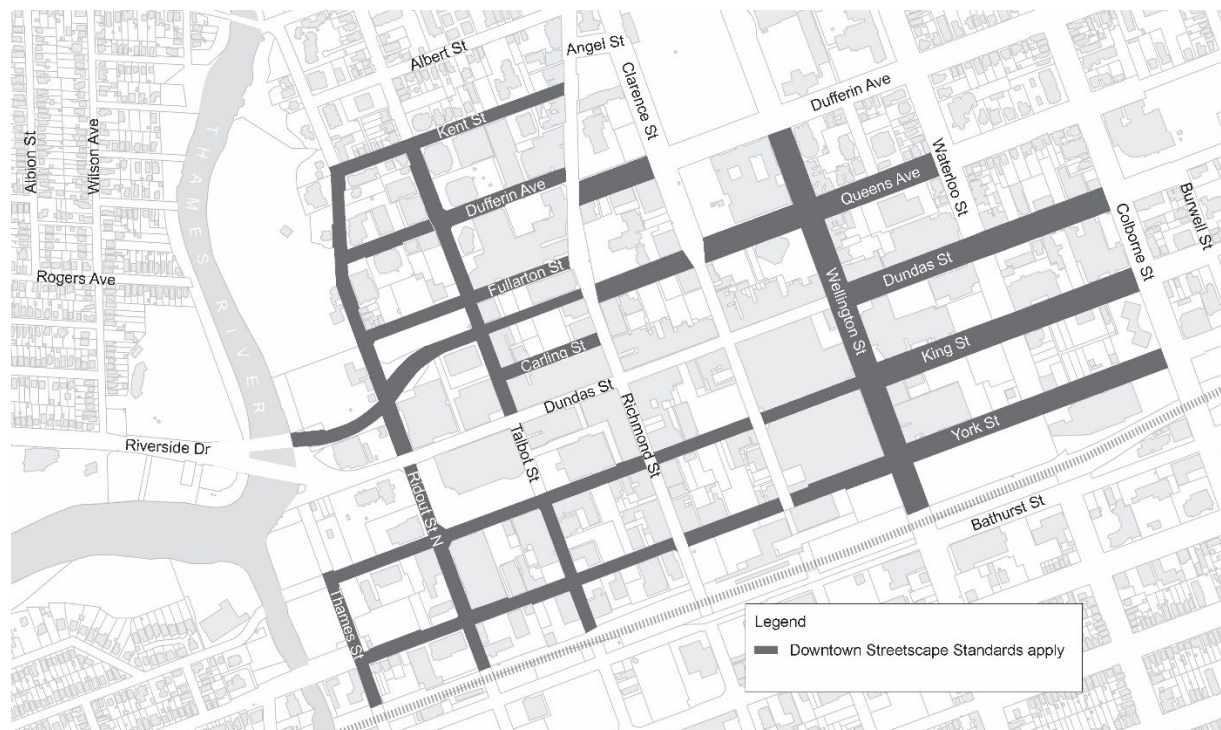
The Downtown Streetscape Standards apply to the following street segments, generally described as:

- York Street, Thames Street to Colborne Street
- Thames Street
- King Street, Thames Street to Colborne Street
- Dundas Street, Wellington Street to Colborne Street
- Carling Street
- Queens Avenue, Thames River to Waterloo Street
- Fullarton Street, Ridout Street North to Richmond Street
- Dufferin Avenue, Ridout Street North to Clarence Street
- Kent Street, Ridout Street North to Richmond Street
- Ridout Street North, train tracks to Kent Street
- Talbot Street, train tracks to King Street
- Talbot Street, Dundas Street to Kent Street
- Wellington Street, train tracks to Dufferin Avenue

These street segments are more precisely identified in **Figure 1**. Where minor discrepancies exist between the above description and **Figure 1**, **Figure 1** takes precedence.

It is important to note that there are street segments within the general downtown area that are excluded from the Downtown Streetscape Standards as other policies direct the streetscape design in these locations.

Figure 1: Downtown Streetscape Standard Location Map

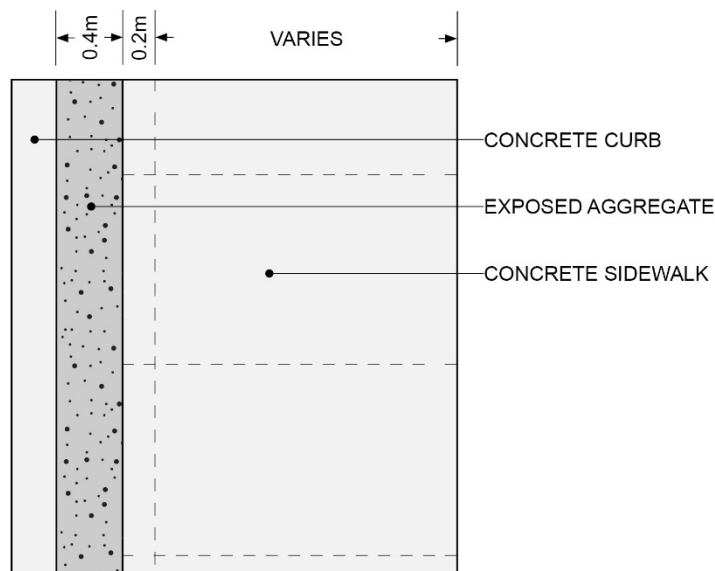


20.1.2 Surface Treatment

20.1.2.1 Standard Surface Treatment

The standard surface treatment for mid-block locations where there is not sufficient room to accommodate street trees within the sidewalk and boulevard area includes a 0.40 metre exposed aggregate band adjacent to the curb edge and a saw cut in the concrete sidewalk parallel to, and 0.20 metres from, the exposed aggregate band. This treatment is illustrated in **Figure 2**.

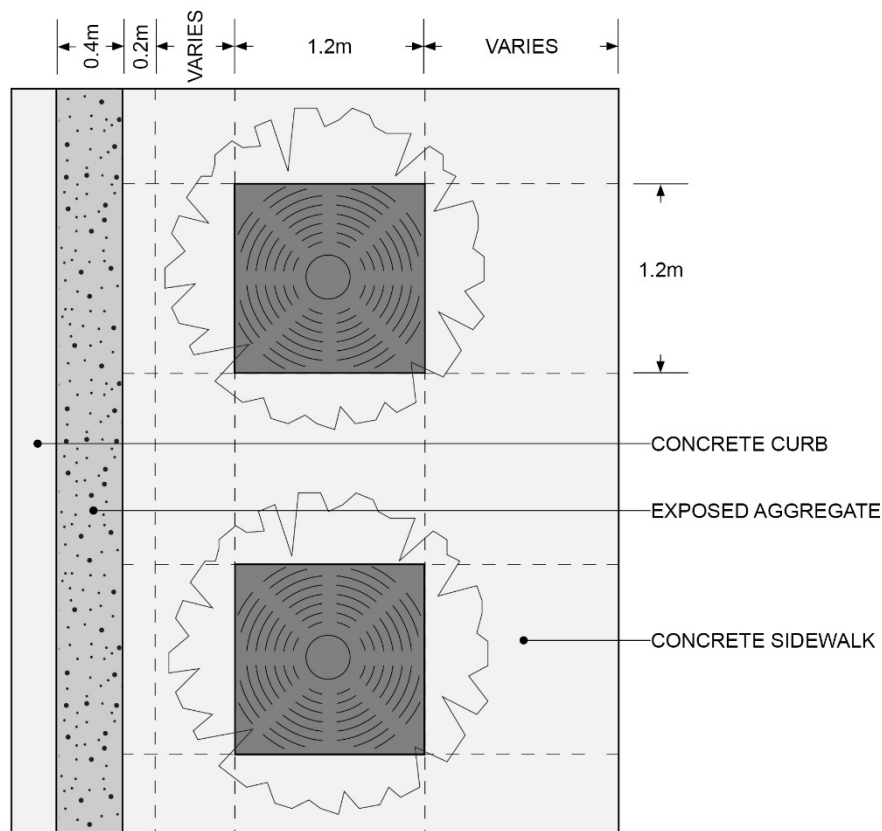
Figure 2: Downtown Standard Surface Treatment



20.1.2.2 Standard Surface Treatment with Street Trees

The standard surface treatment described in **Section 20.1.2.1** applies, with the addition of saw cuts in the concrete that correspond with the placement of the street tree grates. These additional saw cuts extend parallel with all four sides of the street tree grates. The location of street trees and tree grates within the right-of-way will be determined by the context. This standard treatment is illustrated in **Figure 3**.

Figure 3: Downtown Standard Surface Treatment with Street Trees

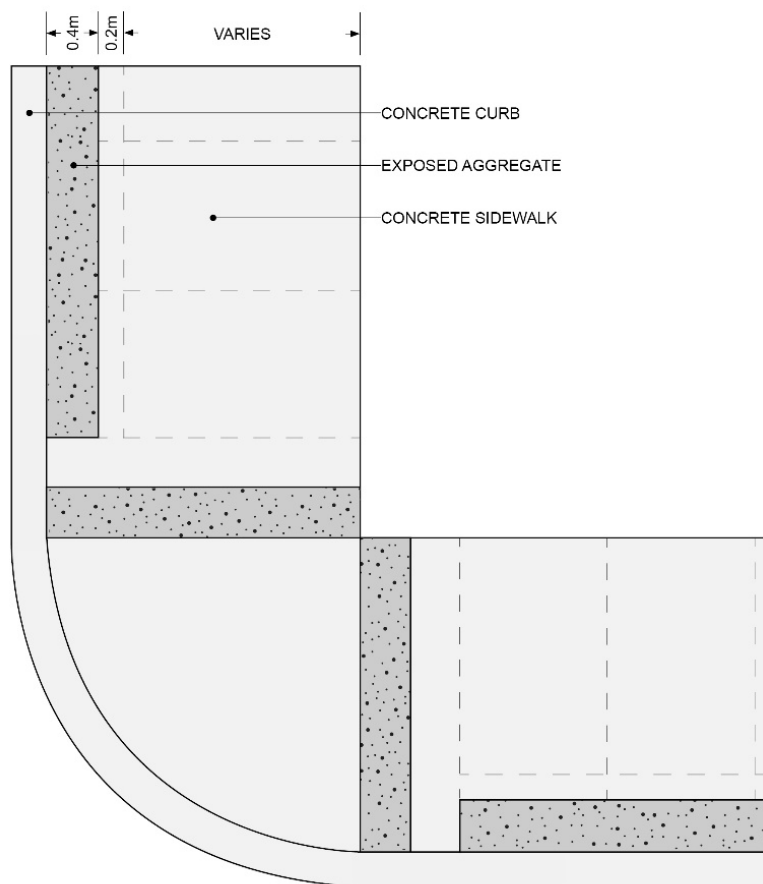


20.1.2.3 Standard Surface Treatment at Corners

The standard surface treatment at corners includes two 0.40-metre exposed aggregate bands, one at each end of the curb radius, perpendicular to the curb edge with two additional saw cuts in the concrete sidewalk that are parallel to, and 0.40 metres from, the exposed aggregate bands.

This standard treatment is illustrated in **Figure 4**.

Figure 4: Downtown Standard Surface Treatment at Corners



20.1.2.4 Curb Engraving

Street names are to be engraved at each intersection on the curb, with the following specifications:

- Front Style: Century Gothic, bold.
- Lettering size: point size 400 (100mm height)
- Ensure engravings are centred on curb vertically and centered within crosswalk markings and tactile plates.
- Ensure lettering follows curve of curb and is consistent width from edge of front curb (25mm).
- Engraving in concrete curb shall be done using sandblasting with very fine particle size.
- Engraved letters shall be to a depth of 6mm or equal to the width of the engraving, whichever is less.
- Finish engraved letters with black colour fill.

- Gaps between letters to be no less than 20mm.
- Spaces between word to be no less than 100mm.

20.1.2.5 Sod Boulevard Treatment

In instances where there is an existing grass boulevard that is essential to the heritage character of the streetscape, as identified by the *Downtown London Heritage Conservation District Plan*, this grass boulevard should be preserved or maintained. Application of the sod boulevard treatment will be determined within the context of the work being completed. In these instances, a grass strip may replace the curbside exposed aggregate band in the surface treatments described in **Sections 20.1. 2.1**, **20.1.2.2**, and **20.1.2.3**, and may vary in width dependent on the context.

20.1.3 Enhanced Crosswalk Treatment

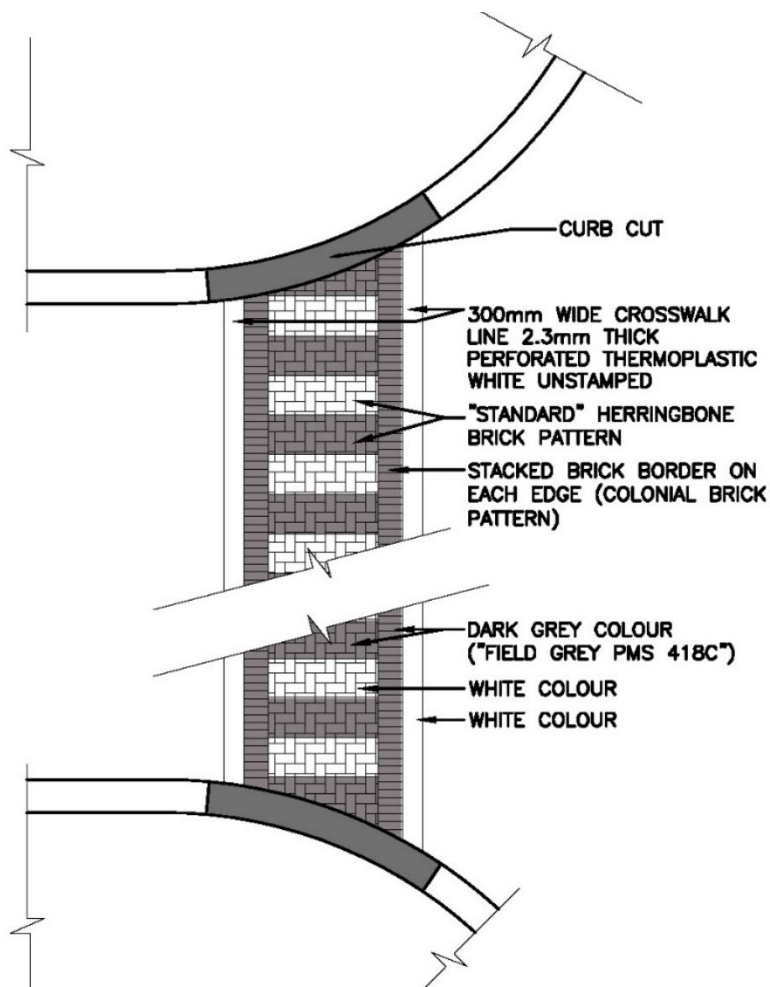
20.1.3.1 Location of Enhanced Crosswalk Treatment

The enhanced crosswalk treatment is to be applied at intersections in close proximity to rapid transit stations, as well as any intersection within the area where the Downtown Streetscape Standards apply as determined by the City of London.

20.1.3.2 Enhanced Crosswalk Treatment Details

The enhanced crosswalk treatment is a stamped asphalt treatment (TrafficPatternXD HUBS Surface Systems) or approved equal. The pattern is “Standard Herringbone” with a “Stacked Brick Border”. The primary crosswalk colour is “Field Grey” (PMS 418C) with white transverse and lateral markings. Details are provided in **Figure 5**.

Figure 5: Downtown Enhanced Crosswalk Treatment



20.1.4 Street Furniture

20.1.4.1 Bicycle Parking

The bike rack standard is the TeMo bike rack in black powder coat. Bike racks can be placed as singles or in groups.

20.1.4.2 Street Lighting

The street light standard is the Lumec Urbanscape LED Post-Top in black powder coat.

20.1.4.3 Tree Guards

The tree grate standard is the GreenBlue Urban Thames Tree Guard typically in the 1.2 metres by 1.2 metres option in black powder coat. Please also see **Section 12.12** diagram D-35A.

20.1.4.4 Tree Grates

The tree guard standard is the GreenBlue Urban Thames Tree Grate in fine-textured black powder coat.

20.1.4.5 Approved Equal

The street furniture described in **Section 20.1.4** are the approved streetscape elements for the Downtown Streetscape Standards area. If circumstances arise where the approved street furniture element cannot be implemented, such as due to product discontinuation, an equal product may be implemented subject to the approval of Planning and Development.

20.2 Richmond Street Streetscape Standards

20.2.1 Introduction

20.2.1.1 Purpose

The purpose of these standards is to provide direction to large- and small-scale projects that involve modifications to the rights-of-way along Richmond Street, with the goal of creating a consistent streetscape treatment that is unique to Richmond Street.

20.2.1.2 Location

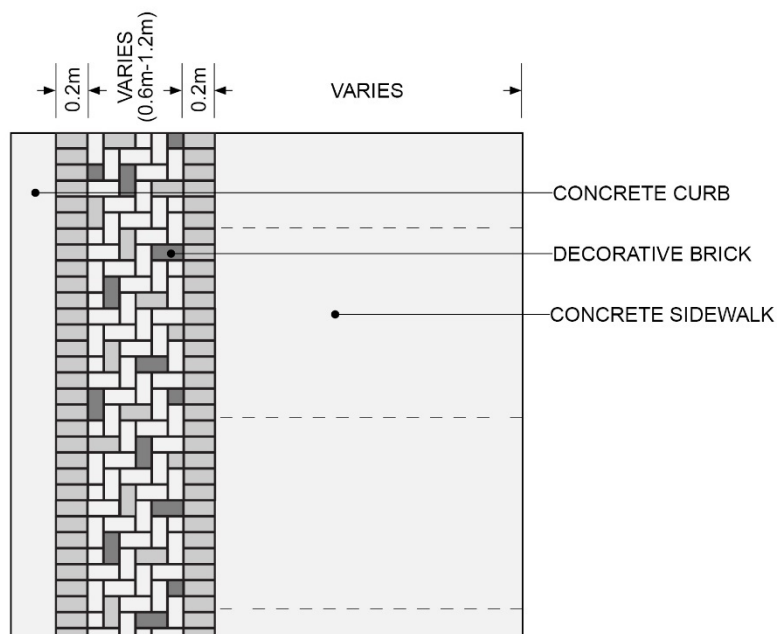
The Richmond Street Streetscape standards apply to Richmond Street between Oxford Street and York Street.

20.2.2 Surface Treatment

20.2.2.1 Standard Surface Treatment

The standard surface treatment includes a decorative brick detail (see **Section 20.2.2.4**) along the curb edge. The brick detail includes a soldier course border with a standard herringbone brick pattern between. The herringbone brick pattern detail varies in width between a minimum of 0.6 meters to a maximum of 1.2 metres and is dependent on the space available. The standard surface treatment is illustrated in **Figure 6**.

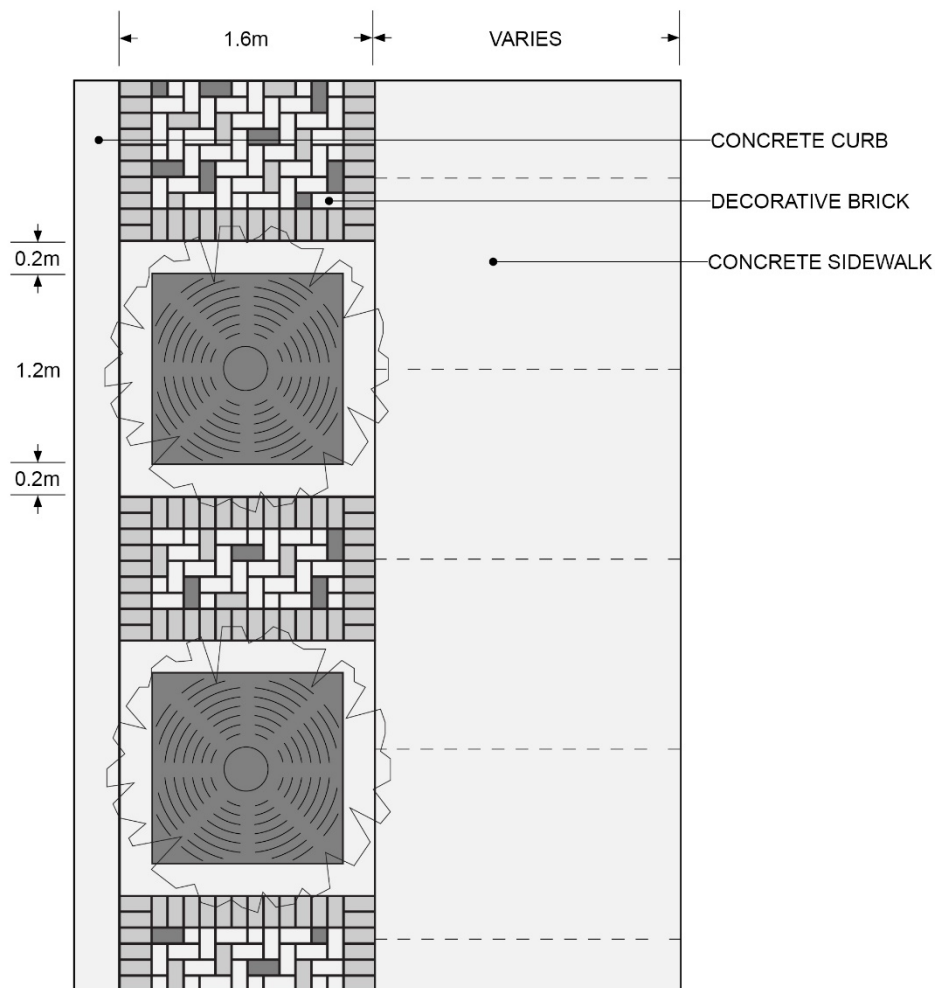
Figure 6: Richmond Street Standard Surface Treatment



20.2.2.2 Standard Surface Treatment with Street Trees

In instances where street trees can be accommodated, the decorative brick detail (see **Section 20.2.2.4**) is 1.6 meters wide along the curb edge and a 0.2 metre concrete border is applied around the tree grate. This standard treatment is illustrated in **Figure 7**.

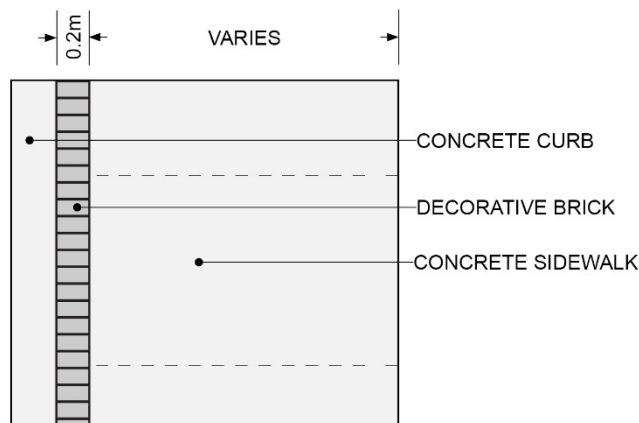
Figure 7: Richmond Street Standard Surface Treatment with Street Trees



20.2.2.3 Minimum Standard Surface Treatment

The minimum standard surface treatment should only be used where space is constrained and where the treatment in **Section 20.2.2.1** and **20.2.2.2** cannot be accommodated due to the specific context. The minimum standard surface treatment includes a single soldier course of brick adjacent to the curb (**see Section 20.2.2.4**) and is illustrated in Figure 8.

Figure 8: Richmond Street Minimum Standard Surface Treatment

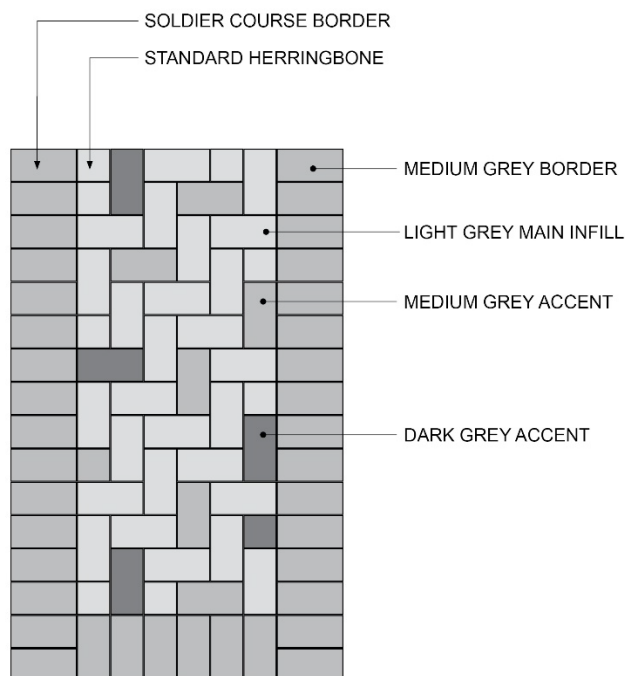


20.2.2.4 Decorative Brick Detail

The application of the decorative brick detail applies to the standard surface treatments as described in **Section 20.2.2.1**, **20.2.2.2** and **20.2.2.3**, as appropriate. Where a soldier course brick detail is used, the bricks should be medium grey. Pavers are to be installed on top of a concrete base in all instances.

Where a standard herringbone pattern is used, as described in **Section 20.2.2.1** and **20.2.2.2**, the main infill should be light grey and make up approximately 80 percent of the pattern. The accent bricks should be medium grey and dark grey and each make up approximately 10 percent of the bricks used within the standard herringbone pattern. This standard brick detail is illustrated in **Figure 9**.

Figure 9: Richmond Street Decorative Brick Detail



20.2.2.5 Curb Engraving

Street names are to be engraved at each intersection on the curb, with the following specifications:

- Front Style: Century Gothic, bold.
- Lettering size: point size 400 (100mm height)
- Ensure engravings are centred on curb vertically and centered within crosswalk markings and tactile plates.
- Ensure lettering follows curve of curb and is consistent width from edge of front curb (25mm).
- Engraving in concrete curb shall be done using sandblasting with very fine particle size.
- Engraved letters shall be to a depth of 6mm or equal to the width of the engraving, whichever is less.
- Finish engraved letters with black colour fill.
- Gaps between letters to be no less than 20mm.
- Spaces between word to be no less than 100mm.

20.2.3 Street Furniture

20.2.3.1 Bicycle Parking

The bike rack standard is the TeMo bike rack in black powder coat. Bike racks can be placed as singles or in groups.

20.2.3.2 Street Lighting

The street light standard is the Lumec Urbanscape LED Post-Top in black powder coat.

20.2.3.3 Tree Guards

The tree grate standard is the GreenBlue Urban Thames Tree Guard typically in the 1.2 metres by 1.2 metres option in black powder coat. Please see **Section 12.12** diagram D-35A.

20.2.3.4 Tree Grates

The tree guard standard is the GreenBlue Urban Thames Tree Grate in fine-textured black powder coat.

20.2.3.5 Approved Equal

The street furniture described in **Section 20.2.3** are the approved streetscape elements for the Richmond Street Streetscape Standards area. If circumstances arise where the approved street furniture element cannot be implemented, such as due to product discontinuation, an equal product may be implemented subject to the approval of Planning and Development.