

## SECTION 4

### GENERAL PROVISIONS

#### 4.0 APPLICATION OF GENERAL PROVISIONS

No person shall erect or use any building or structure or use any land, or cause or permit any building or structure to be erected or used or cause or permit any land to be used in any zone within the City of London for lands affected by this By-Law except in conformity with the regulations specified by the applicable general provisions set out in all subsections of Section 4. (Z.-1-051390)

#### 4.1 ACCESSORY USES

##### 1) ACCESSORY USES PERMITTED IN ALL ZONES

Where this By-Law permits a lot to be used or a building or structure to be erected or used for a purpose, that purpose shall include any building, structure or use accessory thereto, except that no home occupation, open storage or accessory dwelling unit shall be permitted in any zone other than a zone in which such a use is specifically listed as a permitted use. No accessory building, structure or use in an agricultural zone or a residential zone shall be used for human habitation, except where a dwelling unit is permitted as an accessory use or where the zone permits a bed and breakfast establishment, secondary farm dwelling, temporary garden suite or hotel. (Z.-1-051390)

##### 2) LOT COVERAGE

The total lot coverage of all accessory buildings or structures on a lot shall not exceed 10 percent (10%) of the lot area of the said lot. In agricultural zones the size of accessory buildings is limited to 25% of the size of the main farm dwelling excluding main farm buildings such as barns, greenhouses, stables and driving sheds. The percent coverage's of accessory buildings and structures are included in the percentage total coverage permitted on a lot. (Z.-1-051390)

##### 3) HEIGHT RESTRICTIONS

Notwithstanding the definition of height, the height of an accessory building shall be measured from the average finished grade to the upper most point of the building or structure and shall not exceed the following overall height : (Z.-1-97490)

- a) all Residential Zones, 4.0 metres (13.1 feet) except as provided in 4.1.4(b) where the height may be increased to 6.0 metres (19.7 feet); (Z.-1-97490)
- b) all Commercial, Agricultural and Institutional Zones, 6.0 metres (19.7 feet); (Z.-1-051390)
- c) all other zones, 10.0 metres (32.8 feet).

##### 4) LOT REQUIREMENTS OR LOCATION

Accessory buildings or structures are permitted in the following locations: (Z.-1-051390)

- a) no accessory building or structure shall be permitted within a required front yard or the exterior side yard; (Z.-1-97490)
- b) when such accessory building or structure is located in an interior side yard or a rear yard, it shall be no closer than 0.6 metres (2.0 feet) to the side lot line and rear lot line. Where the height exceeds 4.0 metres (13.1 feet) but in no case more than 6 metres (19.7 feet), the side and rear lot line setback shall be increased by the difference in the height above 4.0 metres (13.1 feet); (Z.-1-97490)

- c) when such accessory building or structure is within a non-residential zone and is located in an interior side yard or a rear yard, it shall be no closer than 1.2 metres (3.9 feet) to the side lot line and rear lot line. Where such an accessory building or structure abuts a residential zone, the setback requirement shall be equal to one-half of the overall height of the structure but in no case less than 1.2 metres (3.9 feet) to the side lot line and rear lot line; (Z.-1-97490)
- d) when such accessory building or structure is wholly or partly located in an exterior side yard, or a rear yard abutting a street, the minimum setback shall not be less than the required front yard setback for the zone in which the lot is located; (Z.-1-97490)
- e) when such an accessory building or structure is less than 10 square metres (108 square feet) no yard setbacks shall be required provided the building or structure is located entirely in the rear yard, except as provided in 4.1.4(d); (Z.-1-97490)
- f) when the accessory building or structure is a gatehouse, it may locate in the front yard or the exterior side yard in an industrial zone;
- g) when the accessory building or structure is a satellite dish, it shall be restricted to interior side yards or rear yards;
- h) swimming pools shall comply with the provisions of Subsection 4.25 (Swimming Pools) of this By-Law;
- i) air conditioning units and heat pumps shall comply with the provisions of Subsection 4.27 . (Yard Encroachments Permitted)

#### 5) PARKING STRUCTURES

Within this By-Law, a parking structure will be considered an accessory use but will not be subject to the general provisions subsections 4.1(2), (3) or (4), but rather, subject to the building regulations of the zone in which it is located.

- 6) Minor Motor Vehicle Service and Repair is permitted as an accessory and incidental residential use in all residential zones that permit single, duplex, semi-detached, duplex, triplex, fourplex or converted dwellings excluding uses ancillary to an existing agricultural use, if all of the following conditions are met:
  - 1. The vehicle being serviced or repaired must have a current valid ownership permit issued to an occupant or a spouse, child or sibling of an occupant currently residing in the dwelling.
  - 2. The minor motor vehicle service repair shall be conducted between the hours of 8:00 am to 8:00 pm.
  - 3. No more than one vehicle may be serviced or repaired at the same time.
  - 4. Minor motor vehicle service repair shall not occur for more than 6 consecutive calendar days.
  - 5. Motor vehicle painting, except for minor touch-ups, is prohibited at all times.
  - 6. Minor motor vehicle service repair as a commercial use is prohibited at all times. (Z.-1-071653)

#### 4.2 ACCESS REGULATIONS

No person shall hereafter erect any main building or structure on any lot in any zone unless such lot or an abutting lot held under the same ownership abuts an improved street or is a lot on a registered plan of subdivision. In the case of a parcel of land for which a Development Agreement, Subdivision Agreement and/or a Site Plan or any one of them has been approved and registered against title pursuant to Section 41 or Section 50 of the *Planning Act, R.S.O. 1990, c.*

P.13, as amended from time to time, then the single parcel of land to which the Development Agreement and/or plan applies shall be deemed to be the lot for application of this Section, notwithstanding that part of the parcel may be in separate ownership; does not otherwise satisfy the definition of lot; or does not abut an improved street. (Z.-1-94236) (O.M.B. File # R910387 - Appeal #9006-2 June 4, 1993)

#### 4.3 BONUS ZONES

1) HERITAGE BUILDING DESIGNATION BONUS - FLOOR AREA AND DWELLING UNIT DENSITY BONUS:

For buildings and/or structures designated as historically significant by the City of London, under the *Ontario Heritage Act*, in consideration for their designation, the following regulations shall apply for:

Non Residential Uses:

- a) a 25 percent (25%) increase in the gross floor area of the existing building and/or a 25 percent (25%) decrease in the required minimum parking for a permitted use on the lot or;

Residential Uses:

- b) for every 15 square metres of existing gross building floor area of a designated building, 1 additional dwelling unit shall be permitted for a permitted use on the lot to a maximum increase of 25% of the dwelling units permitted in the zone.

The increase in gross floor area and/or dwelling units shall be permitted in accordance with the regulations of the applicable zone. Where the proposed building is a mixture of residential and non-residential permitted uses, the bonus shall not exceed a combined total percentage of 25 percent (25%) increase in maximum number of dwelling units and non-residential floor area for the lot.  
(Excluding 120 Kent Street - Z.-1-95316)

2) PUBLIC OPEN SPACE

For every 100.0 square metres (1,074 square feet) of public open space which is dedicated to the City (in excess of the required parkland dedication any undevelopable floodplain lands and Class 1,2, or 3 Wetlands), the density of the residential development may be increased by one unit per each 100.0 square metres (1,074 square feet) up to 25 percent (25%) of the total number of units that would otherwise be permitted by this by-Law.  
(Excluding 120 Kent Street - Z.-1-95316)

3) DAY CARE FACILITIES

Where day care facilities are provided within commercial or mixed-use buildings of larger than 1858.0 square metres (20,000 square feet), the floor area devoted to the day care facilities shall not be included in the floor area ratio, the maximum gross or gross leasable floor area permitted, or in the calculation of the parking requirements for the building.

4) SITE SPECIFIC BONUS PROVISIONS

Notwithstanding the Density (D) and Height (H) zones which apply to a lot where a B (Bonus) zone also applies to the lot, the maximum permitted density and/or height and/or parking may be altered in accordance with the provisions set out below on having entered into a Development Agreement, with bonusing clauses, with the City of London. (Z-1-051390)

The number following the letter "B" on the Zone Maps indicates the number of the applicable Bonus Zone provision set out below.

**B-1**

(Added by Z.-1-95315)  
(Deleted by Z.-1-98591)

**B-2**

- i) The maximum permitted density may be increased to 325 units per hectare (132 units per acre) if the building design approved by Council, as contained in the bonusing clause of the development agreement, is built. The design is intended to be sympathetic, with regard to the design of the porch and the colour, to the heritage structure located to the south at 530 Ridout Street North. (Z.-1-98590)

**B-3 435 Colborne Street**

- i) The subject site is being bonused for enhanced urban design and underground parking. The maximum number of units may be increased by thirteen (13) to a total of forty-three (43) if the building design as contained in the bonusing clauses of the development agreement, is built. The building design is identified through the elevations, site plan, and floor plans included in the development agreement. (Z.-1-051380) (Z.-1-071604)

**B-4 484-500 Ridout Street North**

- i) The maximum permitted density may be increased to 750 units per hectare (303 units per acre) if the building design approved by Council, as contained in the bonusing clause of the development agreement, is built. The design is intended to be sympathetic to the existing streetscape and surrounding buildings. The design includes a "human scale" ground level and an upper level setback in addition to complementary colours and balcony treatments. The preferred site plan, landscape plan and building elevation are attached to the development agreement. (Z.-1-051396)

**B-5 180 Mill Street**

- i) To ensure compatibility and a transition between high-rise development to the east and lower scale development to the west, three design features are required to obtain bonus zoning. They include 1) an apartment building on the eastern portion of the site with a progressively lower height as it approaches Mill Street and 2) a row of street-orientated townhouse units along St. George Street and a retaining wall with a high quality "street friendly" design and 3) a high quality "street-friendly" treatment for the townhouses along Mill Street. The maximum permitted density and/or height may be increased with the approval of a design to the satisfaction of the City of London and on having entered into a Site Plan Agreement with the City of London. The cumulative impact of using this bonusing provision shall not result in a density greater than 250 units per hectare (100 units per acre) and a building height greater than 40 metres (131.2 feet). (Z.-1-061457)

**B-6 305-325 Queens Avenue**

- i) A maximum height of 90 m (295 ft.) will be permitted on this site subject to the property owner entering into a development agreement which establishes a high quality of architectural design for this building. The location of the new structure will be identified on the future site plan and bonusing clauses will be inserted in the resultant development agreement to implement the bonusing provisions. (Z.-1-061460)

**B-7 59-73 King Street, 342-360 Ridout Street North and 68 York Street**

- i) To implement Council's initiative to significantly increase the number of new residential units in the Downtown and in return for the contribution of \$100,000 towards public art to be installed on this site, transit-supportive design, enhanced landscaping, commercial uses at grade, the provision of some amount of underground parking, the integration of design features which reduce the visual massing of the building, articulate the base of the building to be sympathetic with surrounding building heights and create a quality pedestrian streetscape and positive contribution to the London skyline, as specified in the bonusing clauses in the development agreement, the maximum density allowed on this site may be increased from 350 units per hectare to 750 units per hectare and height from 90 metres to 95 metres. (Z.-1-061557)

**B-8 863, 867 and 869 Adelaide Street North, and on a portion of 859, 861, and 865 Adelaide Street North**

The subject site is being bonused for enhanced urban design. The building design is identified through the elevations, site plan, and landscaping plan included in the development agreement, which includes one apartment building, 42 units, 3 storeys in height (maximum 10.0 metres), located along the northern boundary of the site, and one stacked townhouse development, 10 units, 2 storeys in height (maximum 8.0 metres), located along the western boundary of the site.

A design character statement has also been included in the development agreement to provide guidance to any minor changes that are made as these plans are finalized through the site plan process.

The following regulations apply with the approved site plan:

Permitted Uses:

- i) Apartments and Stacked Townhouses
  - Lot Area (minimum): 6,000 square metres
  - Lot Frontage (minimum): 15.0 metres
  - Front Yard Setback (minimum): 7.0 metres
  - Interior Side Yard – north (minimum): 3.0 metres
  - Interior Side Yard – south (minimum): 9.0 metres
  - Rear Yard Depth (minimum): 4.5 metres
  - Landscaped Open Space (%) (minimum): 30%
  - Lot Coverage (%) (maximum): 30%
  - Height (maximum): 10.0 metres
  - Density – units per hectare (maximum): 87
  - Parking: 1 space per unit

The cumulative impact of using this bonusing provision shall not result in a density greater than 87 units per hectare (215 units per acre) and a building height greater than 10 metres (32.8 feet). (Z.-1-071641)

**B-9 590 & 598 Beaverbrook Avenue**

The subject site is being bonused for enhanced urban design. The building design is identified through the elevations, site plan, and landscaping plan included in the development agreement, which includes two apartment buildings, 72 units each (combined total of 144 units), and 5 storeys in height (maximum 16.0 metres), located along the southern boundary of the site. An underground parking deck will also be included in the final design.

Design guidelines will be included in the development agreement to provide guidance to any minor changes that are made as these plans are finalized through the site plan process.

The following regulations apply with the approved site plan:

Permitted Uses:	Apartments
Lot Area (minimum):	1.0 hectares
Lot Frontage (minimum):	70.0 metres
Front Yard Setback (minimum):	6.0 metres
Interior Side Yard – north (minimum):	13.0 metres (building); 4.5 metres (parking structure)
Interior Side Yard – south (minimum):	6.0 metres
Rear Yard Depth (minimum):	7.0 metres
Landscaped Open Space (%) (minimum):	30%
Lot Coverage (%) (maximum):	40%
Height (maximum):	16.0 metres (or 5 stories, whichever is less)
Density – units per hectare (maximum):	100
Parking:	1 space per unit

The cumulative impact of using this bonusing provision shall not result in a density greater than 100 units per hectare (247 units per acre) and a building height greater than 16.0 metres.  
(Z.-1-071660)

**B-10 725, 729 Dundas Street and 704, 738 King Street and 380, 382 Lyle Street and 387, 389, 391 Hewitt Street**

Deleted by Z.-1-152397

**B-11 570, 572, 576 & 580 Beaverbrook Avenue**

The subject site is being bonused for enhanced urban design. The building design is identified through the elevations and site plan included in the development agreement, which includes four apartment buildings, 74 units each (combined total of 296 units), and 5 storeys in height (maximum 18.0 metres).

Design guidelines will be included in the development agreement to provide guidance to any minor changes that are made as these plans are finalized through the site plan process.

The following regulations apply with the approved site plan:

Permitted Uses:	Apartments
Lot Area (Minimum)	3.0 hectares
Lot Frontage (Minimum)	65.0 metres
Front Yard Setback (Minimum)	3.0 metres
Interior Side Yard – North (Minimum)	3.0 metres
Interior Side Yard – South (Minimum):	7.0 metres
Exterior Side Yard North (Minimum)	3.0 metres
Rear Yard Depth (Minimum)	3.0 metres
Landscaped Open Space (Minimum)	40%
Lot Coverage (Maximum)	30%

Height (Maximum)	18.0 metres (or 5 storeys, whichever is less)
Density – units per hectare (Maximum)	82
Parking: (Z.-1-091825)	1 space per unit

**B-12 726, 728, 732, 740, 742, 744, 746, 748 Dundas Street and 415 English Street**

The subject site is being bonused for enhanced urban design, underground parking, green roof and the revitalization of the Old East Village. The building design is identified through the elevations and site plan included in the development agreement, which includes a mixed use building with 154 residential units and 9 commercial units.

The following regulations apply with the approved site plan:

i)	Density (Maximum)	483 units per hectare
ii)	Secure Bicycle Parking Spaces (Minimum)	30 (notwithstanding the requirements of Section 4.19 (16)) of this By-law
iii)	Front Yard Depth (Minimum)	3.0 metres above the first 12 metres of building Height
iv)	Setback of balcony projections from lot line (Minimum)	1.5 metres

The cumulative impact of using this bonusing provision shall not result in a density greater than 483 units per hectare (1,194 units per acre). (Z.-1-091836)

**B-13 637 Dundas Street**

The subject site is being bonused for enhanced urban design, landscaping and the promotion of transit usage and the revitalization of the Old East Village District. The required building design is identified through the elevations, site plan, and landscaping plan included in the development agreement, which includes one three storey commercial building with residential units above fronting Dundas Street and a senior apartment building 30m (maximum 9 storeys) in height fronting on Marshall Street with a total of 72 units (280 units/ha).

The following regulations apply with the approved site plan:

Height (maximum):	30.0 metres
Density – units per hectare (maximum):	280
Parking:	15 spaces minimum
Secure Bicycle Parking Spaces	0 spaces minimum
Interior side yard setbacks to 9 storey senior apartment building:	
East:	7.5 metres
West:	21 metres

Dwelling units are permitted on the whole of the first floor including the frontage on Marshall Street, but excludes the Dundas Street frontage.

The cumulative impact of using this bonusing provision shall not result in a density greater than 280 units per hectare and a building height greater than 30.0 metres (100 feet). (Z.-1-101920)

**B-14 1235, 1237, 1245, 1247, and 1253 Richmond Street**

The subject lands are being bonused for enhanced urban design including: Building orientation toward Richmond Street with parking area located to the rear; building elevations that have been divided into a series of modulated components and are defined by complementary changes in height, massing, and cladding materials; the division and articulation of the base, middle and cap of the building; a variation of building materials and the use of cornices that define the major changes in cladding materials on the elevations; the development of a paved and landscaped urban forecourt along Richmond Street; and, a main entrance area marked by an entry canopy, a change in cladding materials, and a 5-storey colonnade. The building design is identified through the elevations and site plan included in the development agreement, which includes a mixed use building with 311, two-bedroom residential units and 1 commercial unit.

The following regulations apply with the approved site plan:

i)	Density (Maximum)	450 units per hectare
ii)	Number of Bedrooms per Dwelling Unit (Maximum)	2 bedrooms
iii)	Front Yard Depth from ultimate road allowance (Minimum)	4.4 metres
iv)	Rear yard depth (Minimum)	1.8 metres
v)	Interior side yard depth (north) (Minimum)	5.1 metres
vi)	Interior side yard depth (south) (Minimum)	2.1 metres
vii)	Landscaped open space (Minimum)	23%
viii)	Lot coverage (Maximum)	64%
ix)	Height (Maximum)	55 metres
x)	Off-street Parking (Minimum)	174 spaces

Notwithstanding these Zoning Regulations, the cumulative impact of this bonus zone shall not result in the creation of more than 311 residential dwelling units. (Z.-1-112002)

**B-15 307 Fanshawe Park Road East**

The subject site is being bonused for:

1. enhanced urban design features:
  - The proposed project provides an appropriate built form height given its location along Fanshawe Park Road and the width of the Public Right-of-Way;
  - Parking is located underground and/or behind the built form (not in the front setback), which provides a distinguishable built edge to the public realm;
  - The project provides for a residential infill opportunity that creates an active frontage along Fanshawe Park Road by orienting building access and including gathering space along the Public Right-of-Way;
  - Underground parking garage is to provide vehicle and bicycle parking along with garbage storage;



- The development retains the original building and incorporates it into the overall design by using design features and materials found in the surrounding neighbourhood. These include: pitched roofs; brick masonry and decorative wood elements;
- The proposal is organised around a common open space, with landscaping, that provides residents of the site opportunities to socialise;
- Maintaining the existing trees along the western boundary of the site;
- The proposed built form and its massing transitions with the topography of the site;
- Mitigation of the building mass through:
  - . variation of height 2 storey and three storey sections
  - . includes a flat roof which decrease height associated with a pitched roof
  - . moderation of facades with vertical banding and store piers
  - . variation of building material
  - . introduction of vegetation and screening on third storey
  - . terracing of end units (wedding cake form)

and the following services and matters:

2. a centralized courtyard which provides a common open space that is functional for passive recreational use;
3. 54% landscaped open space;
4. 42 underground parking spaces; and
5. 0.0m front yard setback to allow for a pedestrian accessibility ramp.

The building design will be in accordance with the elevations sections, and site plan attached as Appendix “B” of this By-law, which includes one apartment building, 16 units, 3 storeys in height (maximum 13.0 metres), and a converted dwelling with two units.

The following regulations apply with the approved site plan:

Permitted Uses:	Apartment and a converted two unit dwelling
Lot Area (minimum):	5,651 square metres
Lot Frontage (minimum):	53.3 metres
Front Yard Setback (minimum):	0.0 metres
Interior Side Yard – east (minimum):	7.2 metres to the converted dwelling
Interior Side Yard- east (minimum):	9.8 metres to the apartment building
Interior Side Yard – west (minimum):	7.1 metres to the stairwell
Interior Side Yard- west (minimum):	9.7m to the apartment building
Rear Yard Depth (minimum):	9.5 metres
Landscaped Open Space (%) (minimum):	54%
Lot Coverage (%) (maximum):	32%
Height (maximum):	13.0 metres
Density – units per hectare (maximum):	32
Parking:	42 Underground spaces and 5 surface spaces
Secure Bicycle Parking	15 Minimum

The cumulative impact of using this bonusing provision shall result in a density not greater than 32 units per hectare (79 units per acre) and a building height not greater than 13 metres (42.6 feet).

(Z.-1-112013)

## **B-16 909 Southdale Road West and 3045 Pomeroy Lane**

The increase in height and density to the zoning of the subject lands shall be permitted in return for enhanced urban design elements and urban design features that consist of:

- Constructing two apartment buildings that are “L” shaped in plan, with terraced profiles that will achieve a maximum height of 50.5m and contain a maximum of 434 dwelling units;
- The two apartment buildings shall be constructed in a mirror image with one apartment building located at the northeast corner of the subject lands with the two building faces constructed parallel to Southdale Road West and Pomeroy Lane and the other building located at the northwest corner of the subject lands with the two building faces constructed parallel to Southdale Road West and the westerly rear lot line in order to frame public realm;
- Defining the base, middle, and top of the apartment buildings:
  - with the base consisting of the portion of the façade between the ground floor and the third and fourth floors and using architectural stone and cornice lines to define the base;
  - with the middle consisting of the portion of the façade between the top of the base to the fourteenth floor and using coloured concrete and cornice lines to define the middle; and,
  - with the top consisting of the portion of the façade above the fourteenth floor and using coloured concrete differentiated from the middle, floorplate setbacks and a roof form profile to define the top;
- Addressing the intersection of Southdale Road and Pomeroy Lane with the concentration of massing and architectural treatment of the eastern apartment building to naturally direct the user to the primary entrances;
- Introducing of trees and vegetation along the street frontage to improve the pedestrian experience along this section of Southdale Road;
- Designing and constructing an attractive, curb free paved and landscaped forecourt using unit pavers between the north facade and the Southdale Road West and the east façade and Pomeroy Lane to create an appealing interface between the buildings and the abutting roads;
- Designing the parking structure elevations to complement the architectural details and materials used in the elevations of the base of the two residential apartment buildings to create visual consistency between the apartment buildings and parking structure;
- Constructing of a 1.8m high solid board privacy fence along the southern property line to provide a visual barrier between the parking structure and the rear yards of the abutting single detached residential lots to the south; and,
- Planting a dense row of coniferous trees between the privacy fence and the parking structure to augment the visual barrier between the parking structure and the rear yards of the abutting single detached residential lots to the south.

The building design is identified through the elevations, site plan, and landscaping plan included in the development agreement.

The following regulations apply with the approved site plan:

- |      |   |                       |           |                     |
|------|---|-----------------------|-----------|---------------------|
| i)   | Height  | 50.5 metres           | (Maximum) | (165.7 feet)        |
| ii)  | Density   | 207 units per hectare | (Maximum) | (83 units per acre) |
| iii) | Interior side yard depth (south) from main building | 35.0 metres           | (Minimum) | 114.8 feet)         |
| iv)  | Rear yard depth                                     | 18.1 metres           | (Minimum) | (59.4 feet)         |

v)	Lot coverage (Maximum) (Z.-1-122088)	40.5%
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**B-17 1461 Huron Street.**

The subject site is being bonused for providing:

1. an affordable apartment building 13m (maximum 3 storeys) in height with a total of 25 units (81 units/ha); and
2. enhanced urban design including the following:
  - site the building in general alignment with property lines and prevailing setbacks of adjacent buildings;
  - provide landscaping which:
  - incorporates a hard surface forecourt with fixed seating that defines the building's main entrance and corner lot location;
  - integrates stone salvaged from the demolished place of worship;
  - defines private outdoor amenity areas with hard surfaced terraces and fence-wall privacy screens for all at-grade residential units;
  - minimizes the need for extensive watering;
  - provide a building which:
  - clearly expresses a base, middle, and top (roof) on all elevations, predominantly using masonry materials;
  - uses variations in massing, materials, scaling and architectural features to identify the north façade and entrance as the front of the building and the south façade and entrance as a service egress; and employs ample glazing to distinguish stairwell forms from the residential units and maximize visibility for security purposes.

The cumulative impact of using this bonusing provision shall not result in a density greater than 81 units per hectare. (Z.-1-122096)

**B-18**

To complement the urban design features in the Council direction given to the Site Plan Approval Authority, the subject site is being bonused for:

- Enhanced urban design features;
- A built form (apartment building) and smaller dwelling unit size that promotes intrinsically affordable housing;
- The provision of parking spaces within the building;
- The use of sustainable design features; and
- The provision of the main entry access at grade for universal accessibility.

The development shall be in accordance with the site concept and renderings attached as Schedule "A" of this By-law and as further addressed or modified below, which includes one new apartment building with a maximum of 33 dwelling units, a maximum height of 15 metres, and a maximum density for the site of 256 units per hectare including the existing building, and shall specifically incorporate the following:

**Site**

- The building is perpendicular to the street allowing views through the site between Hope Street and Princess Avenue.
- A landscaped forecourt in front of the building between the front (south) façade and Princess Avenue.
- The closure of the Hope Street vehicular access while allowing for access to the site for pedestrians.
- The parking area between Hope Street and Princess Avenue is to be treated as a shared vehicular/pedestrian space.
- Green design elements.

## Building

- The four storey height of the building stepping down to two storeys at Hope Street.
- The use of similar design and cladding materials as the existing building on the site and in the surrounding neighbourhood, including brick, pilasters, flat arches topped with keystone details, and quoin corners to complement the existing building.
- The provision of the pedestrian entrance on Princess Avenue in the centre of the front face of the new building.
- Transparent glazing of doors and windows wherever possible, or in the case of the parking garage, open metalwork.
- The use of cornice lines where the building steps back, where the materials and/or colours change between the third and fourth storeys, and at the roofline.
- Provision of residents' common areas and amenities on the main level of the building.
- Placement of a minimum of 30% of the required parking for the site within the apartment building and away from public view.
- Facilities for waste, recyclables and compostibles (where City programs exist) incorporated within the building.
- The extension of the red brick to the top of the building in the central section of the front façade of the building.
- Elimination of the narrow door beside the double door on the rear (north) façade of the building.
- Green design elements.

Notwithstanding anything in the By-law to the contrary including Section 4.28, the following regulations apply to any new building within the approved site plan:

Front Yard Depth (minimum)	6.7 metres (22.0 feet)
West Interior Side Yard Depth (minimum)	5.5 metres (18.1 feet)
Rear Yard Depth (minimum)	5.0 metres (16.5 feet)
Parking spaces (minimum)	0.6 spaces per unit

All other regulations of the Residential R8 (R8-4) Zone apply. (Z.-1-122150)

B-19

B-20

B-21

### **B-22 the lands located on Wellington Street on the east side, between South Street and the Thames River**

The subject site is being bonused to facilitate a development design which includes a 26 storey (94m tall) mixed use building on the southeast corner of Wellington Street and South Street with 19 storeys of residential apartment use, 6 storeys, 4,650m<sup>2</sup> gross floor area of medical/dental, clinic and independent health facility use and 929m<sup>2</sup> of ground floor commercial retail which shall be implemented through a development agreement as described in Appendix "E" attached to the Staff Report dated June 20, 2013 in return for the provision of the following services facilities and matters:

- underground parking spaces;
- placement of the building at the corner of Wellington and South Streets, with the design at its skyline to provide visual signals that both define and distinguish the Gateway area, and building facades using a composition of high quality materials including glass, metal panel, and stone veneer in proportions that, with minor variations at the City's discretion, match the elevations shown in Schedule "1", so the

- architecture of the building itself creates the Gateway;
- utilization of a non-residential podium base with multiple step-backs, and above it a residential point tower, to have regard for the impact of the proposed development on year-round sunlight conditions on adjacent properties and streets and to ensure the form and design of the new development complements the river valley and parklands located adjacent to the site;
- orientation of the primary building and unit entrances into the ground floor commercial spaces so that they face and open onto the Wellington street frontage in order to provide for the enhancement of the pedestrian environment;
- a green roof atop the building podium portions to support innovative and environmentally sensitive development;
- two publicly accessible pedestrian connections through the site, one from Wellington Street and one from South Street to the public open space bordering the Thames River to minimize the obstruction of views of natural features and to provide for improved access to riverside public open space supplementary to any parkland dedication requirements; and,
- landscaping enhancements including financial contribution therefor that are above City standards, of the abutting South Street and Wellington Street road allowances, to achieve high quality landscaping and the creation of an attractive street edge at this strategic Gateway location, to provide for Green Streets as detailed in the SoHo Community Improvement Plan, for suitable locations to accommodate the planting of street trees, and to encourage aesthetically attractive residential developments through the enhanced provision of landscaped open space.

The following regulations apply with the approved site plan:

Lot Area (minimum):	0.47ha
Lot Frontage (minimum):	50m
Front Yard Setback (maximum):	1.0m
Exterior Side Yard – (maximum): from the future Bus Rapid Transit road widening	1.0m
Lot Coverage (%) (maximum):	78%
Height (maximum):	94.0m
Density – units per hectare (maximum):	354
Parking:	510
Bicycle Parking	36
Rear and Interior Side Yard Setbacks (minimum)	0.0m

The cumulative impact of using this bonusing provision shall result in a density not greater than 354 units per hectare and a building height not greater than 94 metres. (Z.-1-132208)

**B-23 Lands within the area, bounded by Waterloo Street on the east, South Street on the north, the limit of the Open Space designation to the south, and the limit of the Main Street Commercial Corridor designation on Wellington Street to the west excluding the lands municipally known as 293 and 299 South Street.**

The subject site is being bonused to facilitate a development design which includes an 18-storey (67m tall) residential building on the southwest corner of Waterloo Street and South Street with 78 retirement lodges and 130 nursing home care units, 65 stacked townhouse dwelling units (15m tall) along South Street with a maximum of 1000m<sup>2</sup> of convenience commercial uses which include Convenience stores, Convenience service establishments, Financial institutions, Personal service establishments, Bake shops, Commercial schools, Florist shops, and Restaurants, at ground level without a drive through and a place of worship use which

shall be implemented through a development agreement as described in Appendix “E” attached to the Staff Report dated June 20, 2013 in return for the provision of the following services facilities and matters:

- secured bicycle parking spaces and temporary bicycle parking spaces for a total of 150 bicycle parking spaces;
- orientation of buildings to the street so that all principal facades of all units face the street and include active building elements such as primary entrances, porches, canopies and windows in order to provide for the enhancement of the pedestrian environment;
- utilization of a podium base with multiple step-backs and a residential point tower above on the building abutting the northeast corner so as to have regard for the impact of the proposed development on year-round sunlight conditions on adjacent properties and streets and to ensure the form and design of the new development complements the river valley and parklands located adjacent to the site;
- building facades using a composition of high quality materials including glass, metal panel, and stone veneer in proportions that, with minor variations at the City’s discretion, match the elevations shown in Schedule “2”, and townhouse facades that vary in their projection and recession along South Street so that they do not appear as one solid plane, so as to ensure a high standard of design for buildings to be constructed in this prominent location at the perimeter of the Downtown, near the edge of the river valley;
- a green roof atop the parking structure and building podium portions to support innovative and environmentally sensitive development;
- a publicly accessible pedestrian and sight line connection through the site from South Street to the public open space bordering the Thames River to minimize the obstruction of views of natural features and to provide for improved access to riverside public open space supplementary to any parkland dedication requirements;
- provision of enhanced landscaped open space at street level and along the Thames Valley corridor to support the provision of common open space that is functional for active or passive recreational use; and,
- landscaping enhancements including financial contribution therefor that are above City standards, of the abutting South Street and Waterloo Street road allowances, to provide for Green Streets as detailed in the SoHo Community Improvement Plan, for suitable locations to accommodate the planting of street trees, and to encourage aesthetically attractive residential developments through the enhanced provision of landscaped open space.

The following regulations apply with the approved site plan:

Lot Area (minimum):	0.87ha
Lot Frontage (minimum):	118m
Front Yard Setback (minimum):	1.0m
Exterior Side Yard (minimum):	1.0m
Lot Coverage (%) (maximum):	46%
Height (maximum):	67.0m
Density – units per hectare (maximum):	275
Parking:	232
Bicycle Parking	150 (secure)

The cumulative impact of using this bonusing provision shall result in a density not greater than 275 units per hectare and a building height not greater than 67 metres. (Z.-1-132209)

**B-24 1057 Richmond Street, 1059 Richmond Street, 1061 Richmond Street**

Increased residential density, up to a total of 9 dwelling units within the existing building at 1061 Richmond Street, will be permitted by this bonus zone in return for heritage preservation by designating 1061 Richmond Street, under Section 29(1) of the Ontario Heritage Act, and also in return for enhanced landscaping

and site design, by constructing a masonry wall, of no more than 1 metre (3.28 ft) in height, matching the materials and architectural character of the existing building located at 1061 Richmond Street, to provide for screening and the creation of a built street edge along the majority of the Richmond Street frontage and by allowing for no vehicular access to the site from the municipal laneway adjacent to the west. Site design will be in accordance with the conceptual site plan shown in the bonusing clause of any development agreement for this site.

a) Permitted Uses:

- i) Dwelling units in existing place of worship.

b) Regulations

- |       |   |                                 |
|-------|---|---------------------------------|
| i)    | Number of Dwelling Units<br>(Maximum)   | 9 units                         |
| ii)   | Number of bedrooms per dwelling unit<br>(Maximum)   | 3                               |
| iii)  | Parking Area Setback from east lot line<br>(Minimum)  | 6.0m (19.7 feet)                |
| iv)   | Parking Area Setback from west<br>and south lot line<br>(Minimum)   | 1.5m (4.9 feet)                 |
| v)    | Parking for Dwelling Units<br>(Minimum)   | 1.2 spaces per<br>dwelling unit |
| vi)   | Landscape Open Space<br>(Minimum)   | 30%                             |
| vii)  | Density<br>(Maximum)  | 50 units per hectare            |
| viii) | The parking area shall be located south of the southernmost portion of the existing place of worship building;  |                                 |
| ix)   | No part of the yard located to the west of the existing place of worship building at 1061 Richmond Street shall be used for any purpose other than landscaped open space;               |                                 |
| x)    | Permitted uses are confined to the existing place of worship building at 1061 Richmond Street on the date of the passing of the By-law.<br>(Z.-1-132252 OMB Decision November 26, 2013) |                                 |

**B-25 100 Fullarton Street and 475 Talbot Street**

This bonus zone is intended to facilitate a development design which includes a 33-storey (108.15m tall) mixed-use apartment building; with a podium base accommodating commercial/retail space on the ground floor and a mix of structured parking, residential, and/or office space above the ground floor; which shall be implemented through a development agreement in return for the provision of the following services, facilities and matters:

- A point-tower building design which, with minor variations at the City's discretion (such variations may include the extension of the podium northward), matches the Site Plan, Elevations, Sections and Renderings shown in Schedule "1" attached to this amending by-law, and includes an architecturally differentiated base, middle and top:
- With the base consisting of the portion of the façades between the ground floor and the top of the 6<sup>th</sup> floor with a maximum height of six (6) storeys; positioned at the front and exterior lot lines at the corner of Talbot Street and Fullarton Street; incorporating architectural detail which creates a prominence on the Talbot/Fullarton Street corner; including retail uses at street level abutting the Fullarton and Talbot Street frontages, with a minimum ceiling height of 3.6 metres and transparent glazing of at least 2.5 metres in height, for 60% or more of the frontages; with entrances to each retail unit provided,

where possible, directly to the street, flush with the sidewalk grade; including permanent canopies or architectural elements projecting above pedestrian entrances at street level; and a mix of structured parking, residential, and/or office space above the ground floor; with any above-grade structured parking being screened with a variation in materials and colours;

- With the middle portion consisting of the portion of the façades between the top of the base and the top of the 32<sup>nd</sup> floor; clad primarily in glass window-wall panels, and employing balcony design which creates articulation and variation in the facades; includes variation in the massing of the tower through building step-backs; with a combination of vegetated green roof and outdoor amenity space incorporated into the building step-backs and terraces;
- With the top consisting of the portion of the façades above the top of the 32<sup>nd</sup> floor; employing building step-backs on the 33<sup>rd</sup> floor to provide for outdoor terraces; employing further step-backs above the 33<sup>rd</sup> floor to articulate the top of the building; using attractive materials and architectural design to screen all mechanical elements located above the 33<sup>rd</sup> floor; using high-quality building materials and incorporating decorative lighting elements to create an aesthetically pleasing cap;
- 2-levels of below grade parking (minimum 65 spaces);
- Locating waste and recycling facilities within the proposed building screened from views of adjacent properties;
- Providing barrier-free access to all floors (to the extent feasible to facilitate access and use); and,
- The provision of public art.

The following regulations apply within the bonus zone:

i)	Height (Maximum)	110.0 metres
ii)	Density (Maximum)	1,155 units per hectare
iii)	All Yard Setbacks (Minimum)	0.0 metres (From existing road allowance) (Z.-1-142269)

### **B-26 670 Garibaldi Avenue**

This bonus zone is intended to facilitate a development design which includes a 6-storey (21 metre) apartment building with underground parking and a total of 138 units, which shall be implemented through a development agreement in return for the provision of the following services, facilities and matters:

- A 6-storey apartment building design that is aesthetically pleasing, functional and pedestrian supportive which, with minor variations at the City’s discretion matches the conceptual plan shown in Schedule “1” of the amending By-law, and includes:
- Exterior construction materials including brick facade and articulation as shown in Schedule “1”.
- Minimum of One (1) level of below grade parking with a minimum of 100 vehicular parking spaces.
- Berm and dense landscaping along the Garibaldi Avenue frontage, south of the location of the Civic Space, to maximize privacy between the proposed parking area on the subject lands and the abutting residential properties to the west.
- Minimal yard depths from the apartment building to Adelaide Street North to allow the maximum achievable yard depth possible from the apartment building to Garibaldi Avenue.
- Pedestrian connections from the subject property to the abutting commercial lands, existing sidewalks and future municipal pathway.
- Dedication of a portion of land, with an area of approximately 500 square



metres, to be used as a Civic Space abutting the pedestrian pathway with a frontage on Garibaldi Avenue. The total land area of this Civic Space may be in combination with the lands at 660 Garibaldi Avenue.

- The Civic Space shall be improved to the satisfaction of the Managing Director, Planning and City Planner at no cost to the City of London prior to its dedication.

The following regulations apply within the bonus zone:

- |     |                                      |                      |
|-----|--------------------------------------|----------------------|
| i)  | Density<br>(Maximum)                 | 99 units per hectare |
| ii) | Height<br>(Maximum)<br>(Z.-1-142290) | 21 metres            |

**B-27 545 Fanshawe Park Road West**

The increase in height and density to the zoning of the subject lands shall be permitted in return for enhanced urban design elements consisting of 2 residential apartment buildings and a podium base which is generally in keeping with the illustrations attached as Schedule "1" of the amending by-law, which shall be implemented through a development agreement in return for the provision of the following services, facilities, and matters:

- A development design which includes two point tower forms with a common podium in the form of two-storey townhouses comprised of:
  - a maximum of 235 residential dwelling units within the two point tower forms (Tower A: 115 units; Tower B: 120 units);
  - a maximum of 15 residential dwelling units within the two-storey townhouses (Tower A: 7 units; Tower B: 8 units);
- Building orientation toward the Fanshawe Park Road West corridor;
- Building elevations that have been divided into a series of modulated components that are defined by complementary changes in articulation and cladding materials;
- A variation of building materials and the use of cornices that define the major changes in cladding materials on the elevations;
- The inclusion of a base, middle and cap with a base consisting of a two-storey townhouse form, a middle that consists of 14-storeys above the base for Tower 'A' and 13-storeys above the base for Tower 'B', and a one-storey cap above the middle;
- A mix of underground and surface parking spaces that are located in the rear yard;
- Enhanced landscaping features including a centralized park feature and an outdoor amenity area integrating the existing open space setting;
- Ground floor and second floor private amenity space; and,
- Rooftop patios

- |      |  |                         |
|------|--|-------------------------|
| a)   | Regulations                                    |                         |
| i)   | Building Height<br>(Maximum)                   | 60 meters               |
| ii)  | Number of Dwelling Units<br>(Maximum)          | 250 units               |
| iii) | Density<br>(Maximum)                           | 246 units per hectare   |
| iv)  | Front Yard Setback<br>(Minimum)                | 5.0 meters (16.4 feet)  |
| v)   | Interior Side Yard Setback (East)<br>(Minimum) | 12.4 meters (40.8 feet) |

vi) Interior Side Yard Setback (West) (Minimum)	9.2 meters (30.2 feet)
vii) Rear Yard Setback (Z.-1-142294)	9.5 meters (31.2 feet)

### **B-27 96 Ridout Street South**

This bonus zone is intended to facilitate a development design which includes a 22-storey (72m tall) residential apartment building; with a 3-storey podium base accommodating eight apartment units and structured parking; with the street-facing podium facades designed to appear as stacked townhouses; and up to 183 residential apartment units in the apartment tower from floors 4 to 22; which shall be implemented through a development agreement in return for the provision of the following services, facilities and matters:

- A building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, matches the Site Plan, Roof Top Terrace Plan, and Elevation Drawings shown in Schedule “1” of the amending by-law, and includes an architecturally differentiated base, middle and top:
- With the base consisting of the portion of the façades between the ground floor and the top of the 3rd floor; positioned toward the front lot line along Ridout Street, generally in line with the established street wall; incorporating architectural detail which creates a prominence on the Ridout Street frontage; including apartment units which appear as “stacked townhouses” along the street-facing facade; with entrances to each “stacked townhouse” unit provided directly to the street; including architectural elements projecting above pedestrian entrances at street level; and above-grade structured parking which is screened with a variation in materials and colours; with specific architectural detail provided to pronounce the primary building entrance along the northern building facade to accentuate the entrance;
- With the middle portion consisting of the portion of the façades between the top of the base and the top of the 20<sup>th</sup> floor; including two separate columns of glazed window-wall on both the north and south facades, and, one column on both the east and west facades; employing balcony design which creates articulation and variation in the facades; includes substantial tower step-backs at the 4<sup>th</sup> floor; with an outdoor terrace on the 4<sup>th</sup> floor which includes a combination of soft landscaping, passive amenity space, communal gardens, and vegetated green roofing;
- With the top consisting of the portion of the façades above the top of the 20<sup>th</sup> floor; employing building step-backs on the 21<sup>st</sup> floor to provide for outdoor terraces; employing further step-backs above the 21<sup>st</sup> floor to articulate the top of the building; using window-wall columns to break up the visual massing of the facades; using attractive materials and architectural details to screen all mechanical elements located above the 22<sup>nd</sup> floor; using high-quality building materials and incorporating decorative lighting elements to create an aesthetically pleasing cap;
- Locating waste and recycling facilities within the proposed building screened from views of adjacent properties; and
- Providing barrier-free access to all floors (to the extent feasible to facilitate access and use);

The following regulations apply within the bonus zone:

i)	Height (Maximum)	73.0 metres (239.5 feet)
ii)	Density (Maximum)	322 units (130 units per hectare per acre)
iii)	Front Yard Setback For Podium Component of Building (Minimum)	5.5 metres (18.0 feet)

iv)	Front Yard Setback For Tower Component of Building (Minimum)	10.9 metres (35.8 feet)
v)	South Interior Yard Setback (Minimum)	4.6 metres (15.1 feet)
vi)	North Interior Yard Setback for Podium Component of Building (Minimum)	6.4 metres (21.0 feet)
vii)	North Interior Yard Setback for Tower Component of Building (Minimum)	19.2 metres (63.0 feet)
viii)	Landscaped Open Space (%) (Minimum)	17.0
ix)	Lot Coverage (%) (Maximum) (Z.-1-142316)	56.0

**B-28 Number not used**

**B-29 21 Central Avenue and 585 & 581 Waterloo Street**

This bonus zone is intended to facilitate a development design which includes a 6-storey (20m tall) residential apartment building with a maximum of 35 residential apartment units and underground parking and shall be implemented through a development agreement in return for the provision of the following services, facilities and matters:

- A building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, is generally consistent with the Site Plan and Illustrations shown in Schedule “1” of the amending by-law, and includes an architecturally differentiated base, middle and top:
- With the base consisting of the portion of the facades between the ground floor and the top of the 3<sup>rd</sup> floor; positioned at, or near, the front and exterior lot lines along Waterloo Street and Central Avenue, generally in line with the established street wall; incorporating architectural detail which creates a prominence at the corner of Waterloo Street and Central Avenue; including apartment units which appear as “townhouses” spanning the length of both the Waterloo Street and Central Avenue street-facing facades; with individual entrances to each “townhouse” unit provided directly to the street; including architectural elements or canopies projecting above pedestrian entrances at street level; including patios and/or amenity space in front of each “townhouse” unit; with specific architectural detail provided to pronounce the primary building entrance along the northern building façade and accentuate the entrance;
- With the middle portion consisting of the portion of the facades between the top of the base and the top of the 4<sup>th</sup> floor; including building step-backs at the 4<sup>th</sup> floor with outdoor terraces provided which may include a combination of soft landscaping, passive amenity space, and vegetated green roofing;
- With the top consisting of the portion of the facades above the top of the 4<sup>th</sup> floor; employing building step-backs on the 5<sup>th</sup> floor to provide for outdoor terraces; incorporating an internal communal amenity space with the exterior facades comprised of primarily transparent glazing in order to provide for views into and out of the building to animate the upper floors of the building; using attractive materials and architectural details to screen all mechanical elements located above the 6<sup>th</sup> floor;
- Locating waste and recycling facilities within the proposed building screened from views of adjacent properties and the public realm;

- Locating all parking facilities below grade within the proposed building screened from views of adjacent properties and the public realm;
- Providing barrier-free access to all floors (to the extent feasible to facilitate access and use);
- Reclamation of suitable building materials such as windows, doors, mouldings, columns, bricks, etc. for potential reuse in the new building on the site or as replacement components for other buildings in the neighbourhood which require repair and restoration over time is required.

The following regulations apply within the bonus zone:

a) Permitted Uses:

Apartment Buildings

b) Regulations:

i)	Lot Area (Min.)	1,600m <sup>2</sup>	(0.4 ac.)
ii)	Lot Frontage (Min.)	40m	(131 ft.)
iii)	Front Yard Depth (Min.)	0.0m	(0 ft.) (From existing road allowance)
iv)	Interior Side Yard (Min.)	9.3m	(30.5 ft.)
v)	Exterior Side Yard Depth (Min.)	0.0m	(0 ft.) (From existing road allowance)
vi)	Rear Yard Depth (Min.)	0.0m	(0 ft.)
vii)	Landscaped Open Space (Min.)	25%	
viii)	Lot Coverage (Max.)	55%	
ix)	Height (Max.)	20m	(66 ft.)
x)	Density (Max.) (Z.-1-14234)	210 Units Per Hectare	

**B- 30 2118 Richmond Street**

The subject site is being bonused for:

- Enhanced urban design features;
- Underground parking and related additional open space; and,
- The planting of nine mature shade trees (minimum diameter at breast height (dbh) of 11 cm) as part of the landscaping plan between the south building face and Sunningdale Road East.

The development shall be in accordance with the site concept and elevations attached as Schedule “1” of the amending By-law which includes an apartment building with a maximum height of 33 metres with eight, three-storey townhouses integrated into the base of the building adjacent to Sunningdale Road East, for a total maximum of 107 units (123 units per hectare). The apartment building component of the structure shall be restricted to the north portion of the property, thereby locating the maximum intensity away from the single detached dwellings within the residential neighbourhood south of Sunningdale Road East. The development shall specifically incorporate the following key components:

## Building

- a building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, matches the site concept and elevation drawings shown in Schedule “1” of the amending By-law;
- the proposed building has an “L” shaped configuration with townhouses located along the Sunningdale Road East frontage and the apartment building portion located along the Richmond Street frontage;
- the building is located in close proximity to the Richmond Street property line and as close as possible to the Sunningdale Road East property line given the setback for the oil pipeline and the need to provide a landscaped amenity area between the building and the street;
- the townhouses facing Sunningdale Road East include individual accesses from each townhouse unit directly out the landscaped open space located along Sunningdale Road East;
- individual entrances to the apartments are located on the ground floor of the apartment building adjacent to Richmond Street and function as front doors rather than patio doors;
- the main entrance into the apartment building is located on the west façade facing Richmond Street;
- The building includes a differentiated base, middle and top:
  - with the base consisting of the portion of the façade between the ground floor and the top of the third floor; design elements from the three storey townhouses being carried through the first three floors of the apartment building.
  - with the middle consisting of the portion of the façade between the top of the base and the 8<sup>th</sup> floor, including material changes, large windows and balconies, and a column of windows following the width of the lobby.
  - with the top consisting of the portion of the façade above the top of the 8<sup>th</sup> floor, including the southern portion of the top of the building as a window wall set back on the 9<sup>th</sup> floor and set back further on the 10<sup>th</sup> floor.

## Site

- Landscape plans include a minimum of nine mature trees with a minimum dbh of 11 cm between the building face and the south property line in order to reduce the visual impact of the new building on the single detached dwellings located on the south side of Sunningdale Road East;
- All parking is located behind and below the building;
- A limited amount of parking in the rear of the building in order to accommodate a landscaped amenity area.

Notwithstanding anything in the By-law to the contrary the following regulations shall apply:

i)	Lot Area (minimum)	0.8 hectares (1.98 acres)
ii)	Lot Frontage (minimum)	60.0 metres (196.8 feet)
iii)	Front Yard Depth (Sunningdale Road East) (minimum)	16.0 metres (52.5 feet)
iv)	Exterior Side Yard Depth (minimum)	8.2 metres (26.9 feet)
v)	Rear Yard Depth (minimum)	8.7 metres (28.8 feet)
vi)	Interior Side Yard Depth (minimum)	1.2 metres (3.9 feet) metres (9.8 feet) of main building height or fraction thereof, but in no case less than 4.5 metres (14.8 feet)

		6.0 metres (19.7 feet) plus 1.0 metres (3.3 feet) per 1.0 metre (3.3 feet) in height for all portions of a building above 6.0 metres (19.7 feet) in height where the subject site abuts lands zoned Residential R1 or Residential R2
vii)	Landscaped Open Space (minimum)	55 %
viii)	Coverage (maximum)	30%
ix)	Height (maximum)	33.0 metres (108.3 feet)
x)	Density (maximum) (Z.-1-142350)	123 units per hectare

### **B-31**

#### **B-32 – 690, 696, 698, 700 King Street, 372 Lyle Street, 701, 729 Dundas Street**

The subject site is being bonused for enhanced urban design, underground parking, the promotion of transit usage and the revitalization of the Old East District. This bonus zone is intended to facilitate a development design which includes the maintaining of 2 existing apartment buildings (21-storeys and 24-storeys) municipally known as 700 King Street and approved through a previous Zoning By-law amendment, and a 21-storey apartment building located at the southwest corner of the subject site which shall be implemented through a development agreement in return for the provision of the following services, facilities and matters:

A building design for a 21-storey apartment building, located at the southwest corner of the site which, with minor variations at the discretion of the Managing Director, Planning and City Planner, matches the Elevation Drawings shown in attached Schedule “1” of the amending by-law, and be constructed with: a 3-storey podium base accommodating residential units which have direct pedestrian access to the City sidewalk along Lyle Street; canopies used to differentiate and identify these individual unit access points at the ground level; an additional setback on the south elevation above the 9<sup>th</sup> storey to provide a break in the elevation, reducing the scale of the overall building, and creating a positive pedestrian interface along King Street and is consistent in massing with the existing apartment buildings directly to the east; the northern elevation includes balcony overhangs, changes in materials and colours and includes three columns of windows to help break up long flat elevations.

The following regulations apply with the bonus zone:

Height (maximum):	80.0 metres (262.5 feet)
Density (maximum):	600 units per ha (243 units per acre)
Parking (minimum):	555 spaces
Secure Bicycle Parking spaces (minimum)	280 spaces
Front Yard and Exterior Side Yard (maximum)	4.5 metres (14.8 feet)
Setbacks from King Street and Hewitt Street (minimum)	0.0 metres
Balcony Encroachment in all yards	1.5 metres (4.9 feet) provided the projection is no closer than 1.5 metres (4.9 feet) to the lot line.

Dwelling units are permitted on the whole of the first floor including the frontage on King Street, Lyle Street and Hewitt Street, but not including Dundas Street frontage.

The cumulative impact of using this bonusing provision shall not result in a density greater than 600 units per hectare (243 units per acre) and a building height greater than 80.0 metres (262.5 feet). (Z.-1-152397)

### **B-33 505-511 Talbot Street and 94 Dufferin Avenue**

This bonus zone is intended to facilitate a development design which includes a 29-storey (97m tall) building with a maximum density of 990 units per ha and three levels of underground parking and shall be implemented through a development agreement in return for the provision of the following services, facilities and matters:

1. A building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, is consistent with the Site Plan and Illustrations shown in attached Schedule "1" of the amending by-law.
2. Architectural Continuity with Heritage Designated Property  
The massing and site-specific design of the development provide for continuity and harmony in architectural style with the heritage designated place of worship abutting the subject property to the north, achieved by:
  - a. Establishing a building podium height consistent with the height of the roof peak of the place of worship;
  - b. Using the height of the place of worship's peaked Talbot Street main entrance and its building setback distance from the street as datum lines for the canopy height over the building's main residential entrances and the building's setback from Talbot Street;
  - c. Setting back the building's podium approximately 5.3m from the northern lot line and 13m from the easterly lot line, to provide space between the new structure and the heritage place of worship and to provide new light into the windows of the place of worship's south façade and apse;
  - d. Establishing a new heritage courtyard within the space created by (c) above, to provide a common open space that is functional for active and/or passive recreational use. This space will be landscaped, include seating, and will include interpretive signage relating to the heritage attributes and history of the buildings that are currently located on the site;
  - e. Using light brick masonry on the podium of the building that is compatible with the place of worship exterior cladding and other nearby buildings;
  - f. Employing a street façade design for the podium that incorporates brick material in discrete units to echo the rhythm of the building massing that currently exists along Talbot Street;
  - g. Salvaging and reclaiming significant architectural artefacts and building materials from the existing buildings on the subject site, for incorporation into the new building's interior and/or exterior; some of these materials may be incorporated into the heritage courtyard.
3. High Design Standards  
The site-specific building design to be constructed in this prominent Downtown location, promoted by:  
Podium Features
  - a. A podium height which is equal to, or lesser, than the height of the peak of the adjacent heritage place of worship;
  - b. Podium design sympathetic to the heritage context of the streetscape, as described above;
  - c. Generous floor-to-ceiling heights on the ground floor, with interior spaces visible through window walls from outdoors, with overhead projecting canopies lining the pedestrian-oriented street wall frontages;
  - d. Abundant use of brick and clear glass materials;

- e. Prominence of the Dufferin Ave-Talbot Street corner, expressed through the use of entrances, clear glazing and architecturally integrated canopies;
- f. Recessed vehicular entrances located well away from the street corner and using driveway materials that continue the sidewalk treatment and character;
- g. Punctuation of the upper storey podium walls with windows clad with frosted glass, allowing for the internal illumination of the parking garage and providing nighttime lighting on the street and a glow that offers a sense of occupancy.

#### Tower Features

- a. Step-back of the tower, from the podium, to the greatest extent possible on the street facing facades, given the limitations of the small lot size. This step-back is expressed in the regulations of the by-law;
- b. A point-tower form with tower floor-plate perimeter dimensions being relatively equal;
- c. Mitigation of building mass with a high proportion of glass materials and a relatively low proportion of exposed concrete or similar materials; the use of transparent curtain glass wall, running in vertical bands on each façade of the tower. Use of clear glass balcony barriers;
- d. Further mitigation of building mass by varying and articulating the plane of all facades.

#### Building Cap Features

- a. A visually interesting building top and distinguishable cap of a design quality that will contribute positively to London's skyline;
- b. The use of building step-back at the top storey, with mechanical penthouse completely concealed in the building's top storey;
- c. Significant amounts of clear glazing, and planned lighting features for the building cap.

#### 4. Site Landscaping/Public Realm

Landscaping used to enhance the appearance of building setback and yard areas by:

- a. Continuous planting in the at-grade green space west of the building;
- b. Raised concrete planters along Talbot Street and Dufferin Avenue, to define the interface of the subject site and the public right of way and to function as informal seating, as approved by the City through the site plan process;
- c. Shade trees in the raised concrete planters;
- d. Widened sidewalks and the incorporation of non-asphalt materials and paving patterns in driveways and drop-off areas as approved by the City through the site plan process;
- e. The heritage courtyard feature, as described above.

#### 5. Underground Parking

- a. Three levels of underground parking to allow for a podium building height that is not greater than the height of the heritage designated place of worship to the north;
- b. Provision of dedicated, sheltered and secure bicycle parking.

#### 6. Sustainability Features

- a. Provision of a green roof on portions of both the podium and the tower roofs;
- b. Provision of sustainability features incorporated into the building that will be equivalent to a LEED gold standard, to the satisfaction of the City Planner.



7. Public Art Contribution
  - a. The contribution of funding for public art in the amount of 1% of the overall project construction value, up to a total of \$250,000.
  - b. Such public art will be established through a process that is consistent with the City’s public art policy as applicable, to the satisfaction of the Manager of Culture and the City Planner, and ensuring input from the developer.

The following regulations apply within the bonus zone:

- b) Regulations:
  - i) Density: 990 units per hectare (Maximum)
  - ii) Height 97 metres (Maximum)
  - iii) Bicycle Parking 0.4 spaces per residential unit (Minimum)
  - iv) Residential Component of Buildings N/A
  - iv) Setback of tower portion of building from lot lines (minimum):
    - a. 4.5 m setback from the northerly property line;
    - b. 3 m setback from the easterly property line;
    - c. 0.7 m setback from the southerly property line;
    - d. 4.5 m setback from the westerly property line.  
(Z.-1-152416)

**B-34 356 Dundas Street**

This bonus zone is intended to facilitate a development design which includes a 6-storey residential apartment building with a maximum of 69 residential apartment units at an increased density up to 558 units per hectare, front yard setback of 0 metres and a minimum rear yard depth of 1.5 metres in return for the provision of the following services, facilities and matters which are described in greater detail in the recommended by-law:

A building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, is consistent with the Site Plan and Illustrations shown in attached Schedule “1” of the amending by-law, except where the below criteria require more specific detail than shown in Schedule “1”, and includes the following:

- i. A 6-storey building design which responds to the existing built form on the abutting lands to the east and west of the subject site. This includes continuing the datum line established by the existing built form to the west of the site and a height and massing which is compatible with the West Woodfield Heritage Conservation District abutting the subject site immediately to the north. The building itself shall maintain a “U” shaped building footprint with the creation of a common outdoor landscaped area located within the interior portion of the “U”.
- ii. Other specific features include a minimum front building façade height on the ground-floor of 3.65 metres (12 feet) across the entire length of the building frontage to establish the building “base” and a glass canopy located above the principal front building entrance extending approximately one-third the length of the building façade. There shall also be a horizontal cornice line located above the glass canopy which visually separates the ground-floor front façade and the upper building storeys and distinguishes the “middle” of the building from the ground-floor “base”. A masonry building material such as brick or block will be used on the façade of the base while avoiding the use of field stone or similar material.
- iii. The use of brick on the exterior façade on the “middle” of the building extending the entire length of the front building elevation between floors 2-4 (inclusive) beginning above the cornice line and the inclusion of decorative moulding between floors 4 and 5 to create visual interest and distinguish the “top” of the building from the “middle”.

- iv. Alternative window treatments shall be used to provide an enhanced appearance. The use of clear vision glass in all of the building's openings on the entire building frontage between floors 2-6 (inclusive) will be required. The use of spandrel glass panels will be avoided. Clear vision glass will be required in all of the transom windows on the base of the building and where the windows are associated with internal facilities/rooms that promote active uses.
- v. A minimum of three glass Juliette balconies per floor between floors 2-5 (inclusive) and the inclusion of decorative moulding and decorative bracket detail at the top of the 6<sup>th</sup> floor to cap the "top" of the building.

The following regulations apply with the bonus zone:

Density (maximum):	558 units per hectare (222 units per acre)
Front Yard Setback (minimum)	0 metres (0 feet)
Rear Yard Setback (maximum) (Z.-1-152424)	1.5 metres (4.92 feet)

### **B-35 89 York Street**

This bonus zone is intended to facilitate a 10 storey mixed-use development design which includes commercial and retail floorspace, and 18 residential units, with a density of 754 units per hectare, in return for the provision of the following services, facilities and matters:

1. A building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, is consistent with the Site Plan and elevations shown in attached Schedule "1", except where the below criteria require more specific detail than shown in attached Schedule "1" of the amending by-law.
2. A high quality of building design, including the following:
  - i. Abundant use of transparent glazing on the ground and second floors;
  - ii. A high proportion of glass materials and natural stone and a relatively low proportion of exposed concrete or similar materials;
  - iii. Provision of an outdoor area on the second storey in the form of a patio or terrace which faces the street to provide: articulation of the front façade, shelter for the ground floor entry, and activation of the street edge along York Street;
  - iv. Parking provided by a mechanical parking system to be located on the third storey (spanning a maximum of 1.5 storeys in height).
  - v. Inclusion of floor to ceiling translucent glazing on the third storey to screen the parking level which may be backlit or have graphics etched on or placed behind, to provide a visibly interesting and artistic treatment, and animation of the street edge;
  - vi. All parking spaces provided are to be accessible parking spaces;
  - vii. The use of solar panels on the 10<sup>th</sup> floor roof;
  - viii. Sustainable development features including green roof treatments on the fifth and tenth levels; and
  - ix. The visually interesting angled and stepped back building cap that will contribute positively to the Downtown London skyline;
3. The following regulations apply within the bonus zone:
  - i. Density (Maximum): 754 units per hectare (305 units per acre)
  - ii. Height (Maximum): 44.3 metres (145.3 feet)
  - iii. Residential Portion Setback (Minimum): 0 metres (0 feet)

- iv. Notwithstanding Section 4.19, the parking provided by the mechanical stacked parking system shall be exempt from the size requirements of Section 4.19 2) *Dimensions of Parking Spaces*, and no access aisles are required as per Sections 4.19 2.1) *Access Aisles*, and 4.19 6) (j) *Access Aisles for Parking Spaces with Disabilities* of the Z.-1 Zoning By-law. (Z.-1-162446)

**B-36 50 King Street (Z.-1-162447)**

This bonus zone is intended to facilitate a development design which includes a mixed-use apartment building with a maximum height of 95 metres (28-storeys) and a maximum density of 750 units per hectare, which shall be implemented through a development agreement in return for the facilities, services and matters outlined in the site-specific “Bonusing Criteria” attached hereto as Schedule “1” to the amending by-law.

The following regulations apply within the bonus zone:

- a) Regulations:
  - i) Density (Max.) 750 units per hectare
  - ii) Height (Max.) 95 metres
  - iii) Setback for residential component of building (Min.) 0 metres

**B-37 455 Clarence Street**

This bonus zone is intended to facilitate a development design which includes a 32-storey mixed-use apartment building with a maximum of 182 residential apartment units and 700m<sup>2</sup> of retail space on the ground floor. The apartment will exist at a density up to 1180 units per hectare, height of 105 metres, and a setback of 4.5 metres for any residential use above 15 metres in return for the provision of the following services, facilities and matters:

A building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, is consistent with the Site Plan and Illustrations shown in attached Schedule “1” of the amending by-law, except where the below criteria require more specific detail than shown in attached Schedule “1”, and includes the following:

- i) A podium design which is sensitive to, and responds to, the surrounding heritage features and implements the design criteria of the Downtown Heritage Conservation District.
- ii) A high quality of building design, including the following design elements:
  - Podium
    - i. The use of stone veneer and granite, similar to the materials used on the historic office building across the street.
    - ii. Generous double height transparent windows at the base of the podium along Clarence Street in order to activate the street frontage.
    - iii. A projecting glass canopy along the length of the podium, along the Clarence Street frontage, in order to provide weather protection to pedestrians.
    - iv. Large bay windows with transparent glass above the main floor to help soften and break up the stone veneer walls while providing light into the parking garage and animating the upper levels of the podium.
  - Mid-Block Connection
    - i. A covered pedestrian alley, along the south property line, with active storefronts in order to implement the “laneways connection”

transformational project found in *Our Move Forward: London's Downtown Plan*.

#### Tower

- i. A point tower form with the massing of the building mitigated by a high proportion of glass materials and a relatively low proportion of panels or similar materials.
- ii. A variation of material colours in order to add interest and break up blank wall sections.
- iii. Continuous window wall within groupings of vertical window sections accented by a colour and projecting (roughly 1ft) in order to provide a unique architectural element to the building and break up the large flat portion of wall on the Clarence Street façade.
- iv. A visually interesting building cap that will contribute positively to the Downtown London skyline.

#### Streetscape

- i. High quality design and landscaping, within the City Boulevard as required to achieve the goals of the Downtown Master Plan.

#### Public Art

- i. The provision of public art, to be administered in accordance with the City of London Art Policy in the amount of 1% of construction value up to \$250,000

The following regulations apply with the bonus zone:

- |      |                                       |   |
|------|---------------------------------------|---|
| i)   | Density<br>(Maximum)                  | 1180 units per hectare<br>(475 units per acre)  |
| ii)  | Height<br>(Maximum)                   | 105 metres<br>(335 feet)  |
| iii) | Podium Height<br>(Maximum)            | 19.5 metres   |
| iv)  | Residential Component                 | The required setback for the residential portion of Building of the building shall be a minimum of 4.5 metres (14.76 feet) for any portion above 15 metres (49.2 feet). |
| v)   | Parking<br>(minimum)<br>(Z.-1-162488) | 0 parking spaces  |

### **B-38 100 Fullarton Street, 475-501 Talbot Street and 93-95 Dufferin Avenue**

This bonus zone is intended to facilitate a development design which includes three new mixed-use apartment buildings ranging from 9 to 38 storeys (max. 129m tall) in height, with a maximum density of 1200 units per hectare across the overall site. It is intended that the proposed development will occur in a phased manner over time with Phase One including a 9-storey (39m) building with 92 residential units and 610m<sup>2</sup> of commercial space, with Phase Two including a 38-storey (128.85m) building with 349 residential units and 660m<sup>2</sup> of commercial space, and Phase Three including a 29-storey (101.5m) building with 262 residential units and 660m<sup>2</sup> of commercial space. The proposed development shall be implemented through one or more agreements in return for the provision of the following services, facilities and matters:

1. A building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, is consistent with the Site Plan and Illustrations attached as Schedule "1" to the amending by-law (variations may include, but are not limited to, the reduction of above-grade structured parking in lieu of additional underground parking and/or the reduction of the total number of parking spaces and variations in the proposed unit types).

## 2. High Design Standards

Specifically, the building design referred to in Clause 1 above, and shown in the various illustrations contained in attached Schedule "1" of the amending by-law, is being bonused for the following features which serve to support the City's objectives of promoting a high standard of design for buildings constructed in prominent locations such as the Downtown, as outlined in Chapter 11 of the Official Plan and the Downtown Design Guidelines:

### Overall Design Features

- a. A modern architectural design that utilizes vision glass and spandrel glass (window-wall) as the primary form of cladding, contains a low proportion of exposed concrete, and uses clear glass balcony barriers, all of which serve to mitigate the overall visual building mass and provide a light and refined appearance in the city skyline.

### Podium Design

- a. Incorporates architectural details that creates a prominent and distinctive base including ceramic panels skirting the face of the second and third floor terraces.
- b. A ground floor ceiling height that is appropriate for a range of commercial uses, with the façade comprised primarily of floor-to-ceiling vision glazing for views into and out of the building contributing to an animated streetscape.
- c. Separate and direct exterior entrances to commercial uses on the ground floor to animate the pedestrian realm from the city sidewalk.
- d. Permanent architecturally integrated canopies above the first floor to architecturally differentiate the building base and provide overhead protection from natural elements.

### Tower Design

- a. Point tower forms with floor-plates no greater than 1,000m<sup>2</sup>.
- b. Clad primarily in vision glass and spandrel glass (window-wall) panels.
- c. Includes horizontal projecting balconies and vertical precast end panels to create articulation and variation in the massing of the facades.

### Building Caps

- a. A visually interesting building top and distinguishable cap of a design quality that will contribute positively to London's skyline.
- b. The use of building step-backs and variation in massing to define the building cap with mechanical elements completely concealed within the overall architectural design of the top of the buildings.

## 3. Heritage Conservation (93-95 Dufferin Avenue)

The significant attributes of the built heritage resource located at 93 and 95 Dufferin Avenue shall be conserved in the following manner:

- a. The designation of heritage attributes (generally associated with the northerly and westerly facades) of the original building under Part IV of the Ontario Heritage Act.
- b. The complete retention, in-situ, of 93-95 Dufferin until such time as partial removal is necessary to facilitate Phase 3 of the proposed redevelopment.
- c. The incorporation of significant heritage attributes of the original building, including the northern and western facades in situ, into the overall design of Phase 3 of the new development as is depicted in attached Schedule "1" to the amending by-law.

4. Heritage Commemoration - Camden Terrace (479-489 Talbot Street)
  - a. The documentation and appropriate storage of original materials including brick and exterior masonry work to retain their integrity.
  - b. The construction of a commemorative monument which interprets a range of the significant heritage attributes of the Camden Terrace front facade in the manner documented in the January 4, 2016 "Heritage Overview Report" prepared by Stantec Consulting Ltd. and further described in the accompanying August 19, 2016 "Commemoration Overview" letter and shown in attached Schedule "1" to the amending by-law, and which generally includes the following features:
    - i. Multi-structure residential row house proportions including six buildings enclosed within three sections, with each containing two terrace residences with mirrored facades;
    - ii. With the individual row house sections containing the same proportion of bay windows, doors and upper-storey windows as the original structures.
    - iii. A uniformed setback from the public sidewalk.
    - iv. A uniformed eaves line.
    - v. The use of original building materials salvaged during the demolition of Camden Terrace including stone lintels and original yellow (London) brick.
    - vi. Other details such as pilasters, drip course, and cornice frieze, high transoms above front entry doors, and half-elliptical lintels on upper storeys.
  - c. The provision of plaques, interpretive signage and/or other commemorative items which relate to the heritage attributes of the site and includes the following subject matter: site history with an emphasis on 19th century character of the Talbot Street Corridor; the origins and construction of Camden Terrace; and, details regarding the deconstruction and reconstruction of the commemorative monument (facade replication).
  - d. The provision of clear glazing along the length of the Talbot Street building façade which is east of the commemorative monument so as to maintain public views to the monument in perpetuity.
5. Urban Plaza/Forecourt
  - a. The provision of a publically accessible urban plaza/forecourt along the Talbot Street frontage which is generally located in the vicinity of the primary entrance to the Talbot Street building (Phase 1), east of the commemorative monument as shown in attached Schedule "1" to the amending by-law.
  - b. The provision of two-storey vision glass panels along portions of the Talbot Street building facade that front the urban plaza/forecourt to maintain views to active ground floor uses and the commemorative monument.
6. Parking Strategy
  - a. A minimum of 3 levels of underground parking for Phase 1 and a minimum of 4 levels of underground parking for subsequent phases.
  - b. Any necessary above grade structured parking is to be located to rear of the proposed building(s) and screened from the street by commercial and/or residential uses.
7. Site Landscaping/Public Realm
 

Landscaping used to enhance the appearance of building setback and yard areas by:

  - a. A continuous row of planting along the western lot line of the property currently known as 93 Dufferin Avenue.
  - b. Raised concrete planters along Talbot Street and Dufferin Avenue

and Fullarton Street, to define the interface of the subject site and the public right of way and to function as informal seating, as approved by the City through the site plan process.

- c. Shade trees in the raised concrete planters.
- d. The incorporation of non-asphalt materials and paving patterns into pedestrian sidewalks and pathways adjacent to the proposed buildings as approved by the City through the site plan process.

8. Public Art Contribution

- a. The contribution of funding for public art in the amount of \$250,000 to be provided during the site plan approval process for Phase One.
- b. Such public art will be established through a process that is consistent with the City's public art policy as applicable, to the satisfaction of the Manager of Culture and The City Planner, and ensuring input from the developer.

The following special regulations apply within the bonus zone:

a) Regulations:

- i) Density: 1200 units per hectare  
(Max.)
- ii) Height: 129 metres  
(Max.)
- iii) Setback for Residential Component of Buildings: 0 metres  
(Min.)  
(Z.-1-162518)

**B-39 545 Fanshawe Park Road West**

The increase in height and density to the zoning of the subject lands shall be permitted in return for enhanced urban design elements described below consisting of 2 residential apartment buildings and a podium base which is generally in keeping with the Site Plan attached as Schedule 1 to the amending by-law and Architectural Elevations attached as Schedule 2 to the amending by-law, which shall be implemented through a development agreement in return for the provision of the following services, facilities, and matters:

- i. Supporting the preservation of natural areas;
- ii. Supporting the provision of common open space that is functional for active or passive recreational use;
- iii. Supporting the provision of, and improved access to, public open space supplementary to any parkland dedication;
- iv. The inclusion of arbors/ pergolas and seating areas along the planned pedestrian trail in the large outdoor amenity area planned for the northwest quadrant of the site, with this trail being connected to the existing trail located along the stormwater management pond to the north of the site;
- v. The inclusion of green roof features in order to reduce the building's heating and cooling requirements, and reduce the heat island effect;
- vi. The inclusion of "hardscape" forecourts on either side of the main driveway in front of the entrances into the apartment buildings leading to one of the entrances of the planned pedestrian trail in the Open Space lands on the north portion of the site;
- vii. The preservation of the view corridor to the Open Space lands on the north portion of the site by way of the main driveway from Fanshawe Park Road West;
- viii. A building design that provides for a positive interface with Fanshawe Park Road West by including:
  - Extensive landscaping along the Fanshawe Park Road West frontage

- to create a more pleasant and engaging experience for pedestrians;
- Definition to the base, middle and top of the buildings with the base consisting of an articulated two storey brick section that extends beyond the main south elevation of the tower acting as a partial podium for the tower above, the middle consisting of a 15 storey tower above the base for Tower “A” and a 14storey tower above the base for tower “B”, and a top consisting of architectural features that will create a dynamic skyline;
- Individual ground floor unit entrances with access to Fanshawe Park Road West by way of a landscaped court yard;
- Tower elevations that have been divided into a series of modulated components that are defined by complimentary changes in articulation. These components have been defined by the use of architectural walls that protrude beyond the main building wall and return at the top of the tower;
- The inclusion of a variety of window sizes and types in order to add visual interest and further break up the massing of the building; and,
- Underground parking for the majority of the required parking with a limited amount of at-grade parking spaces located behind the buildings away from the street edge.

a) Regulations

i)	Front Yard Depth (Minimum)	0.0 meters (0.0 feet)
ii)	Rear Yard Depth (Minimum)	4.0 metres (13 feet) to an OS4 Zone
iii)	Interior Side Yard Depth (East) (Minimum)	12.0 meters (39 feet)
iv)	Interior Side Yard Depth (West) (Minimum)	9 meters (29 feet)
v)	Landscaped Open Space (%) (Minimum)	29%
vi)	Lot Coverage (%) (Maximum)	64%
vii)	Height (Maximum)	Tower “A” 62 metres (203 ft.) Tower “B” 65 metres (213 ft.)
viii)	Density (units per hectare) (Maximum)	281
ix)	Off-street Parking (Minimum) (Z.-1-172546)	327 parking spots

**B-40 801 Sarnia Road**

The development shall be in accordance with the site concept and elevations attached as Schedule “1” of this By-law which includes an apartment building with a maximum height of 16 metres with total maximum of 72 units (96 units per hectare). The development shall specifically incorporate the following services, facilities and matters:

Building

- a building design which, with minor variations at the discretion of the Managing Director, Planning and City Planner, matches the site concept and elevation drawings shown in Schedule “1”;
- The building includes a differentiated base, middle and top;
- Individual entrances to the apartments are located on the ground floor of the apartment building adjacent to Sarnia Road and function as front doors rather than patio doors



- Ground floor amenity spaces are to be designed as open courtyards extending wider than the balconies above and into the front setback;

Site

- Provide glass or metal railings, or masonry walls to delineate the individual courtyards from the public realm.
- Railings are not to exceed 1m in height and masonry walls are not to exceed 0.9m in height in order to maintain visibility.
- Incorporate low landscaping to frame amenity areas.
- Direct walkway access from the front doors to the public sidewalk will be provided.
- Enhanced landscaping within the future public pathway corridor which includes additional plantings and public sitting areas;
- Enhanced entrance feature along Sarnia Road which includes the use of differencing paving materials, vegetation and public sitting areas: and
- All parking is to be located behind the building or enhanced screening.

Notwithstanding anything in the By-law to the contrary the following regulations shall apply:

i)	Front Yard Depth (minimum)	4.0 metres (13.1 feet)
ii)	Rear Yard Setback to Open Space (OS1) Zone (minimum)	13 metres (42.6 feet)
iii)	Height (maximum)	16 metres (52.4 feet)
iv)	Density (maximum)	96 units per hectare (72 dwelling units)
v)	Parking (minimum) (Z.-1-172553)	1 space per dwelling unit

**B-41 515 Richmond Street**

The increase in height and density to the zoning of the subject lands shall be permitted in return for the enhanced urban design elements described below consisting of a residential point-tower, pedestrian plaza and forecourt which, with minor variations at the discretion of the Managing Director, Planning and City Planner, is in keeping with the architectural elevations, site plan and renderings included as Schedule 1 of the amending by-law, and shall be implemented through a development agreement in return for the provision of the following services, facilities, and matters:

Base Features:

- A base height equal to two stories, including floor to ceiling vision glass on the east façade and wrapping around partially to the south and north facades.
- 60% vision glass on the south, east and north facades, including a secondary entrance on the south façade facing the storefronts of the adjacent commercial building.
- Seven architectural stainless steel columns to the east and south of the building supporting the residential floors above.
- A large canopy supported by steel columns, extending towards the street on a slight angle with lighting integrated on the underside.
- An enclosed corridor attaching the main building to the parking structure, including alternating resident storage areas and vision glass into the corridor.

Tower Features:

- A point tower form with a tower floor-plate maximum of 700 square metres.
- 60% (linear) vision glass on residential floors.

- c. Fully wrap-around balconies on floors 3 through 28, alternating every 4<sup>th</sup> or 5<sup>th</sup> floor between the following:
  - i. wood finish metal panel on the underside and visible portions of the balcony, paired with tinted glass and a coordinated colour for metal portions of the railing; balconies rotated on a slight angle (~ 3 degrees) from the angle of the building.
  - ii. Light grey finish metal panel on the underside and visible portions of the balcony, paired with clear glass and a coordinated colour for metal portions of the railing; balconies rotated on the opposite angle of above.
- d. Light grey finish metal panel balconies wrapping two separate portions of the building on floors 29 through 32, with wood finish metal panel balconies wrapping the north east corner with a higher portion of vision glass and a taller roof height and cap.
- e. A mechanical penthouse fully enclosed and clad in materials complimentary to the building.

**Pedestrian Plaza and Forecourt:**

- a. A pedestrian-only plaza located in the setback from Richmond Street, delineated from the public sidewalk by fixed, architectural planters with integrated seating.
- b. A decorative pavement pattern extending across the east entry plaza and into the linear plaza on the south side of the building.

a)	Regulation[s]	
i)	Height (maximum)	101 metres (332 ft.)
ii)	Density (maximum)	342 units per hectare (845 units per acre)
iii)	Front Yard Depth (maximum) (Z.-1-172581)	4.5 (15 ft.)

**B-42 420 Fanshawe Park Road East**

The Bonus Zone shall be implemented through a development agreement to provide for an apartment building of 14.6m (48 ft) with an increased density up to 100 units per hectare in return for the provision of the following services, facilities and matters which are described in greater detail in the proposed by-law:

- i. A high quality development which substantively implements the site plan and elevations attached as Appendix “1” to the amending by-law, with minor revisions at the discretion of the Managing Director of Planning and City Planner;
- ii. A contemporary building design with a mix and articulation of building materials including brick, metal panels, concrete, wood veneer and vision and spandrel glass;
- iii. The provision of individual entrances to ground floor units on the south façade, with operable and lockable front doors, pedestrian scale features such as lighting and weather protection, and private amenity areas designed as courtyards enclosed with a combination of planters and decorative fencing;
- iv. The provision of all but 18 required parking spaces below grade generally located under the building footprint;
- v. The provision of enhanced universally accessible features such as barrier-free access to all floors, and wide routes, paths and corridors which provide adequate width for persons using wheelchairs, scooters, pushing strollers;
- vi. The location of all service and loading facilities within and behind the building and not visible from the public street;
- vii. The enhanced provision of landscaping and retention of mature trees in a park-like setting at the north of the site with a minimum 60% landscaped open space;

- viii. The provision of a commemorative garden and/or signage to acknowledge the historic affiliation of the property within the landscaped open space towards the north of the site.

The following regulations apply within the bonus zone:

- a) Permitted Use:  
Apartment Building
- b) Regulations:
 

i)	Lot Area (Min.)	14,232m <sup>2</sup>	(3.5 ac.)
ii)	Lot Frontage (Min.)	121m	(397 ft.)
iii)	Front Yard Depth (Min.)	15m	(49 ft.)
iv)	East Interior Side Yard (Min.)	15m	(49 ft.)
v)	West Interior Side Yard (Min.)	11m	(36 ft.)
vi)	Rear Yard Depth (Min.)	39m	(128 ft.)
vii)	Landscaped Open Space (Min.)	60%	
viii)	Lot Coverage (Max.)	24%	
ix)	Height (Max.)	14.6m	(48 ft.)
x)	Density (Max.) (Z.-1-172582)	100 Units Per Hectare	

**B-43 633, 635, 637, 645, 649, 651, 655 Base Line Road East**

The Bonus Zone shall be implemented through a development agreement to provide for a continuum of care facility and 12 townhouse units with an increased building height of eight (8) storeys in return for the provision of the following services, facilities and matters which are described in greater detail in the proposed by-law:

- i. A development that is consistent with the site plan and elevations attached as Appendix “1” to the amending by-law, with minor revisions at the discretion of the Managing Director, Planning and City Planner;
- ii. A high quality building design, including a coordinated palette of high quality materials to be further refined through the site plan approval process, with a high proportion of brick and glass;
- iii. A significant stepback above the two (2) storey podium along Base Line Road East to provide a pedestrian scale that is in keeping with the character of the buildings to the north;
- iv. The provision of active ground floor uses and access to the front terrace located at grade along Base Line Road East;
- v. A building design that breaks up the building horizontally and vertically through articulation, architectural details and an appropriate proportion and rhythm of windows and balconies;
- vi. The provision of one level of underground parking;
- vii. The provision of enhanced universal accessibility of all common open spaces, including exterior landscaped amenities, as well as barrier-free bathrooms in all independent seniors living suites ;
- viii. The use of low impact development (LID) features including the bioswale environmentally sensitive stormwater management; and

Enhanced landscaped open space including an outdoor amenity area on the west portion of the site including enhanced landscaping of the bioswale and native plant palette to provide pollinator species habitat.

The following regulations apply within the bonus zone:

- a) Permitted Uses:  
Continuum-of-Care Facility  
Cluster Townhouse Dwellings
- b) Regulations:
  - i) Lot Area (Minimum) 12,120m<sup>2</sup> (130,459 sq. ft.)
  - v) Lot Frontage (Minimum) 129m (423 ft.)
  - vi) Front Yard Depth (Minimum) 6m (19 ft.)
  - vii) Landscaped Open Space (Minimum) 34%
  - viii) Lot Coverage (Maximum) 36%
  - ix) Density (Maximum) 68 Units Per Hectare

Regulations - Continuum of Care Facility:

- i) Height (Maximum) 38.5m (126 ft.)
- ii) East Interior Side Yard (Minimum) 13.5m (42 ft.)
- iii) West Interior Side Yard (Minimum) 32m (104 ft.)
- iv) Rear Yard Depth (Minimum) 20m (66 ft.)

Regulations – Cluster Townhouse:

- i) Height (Maximum) 12m (39 ft.)
- ii) East Interior Side Yard (Minimum) 4m (13 ft.)
- iii) West Interior Side Yard (Minimum) 4m (13 ft.)
- iv) Rear Yard Depth (Minimum) 6m (19 ft.)

- c) Special Definitions applying exclusively to this zone:
  - i) “SENIOR CITIZEN APARTMENT BUILDING” means an apartment building designed for the accommodation of the elderly. (Z.-1-172591)

**B-44 150 Dundas Street and 153 Carling Street**

This bonus zone is intended to facilitate a high quality development which substantively implements the site plan and elevations attached as Appendix “1” to the amending by-law, with minor revisions at the discretion of the Managing Director of Planning and City Planner; and

The contribution to My Dundas Place for the provision of facilities, services, programming, public art or other matters that will provide for positive project enhancements including:

- i) The financial contribution of funding in the amount of 1% of the construction value up to \$250,000, to be provided at the time of site plan approval, or
- ii) The provision of 465m<sup>2</sup> (5,000 sq ft) of storage space, in a form and configuration to the satisfaction of the City, for a long-term lease not less than 15 years, at a cost to the City of \$1.00 per annum.

The following special regulations apply within the bonus zone:

- a) Regulations:
  - i) Density: 1,498 units per hectare  
(Maximum)
  - iii) Height: 85 metres (278 ft)  
(Maximum)
  - iii) Ground Floor Commercial Uses: 600m<sup>2</sup> (6,458 sq ft)  
(Minimum)
  - iv) Residential Component Of Building (Minimum) The required east and west setbacks for the residential tower portion of the building shall be a minimum of 2.5 metres (8.2 feet) above 15m (49.2m)
  - v) Tower Setback Storeys 4 and 5  
The required setback for the tower portion shall be 5m from the property boundaries along the Dundas Street and Carling Street frontages, save and except a maximum of 10m (32.8 ft) width in the middle which shall have a minimum setback of 1.2m (3.9 ft)  
  
Tower Setback Storeys 6 and above  
The required setback for the tower portion shall be 5m from the property boundaries along the Dundas Street and Carling Street frontages, save and except a maximum of 10m (32.8 ft) width in the middle which shall have a minimum setback of 3.5m (11.4 ft).  
(Z.-1-172601)

**B-45 32, 36, and 40 York Street and part of 330 Thames Street**

This bonus zone is intended to facilitate a high quality development which substantively implements through the required development agreement(s), the Site Plan, Landscape Plan, Elevations, Renderings and Floor Plans attached as Schedule “1” to the amending by-law; and,

- i) The contribution of funding for an identified component for *Back to the River* in the amount of \$100,000 to be provided during the site plan approval process. The identifiable component of Back to the River will be identified by the City Planner with input from the applicant.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
  - i) Density: 414 units per hectare  
(maximum)
  - ii) Setback for Residential Component of Buildings: 1.0 metres (3.3 ft.) north  
2.5 metres (8.2 ft.) east  
(minimum) 2.0 metres (6.5 ft.) south  
8.0 metres (26.2 ft.) west
  - iii) Parking (underground) (minimum) (Z.-1-172610) 108 spaces

**B-46 32, 36, and 40 York Street**

This bonus zone is intended to facilitate a high quality development which substantively implements through the required development agreement(s), the Site Plan, Landscape Plan, Elevations, Renderings and Floor Plans attached as Schedule “2” to the amending by-law; and,

- i) The contribution of funding for an identified component for the *Back to the River project* in the amount of \$150,000 to be provided during the site plan approval process. The identifiable component of Back to the River will be identified by the City Planner with input from the applicant.
- ii) The contribution of funding for the *Downtown Heritage Conservation District* in the amount of \$100,000 to be provided during the site plan approval process.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
  - i) Density: 534 units per hectare  
(maximum)
  - ii) Setback for Residential 13 metres (42.6 ft.) north  
Component of Buildings: 2.5 metres (8.2 ft.) east  
(minimum) 8.5 metres (27.9 ft.) south  
6.0 metres (19.7 ft.) west
  - iii) Parking (underground) 101 spaces  
(minimum) (Z.-1-172610)

**B-47 661 and 667 Talbot Street**

The Bonus Zone shall be implemented through a development agreement to facilitate the development of a high quality, multi-storey residential apartment building, with an increased building height of up to sixteen (16) storeys (49.5m) and a maximum of 236 dwelling units (403 units per hectare), which substantively implements the Site Plan, Elevations, and Renderings attached as Schedule “1” to the amending by-law; and,

- i) The provision of two levels of underground parking.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
  - i) Density 403 units per hectare  
(maximum) (163 units per acre)
  - ii) Height 49.5 metres (162 feet)  
(maximum)
  - iii) Front Yard Setback 2.5 metres (8.2 feet)  
(minimum)
  - iv) Rear Yard Setback 0 metres (0 feet)  
(minimum)
  - v) North Interior Side Yard Setback 16 metres (52 feet)  
(minimum)
  - vi) South Interior Side Yard Setback 10 metres (32 feet)  
(minimum)
  - vii) A 2.5 metre stepback after the first 4-storeys in height on the portion of the building fronting Talbot Street to a maximum of 7-storeys in height before an additional 9.5m stepback is required before 16-storeys (49.5m) of building height is permitted.
  - viii) Parking 171 parking spaces  
(minimum)

ix)	Lot Coverage	42%
x)	Landscaped Open Space (Z.-1-172622)	23%”

**B-48 329 and 331 Richmond Street**

The Bonus Zone shall be implemented through a development agreement to facilitate the development of a high quality, multi-use building, a total of 36 dwelling units and density of 556 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
  - i) Density hectare (maximum) per acre) 556 units per (226 units
  - ii) Height (maximum) 27 metres (88.58 feet)
  - iii) Parking (minimum) 0 parking spaces
  - iv) Front/Exterior Side Yard (minimum) 0 metres (0 feet)
  - v) Stepback after 4<sup>th</sup> storey at 331 Richmond Street (Minimum) 2 metres (6.56 feet)
  - vi) Stepback after 5<sup>th</sup> storey at 329 Richmond Street (Minimum) (Z.-1-182632) 2 metres (6.56 feet)

**B-49 250, 268, 270, and 272 Springbank Drive**

The Bonus Zone shall be implemented through a development agreement to provide for a development that is consistent with the site plan and elevations attached as Appendix “1”, in return for the provision of the following services, facilities and matters:

- i. enhanced building and site design features including buildings with a multi faceted street face and setback podium creating a pedestrian area linked to the public sidewalk.
- ii. remediation of an existing brownfield condition notwithstanding potential compensation through brownfield incentives;
- iii. construction of 2 levels of fully underground parking;
- iv. dedication of the open space (OS4) lands as a public link and to compliment the adjacent Environmentally Sensitive Area with all existing asphalt and buildings removed and landscaped in conjunction with landscaping plans for this area to be approved by Parks Planning;
- v. to provide a green roof on the office commercial podium for active open space for building residents
- vi. current public transit lines (23 and 5);
- viii. construction of a LEED certified building

The following regulations shall apply with the approved Site Plan:

- i) Height Maximum (Residential Towers) 51.0 metres (167 feet)
- ii) Height Maximum (3 storey Commercial podium) 17.0 metres (56 feet)
- iii) Front Yard setback Minimum residential 4.0 metres (6.6 feet)

iv) Front Yard setback Maximum podium	6.0 metres (19.7 feet)
v) Density Maximum	306 units per hectare (114 units per acre)
vi) Maximum commercial gross floor area	2,000 sq. m (21, 528 sq. ft)
vii) Minimum easterly side yard	0.0 metres from OS4 zone
viii) Minimum westerly side yard	24.0 metres (59 feet)
ix) Minimum read yard westerly building	30.0 metres (98.4 feet)
x) Maximum coverage (OMB File No. PL160951 - Z.-1-182657)	28%

#### **B-50 1039, 1041, 1043, 1045, 1047 Dundas Street**

The Bonus Zone shall be implemented through a development agreement to facilitate the development of a high quality, multi-use building, a total of 41 dwelling units and density of 205 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

##### a) Regulations:

- |      |  |  |
|------|--|--|
| i)   | Density<br>(maximum)   | 205 units per<br>hectare (83 units per acre) |
| ii)  | Height<br>(maximum)  | 18 metres<br>52.50 feet)                     |
| iii) | Parking<br>(minimum)   | 22 Parking Spaces                            |
| iv)  | Bedrooms<br>(maximum)  | 1 per unit                                   |
| iv)  | Dwelling units are permitted on the whole of the first floor.<br>(Z.-1-182664) |  |

#### **B-51 391 South Street**

This bonus zone is intended to facilitate a high quality development which substantively implements through the required development agreement(s), the Site Plan, Renderings and Concept Landscape Plan, attached as Schedule “1” and Schedule “2” to the amending by-law; and

- i) Conservation, retention and adaptive re-use of the existing heritage designated Colborne Building;
- ii) Provision of the publicly accessible courtyard and public art within the courtyard
- iii) Provision of two levels of underground parking

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- |      |                      |                                   |
|------|----------------------|-----------------------------------|
| a)   | Regulations:         |                                   |
| i)   | Density:<br>Maximum  | 705 units per hectare             |
| ii)  | Frontage:<br>Minimum | 98m (328 ft)                      |
| iii) | Lot Area:<br>Minimum | 9,000m <sup>2</sup> (9,687 sq ft) |



ix)	Front, Interior, Exterior and Rear Setbacks: Minimum	0m (0ft)
x)	Height: Maximum	80 metres (262 ft)
xi)	Tower floor plate above 9 <sup>th</sup> Storey Maximum	800m <sup>2</sup>
xii)	Parking total for all uses: Minimum	360 spaces

**B-52 147-149 Wellington Street and 253-257 Grey Street**

The Bonus Zone shall be implemented through a development agreement to facilitate the development of a high quality apartment building with a maximum of 18 storeys, 246 dwelling units and density of 560 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law; and

- i) Transit Station  
The financial contribution of funding to the future Transit Station at Wellington Street and Grey Street in the amount of \$200,000, for the provision of public art or other station enhancements to be provided at the time of site plan approval or construction of the station, whichever occurs first.
- ii) Provision of Affordable Housing  
The provision of 10 affordable housing units, established by agreement at 95% of average market rent for a period of 20 years. An agreement shall be entered into with the Corporation of the City of London, to secure those units for this 20 year term.
- iii) 2 levels of underground parking
- iv) The construction of a civic space provided at the main pedestrian entrance to the building and enhanced landscaping along Wellington Street, consistent with the conceptual site plan and renderings shown in Schedule 1 of the amending by-law.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
  - ii) Density (Maximum): 560 units per hectare (226 units per acre)
  - iii) Height (Maximum): 63 metres (206.7 feet)
  - iv) Parking (Minimum): 197 Parking Spaces
  - v) Interior Side Yard Depth (Minimum): 1.6 metres (5.25ft) for floors 1-4
  - vi) Interior Side Yard Depth (Minimum): 6.0 metres (19.7ft) for floors 5-8
  - vii) Interior Side Yard Depth (Minimum): 11.5 metres (37.7ft) for floors 9-18
  - viii) Lot Coverage 31%
  - ix) Dwelling units are permitted on the first floor. (Z.-1-182701)

**B-53 131 King Street**

The Bonus Zone shall be enabled through one or more agreements to facilitate the development of a high quality residential apartment building, with a maximum

of 30-storeys, 266 dwelling units and density of 931 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law; and

i) Public Parking

The provision of 41 publicly accessible parking spaces on level 1 of the underground parking facility and accessed from York Street. An agreement shall be entered into between the Corporation of the City of London and the property owner to facilitate this requirement.

ii) Design Feature

As depicted in the elevations shown in Schedule “1” to the amending By-law a design feature will be located over the main vehicular access off of King Street. The details for this feature, including design, appearance and materials, will be determined in consultation with Staff through the site plan approvals process.

iii) Public Art

The financial contribution of funding to a future public art project within the Downtown Area in the amount of 1% of the construction value up to a maximum of \$250,000, to be provided at the time of site plan approval.

iv) 3 levels of underground parking

v) Publicly accessible civic space located at the York Street entrance.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

a) Regulations:

- |  |                            |
|--|----------------------------|
| i) Density   | 931 uph                    |
| ii) Height<br>(Maximum):   | 102 metres<br>(334.6 feet) |
| iii) Residential Component<br>Easterly Side Yard<br>(Minimum):                 | 1.2 metres                 |
| iv) Residential Component<br>Westerly Side Yard<br>(Minimum):                  | 1.5 metres                 |
| v) Residential Component<br>Northerly Side Yard<br>(Minimum):<br>(Z.-1-182708) | 1.3 metres                 |

**B-54 809 Dundas Street**

The Bonus Zone shall be enabled through one or more agreements to facilitate the development of a high quality apartment building with a maximum of 24 storeys, 480 dwelling units and density of 710 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” and Schedule “2” to the amending by-law; and

- i) Provision of one level of underground parking
- ii) Provision of Affordable Housing

The provision of 25 affordable housing units, established by agreement at 95% of average market rent for a period of 25 years. An agreement shall be entered into with the Corporation of the City of London, to secure said affordable housing units for the 25 year term.

The following special regulations apply within the bonus zone:

- a) Regulations:
  - i) For the purpose of this by-law the front lot line shall be deemed to be Dundas Street

ii)	Density: (Maximum):	710 units per hectare
iii)	Height: (Maximum):	82 metres (269 ft)
iv)	Front Stepback of Tower above the Third Storey (Minimum):	11 metres (36 ft)
v)	Individual Tower floor plate above 3 <sup>rd</sup> Storey (Maximum):	1,060m <sup>2</sup> (11,409 sq ft)
vi)	Ground Floor Commercial Uses (Minimum):	1,750m <sup>2</sup> (18,836 sq ft)

#### **B-55 230 North Centre Road**

The Bonus Zone shall be implemented through the required development agreements to facilitate the development of a high quality residential apartment building, with a maximum of 15-storeys, 222 dwelling units and density of 192 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law; and

i) Transit Station

The financial contribution of funding to the future Transit Station at Masonville Mall in the amount of 1% of the construction value up to \$250,000, for the provision of facilities, services, programming, public art or other matters for positive project enhancements to be provided at the time of site plan approval or construction of the station, whichever occurs first.

ii) 1 level of underground parking

iii) Publicly accessible civic space located at the southwest corner.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

a) Regulations:

i)	Density	192 uph
ii)	Height (Maximum):	56 metres (183.7 feet)
iii)	Front Yard Depth for floors 1-3 (Minimum):	4.5 metres (14.76ft)
iv)	Exterior Side Yard Depth for floors 1-2 (Minimum):	7.5 metres (24.60ft)
v)	Rear Yard Depth for floors 1-3 (Minimum):	3.5 metres (11.48ft)
vi)	Rear Yard Depth for floors 4-14 (Minimum):	6 metres (19.68ft)
vii)	Rear Yard Depth for floor 15 (Minimum):	8.5 metres (26.25ft)
viii)	Maximum Lot Coverage (Maximum):	64.5%
ix)	Landscaped Open Space (Minimum):	28.5%

### **B-56 3080 Bostwick Road (Site 1)**

The Bonus Zone shall be enabled through one or more agreements to facilitate the development of a high quality apartment building with a maximum of 21 storeys, 372 dwelling units and density of 262 units per hectare, which is generally in accordance with the Site Plan and Elevations attached as Schedule “1” and Schedule “2” to the amending by-law, except where described in more detail below or in the Urban Design Guidelines for the High Density Residential designated lands within the Bostwick Neighbourhood; and

- 1) Contribution of \$25,000 towards creating a skate park on the Community Centre lands;
- 2) Provision of 20% accessible apartment units;
- 3) Large caliper boulevard tree planting with a minimum 100mm caliper and a minimum distance of 10m between tree planting for the extent of the site frontage for Southdale Road West, Bostwick Road, and both sides of Street A;
- 4) Provision of one accessible electric vehicle charging station located on the Community Centre lands or in a publicly accessible location on Site 1;
- 5) Provision of two publicly accessible vehicle share facilities/spaces;
- 6) Provision of ten publicly accessible bicycle share facilities;
- 7) The following provisions shall be delivered as part of the first development phased of either Site 1 or Site 5 at the time of the development agreement:
  - i) Dedication of 0.64ha of park land identified as Block 4 in the draft plan of subdivision 39T-18502 to the City of London, above and beyond (in excess of) the full standard dedication required by by-law CP-9 for parkland or cash-in-lieu for both Sites 1 and 5; and other proposals on Sites 2, 3 and 6;
  - ii) Provision of a pavilion and lighting within the dedicated park land, or the commensurate financial equivalent for the features as a identified in by-law C.P.-1496-244, to the City’s discretion;
  - iii) Provision of a pedestrian bridge from the park land across the Thornicroft Drain to the Bostwick Community Centre, or the commensurate financial equivalent for the feature as a identified in by-law C.P.-1496-244, to the City’s discretion;

The following special regulations apply within the bonus zone:

- a) Regulations:
  - i) Density (Maximum): 262 units per hectare
  - ii) Height (Maximum): 77 metres (252 ft)
  - iii) Exterior Side Yard Setback (Minimum): 0.3m (0.98 ft)
  - iv) Rear Yard Setback (Minimum): 20m (65.6 ft)
  - v) Interior Side Yard Setback (Minimum): 14m (45.9 ft)
  - vi) Lot Coverage 38%
  - vii) Parking Total for All Uses 725 spaces

### **B-57 3080 Bostwick Road (Site 5)**

The Bonus Zone shall be enabled through one or more agreements to facilitate the development of a three storey (13.5m (44 ft)) mixed office and commercial

building and one 17 storey (68m (223ft)) tower, with an increased density up to 209 units, which is generally in accordance with the Site Plan and Elevations attached as Schedule “1” and Schedule “2” to the amending by-law, except where described in more detail below or in the Urban Design Guidelines for the High Density Residential designated lands within the Bostwick Neighbourhood; and

- 1) Contribution of \$25,000 towards creating a skate park on the Bostwick Community Centre lands;
- 2) Provision of 20% accessible apartment units;
- 3) Large caliper boulevard tree planting with a minimum 100mm caliper and a minimum distance of 10m between tree planting for the extent of the site frontage for Southdale Road West, and both sides of Street B;
- 4) Provision of one accessible electric vehicle charging station located on the Bostwick Community Centre lands or in a publicly accessible location on Site 5;
- 5) Provision of two publicly accessible vehicle share facilities/spaces;
- 6) Provision of ten publicly accessible bicycle share facilities;
- 7) The following provisions shall be delivered as part of the first development phased of either Site 1 or Site 5 at the time of the development agreement:
  - a. Dedication of 0.64ha of park land identified as Block 4 in the draft plan of subdivision 39T-18502 to the City of London, above and beyond (in excess of) the full standard dedication required by by-law CP-9 for parkland or cash-in-lieu for both Sites 1 and 5; and other proposals on Sites 2, 3 and 6;
  - b. Provision of a pavilion and lighting within the dedicated park land, or the commensurate financial equivalent for the features as a identified in by-law C.P.-1496-244, to the City’s discretion;
  - c. Provision of a pedestrian bridge from the park land across the Thornicroft Drain to the Bostwick Community Centre, or the commensurate financial equivalent for the feature as an identified in by-law C.P.-1496-244, to the City’s discretion.

The following special regulations apply within the bonus zone:

- a) Regulations:
 

i)	Density (Maximum):	209 units per hectare
ii)	Height (Maximum):	68 metres (223 ft)
iii)	Exterior Side Yard Setback (Minimum):	3.5 metres (11 ft)
iv)	Rear Yard Setback (Minimum):	5 metres (16 ft)
v)	Interior Side Yard Setback (Minimum):	15 metres (42 ft)
vi)	Number of dwelling units (Maximum):	208
vii)	Parking Total for All Uses (Z.-1-182713)	472 spaces

**B-58 440 Clarke Road**

The Bonus Zone shall be enabled through one or more agreements to facilitate the development of a high quality residential apartment building, with a maximum of 4-storeys, 65 dwelling units and density of 95 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law and the provision of affordable housing in the form of 65

dwelling units (95 units per hectare), consisting of one and two bedroom units.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Permitted Uses:
  - i) Apartment Building;
- b) Regulations:
  - i) Density 95 uph
  - ii) Height 18 metres  
(maximum): (59 feet)
  - iii) Parking 65 spaces  
(Z.-1-192730)

### **B-59 462, 468, 470, 472 Springbank Drive**

The B-59 Zone shall be implemented through the required development agreements to facilitate the development of a high quality residential apartment building, with a maximum of 9-storeys with 186 dwelling units which substantively implements the Site Plan and Elevations attached as Schedule "1" to the amending by-law; and

- i) Provision of Affordable Housing  
10% of the total unit count (rounded up to the nearest unit), above the 150 unit per hectare threshold, to a maximum of 8 units, shall be allocated for affordable housing units (1 bedroom units) established by agreement at 95% of average market rent for a period of 25 years. An agreement shall be entered into with the Corporation of the City of London, to secure those units for this 25 year term.
- ii) 1 level of underground parking  
The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):
  - a) Additional Permitted Uses:
    - i) Bake shops;
    - ii) Brewing on Premises Establishment;
    - iii) Clinics;
    - iv) Commercial schools;
    - v) Convenience business service establishments;
    - vi) Convenience service establishments;
    - vii) Convenience stores;
    - viii) Day care centres;
    - ix) Financial institutions;
    - x) Florist shops;
    - xi) Food stores;
    - xii) Medical/dental offices;
    - xiii) Offices;
    - xiv) Personal service establishments;
    - xv) Pharmacies;
    - xvi) Retail Stores;
    - xvii) Restaurants, eat-in;
    - xviii) Restaurants, take-out;
    - xix) Studios;
  - b) Regulations:
    - i) Density 265 uph (107.25 units per acre)
    - ii) Height 32 metres  
(Maximum): (105 feet)
    - iii) Front Yard Depth 2.5 metres  
(Minimum): (8.2 feet)

- |       |   |                                       |
|-------|---|---------------------------------------|
| iv)   | Rear Yard Depth<br>(Minimum):   | 7.0 metres<br>(23 feet)               |
| v)    | Westerly Interior<br>Side Yard Depth<br>(Minimum):  | 5.5 metres<br>(18 feet)               |
| vi)   | Easterly Interior<br>Side Yard Depth<br>(Minimum):  | 12.1 metres<br>(39.70 feet)           |
| vii)  | Residential Parking<br>(Minimum):   | 1 space per unit                      |
| viii) | Commercial Parking<br>(Minimum):  | 17 spaces                             |
| ix)   | No drive-through will be permitted for any of the additional permitted uses.                      |                                       |
| x)    | The permitted commercial uses will only be permitted on the first floor of an apartment building. |                                       |
| xi)   | The maximum gross floor area for specific individual uses shall be as follows:                    |                                       |
| a)    | Commercial schools<br>and Pharmacies  | 300 m <sup>2</sup><br>(3,229 sq. ft.) |
| b)    | Restaurants - eat-in<br>and take-out  | 300 m <sup>2</sup><br>(1,616 sq. ft.) |
| c)    | Food stores   | 500 m <sup>2</sup> (5,382 sq. ft.)    |
| d)    | All other permitted uses<br>(Z.-1-192748)   | 400 m <sup>2</sup> (4,305 sq. ft.)    |

#### **B-60 3087 White Oak Road**

This bonus zone is intended to facilitate a high quality development which substantively implements through the required development agreement(s), the Site Plan, Elevations and Concept Landscape Plan, attached as Schedule "1" to the amending by-law; and

- i) Enhanced landscaping along White Oak Road with wrought iron (or similar) fencing and provision of a pedestrian pathway from Petty Road to White Oak Road;

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
  - i) For the purpose of this by-law the front lot line shall be interpreted as Petty Road
  - ii) Front Yard Depth  
(Minimum): 3m (9.8ft)
  - iii) Density  
(Maximum): 79 units per hectare
  - iv) Height  
(Maximum): four storeys 16m (52.5 ft)  
(Z.-1-192756)

#### **B-61 475 Edgevalley Road**

The Bonus Zone shall be enabled through one or more agreements to facilitate the development of three (3) low-rise apartment buildings, with a maximum of four (4) storeys (Building A = 16m), five (5) storeys (Building B= 18m) and six (6) storeys (Building C = 22m), a total of 147 dwelling units (Building A = 39 dwelling units; Building B = 49 dwelling units; Building C = 59 dwelling units), and a density of 100 units per hectare,

## Provision of Affordable Housing

The provision of four (4) affordable rental housing units, established by agreement at 85% of the CMHC average market rent for a period of 15 years. An agreement shall be entered into with the Corporation of the City of London, to secure said affordable housing units for the 15 year term.

The following special regulations apply within the bonus zone:

- a) Regulations:
  - i) For the purpose of this by-law, the front lot line shall be deemed to be Kilally Road.
  - ii) Density: 100 units per hectare  
(Maximum):
  - iii) Height of Building A: Four (4) storeys  
(Maximum): 16 metres (52.5 feet)  
Height of Building B: Five (5) storeys  
(Maximum): 18 metres (59.1 feet)  
Height of Building C: Six (6) storeys  
(Maximum): 22 metres (72.2 feet)
  - iv) Front Yard Setback  
(Minimum): 2.0 metres (6.6 feet)  
(Maximum): 6.0 metres (19.7 feet)
  - v) Exterior Side Yard Depth - Edgevalley Road  
(Minimum): 3.0 metres (9.8 feet)  
(Maximum): 7.0 metres (23.0 feet)
  - vi) Exterior Side Yard Depth - Agathos Street  
(Minimum): 2.0 metres (6.6 feet)
  - vii) Interior Side Yard Depth  
(Minimum): 2.75 metres (9.1 feet)  
(Z.-1-192785)

## **B-62 800, 805 and 810 Chelton Road**

The Bonus Zone shall be implemented through the required development agreement(s) to facilitate the development of low to mid-rise apartment buildings up to a maximum of five (5) storeys and six (6) storeys which substantially implements the vision and principles of the Chelton Road Master Plan for 800, 805 and 810 Chelton Road attached as Schedule "1", and site concept plans attached as Schedule "2" to the amending by-law; and,

- i. Enhanced building and site design features including an active street edge along Bradley Avenue, Chelton Road, and Meadowgate Boulevard;
- ii. Dedication of public open space (OS5) lands;
- iii. Measures to enhance the Natural Heritage System including substantial buffer planting with native vegetation to protect adjacent natural features;
- iv. Large quantities of secure bicycle parking and cycling infrastructure including indoor and outdoor storage facilities, and a multi-use pathway internal to the site providing connections to surrounding public sidewalks and pedestrian trails; and,
- v. Substantial tree planting of large caliper trees to be planted along the street frontage and outdoor amenity areas.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
  - 805 Chelton Road
  - i) Height of Buildings A and D: Five (5) storeys  
(Maximum): 18 metres



800 – 810 Chelton Road

- |     |  |                               |
|-----|--|-------------------------------|
| i)  | Height of Buildings A and E<br>(Maximum):                  | Six (6) storeys<br>22 metres  |
| ii) | Height of Buildings B and D<br>(Maximum):<br>(Z.-1-192794) | Five (5) storeys<br>18 metres |

**B-63 1339 – 1347 Commissioners Road West**

The Bonus Zone shall be implemented through one or more agreements to facilitate the development of a high quality residential apartment building, with a maximum height of 5 storeys, 34 dwelling units and a maximum density of 132 units per hectare, which substantively implements the Site Plan, Landscape Plan and Elevations attached as Schedule “1” to the amending by-law, provides for affordable housing in the form of 34 dwelling units (132 units per hectare), enhanced landscaped open space and underground parking. The affordable housing component shall consist of:

- a total of 3, two-bedroom affordable rental units (two of which, at minimum, are to be accessible and located on the ground floor);
- rents not exceeding 85% of the Average Market Rent (AMR) for the London Census Metropolitan Area as determined by the CMHC at the time of building occupancy; and,
- the duration of affordability shall be set at 15 years from the point of initial occupancy of all 3 two-bedroom units.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
- |      |   |                         |
|------|---|-------------------------|
| i)   | Front Yard Depth<br>(Minimum):                | 2.0 metres (6.56 ft.)   |
| ii)  | Interior Side Yard Depth (West)<br>(Minimum): | 2.2 metres (7.2 ft.)    |
| iii) | Height<br>(Maximum):                          | 17.5 metres (57.41 ft.) |
| iv)  | Density<br>(Maximum):<br>(Z.-1-192795)        | 132 units per hectare   |

**B-65 1018 and 1028 Gainsborough Road**

The Bonus Zone shall be enabled through one or more agreements to facilitate development of a residential apartment building, with a maximum height of 12 storeys, 182 dwelling units and a maximum density of 392 units per hectare, which generally implements in principle the site concept and elevation plans attached as Schedule “1” to the amending by-law, with further refinements to occur to the orientation of the apartment building and parking area through the site plan approval process, in return for the following facilities, services and matters:

- A total of 18 affordable rental apartment units consisting of 16, one bedroom units and 2, two bedroom units to be provided and located within Building “B”;
- A minimum of 3, one-bedroom and 1, two-bedroom accessible units are to be provided and located within Building “B”;
- 4, one bedroom units and 2, two bedroom units shall not exceed 90% of the Average Market Rent (AMR) for the London Census Metropolitan Area as determined by CMHC at the time of building occupancy; and duration of affordability shall be set at 10 years from the point of initial occupancy; and,
- 12, one bedroom units shall not exceed 75% of the Average Market Rent

(AMR) for the London Census Metropolitan Area as determined by CMHC at the time of building occupancy; and duration of affordability shall be set at 20 years from the point of initial occupancy.  
(Z.-1-202816)

**B-66 2300 Richmond Street**

The Bonus Zone shall be implemented through one or more agreements to facilitate the development of two residential apartment buildings, with a maximum height of eight (8) storeys, and a maximum of 320 dwellings units, which substantively implements the Site Plan, Renderings and Elevations attached as Schedule “1” to the amending by-law, providing for:

- A site designed to appropriately accommodate the provision of the City proposed Pedestrian Bridge to cross over Richmond Street;
- The incorporation of urban design features into the building elevations and overall site design to help create a gateway into the City;
- The provision and naturalization of a corridor linkage along the easterly property line; and
- A monetary contribution toward enhancements to the design and construction of the Pedestrian Bridge/Gateway feature.

The development shall specifically incorporate the following key components:

- A building design which, with minor variations at the discretion of the Director of Development Services, matches the site plan, renderings and elevation drawings shown in Schedule “1”;
- Buildings collectively forming a T-shaped configuration, located in staggered positions, perpendicular to the Richmond Street frontage and with at least 60 metres of building wall facing Richmond Street;
- Inclusion of underground parking, which may be partially exposed where grades necessitate, except in the southwesterly area where the underground parking requires plantings at grade level as part of a buffer function;
- A central drive aisle provided between the two apartment buildings, leading to a parking structure that provides underground parking integrated into the buildings;
- Provision for locating the City proposed pedestrian bridge crossing at Richmond Street;
- Site design that makes best efforts to maintain the existing “knoll” feature;
- A built form which includes a high standard of design, in accordance with the Urban Design policies (chapter 11) of the Official Plan, as well as the Placemaking Guidelines, as this site is located at a major entryway into the City;
- Include on the west, north and south facades articulation, generous amounts of glazing, and a variety of materials and colours to add interest to these facades;
- Building heights stepping down to 7 storeys at the rear of the buildings, and to 7 and then 6 storeys at the front of the buildings as per attached elevations;
- Design which breaks up the building horizontally by differentiating the top two or three floors of the building as per attached elevations;
- Design which breaks up the building vertically by articulating the façade as per attached elevations;
- Bird friendly design, snow removal control (no snow storage in the buffer or the ecological feature), lighting controls on parking lot and building lights so as to minimize shining into the natural area;
- A monitoring plan for the ecological buffers/features.

The following special regulations apply within the bonus zone upon the execution and registration of the required agreement(s);

- |    |                                |            |
|----|--------------------------------|------------|
| i) | Front Yard Depth<br>(Minimum): | 8.0 metres |
|----|--------------------------------|------------|

ii)	Interior Side Yard Depth for Main Buildings (Minimum):	2.0 metres
iii)	Interior Side Yard Depth for Parking Structures (Minimum):	0.0 metres
iv)	Rear Yard Depth for Parking Structures (Minimum):	0.0 metres
v)	Number of Dwelling Units (Maximum):	320
vi)	Building Height (Maximum):	28.0 metres
vii)	Landscaped Open Space (Minimum): (Z.-1-202833)	28 percent

### **B-67 754-760 Base Line Road East**

The Bonus Zone shall be implemented through one or more agreements to facilitate the development of a high quality residential apartment building, with a maximum height of 4-storeys and a maximum density of 165 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law, provides for affordable housing and enhanced landscaped open space. The affordable housing component shall consist of:

- one, one-bedroom barrier-free affordable rental unit;
- rents not exceeding 85% of the Average Market Rent (AMR) for the London Census Metropolitan Area as determined by the CMHC at the time of building occupancy; and,
- the duration of affordability shall be set at 25 years from the point of initial occupancy the unit.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

a)	Regulations	
i)	Base Line Road East shall be deemed to be the front lot line	
ii)	Front Yard Depth (Minimum)	0.1 metres (0.32 feet)
iii)	Exterior Side Yard Depth (Minimum)	1.7 metres (5.57 feet)
iv)	Interior Side Yard Depth (Minimum)	2.6 metres (8.53 feet)
v)	Height (Maximum)	17 metres (55.7 feet)
vi)	Density (Maximum)	165 units per hectare
vii)	Parking (Minimum) (Z.-1-202853)	0.9 spaces per unit

### **B-68 1230 Hyde Park Road**

The Bonus Zone shall be implemented through a development agreement to facilitate the development of high quality mixed-use apartment buildings and standalone apartment buildings with a maximum height of 22 metres (6-storeys) which substantively implements the site-specific “Design Criteria”.

## Design Criteria

### Site Development

- Building Sitting:
  - Buildings shall be located along the majority of the Hyde Park Road and Street 'A' frontages to provide for a built edge along the street;
  - All service and loading facilities associated with building shall be located within and/or behind buildings away from amenity areas and not visible from the public street.
- Pedestrian Connectivity:
  - Mid-block walkway connections from Hyde Park Road through Block 1 shall be provided between buildings fronting Hyde Park Road, leading to internal parking area and to walkways behind buildings leading to sidewalks along the northern and southern portions of Street 'A';
  - Walkway connections from the sidewalk along Hyde Park Road shall be provided from both ends of the building for Block 2, with a further connection to the sidewalk along Street 'A'.
- Access and Parking:
  - Vehicular access for both Blocks 1 and 2 shall be provided from Street 'A'.
  - No parking or vehicular drive isles shall be located between buildings and the Hyde Park road frontage.
  - Low masonry walls (max. 0.75m), complimentary in design to the buildings, shall be provided with a combination of landscaping to screen any parking located along the edge of Street 'A'.
- Common Outdoor Amenity Areas:
  - Outdoor common amenity areas shall be provided for each building, alternatively these spaces can be combined into one, centrally located common outdoor amenity space per Block.
  - These spaces shall be an appropriate size to provide adequate amenity for the proposed number of residents and provide the opportunity for passive and/or active recreation.
  - These spaces shall be located within close proximity to a building entrance/exit. Alternatively a safe, appropriately sized, and conveniently aligned walkway connection(s) will be provided from the nearest building entrance/exit. If the spaces are combined into one centrally located space provide for walkways from each building to the space.
  - Enhanced landscaping shall be provided along the Hyde Park Road frontage in the form of small amenity areas and trees between buildings. Efforts will be made to provide opportunities for additional amenity space through site and building design, with the intention of providing spaces for residents and visitors to enjoy the outdoors and animating street frontages to facilitate pedestrian oriented environments

### Built form

- All buildings:
  - The development shall feature contemporary building designs with a mix and articulation of building materials including brick, metal panels, concrete, wood veneer and vision and spandrel glass;
  - Buildings shall be designed in a way that breaks up the structures horizontally and vertically through articulation, architectural details, and an appropriate proportion and rhythm of windows and balconies
- Buildings along Hyde Park frontage:
  - The design for buildings facing Hyde Park Road shall be defined by complementary changes in building articulation and design above the 4<sup>th</sup> floor that will contribute to the provision of a human-scaled environment along the street. Potential design solutions may include a

- step-back, balconies or outdoor areas, which would provide proportionate step-backs from the front face of the building.
  - Buildings shall include active commercial uses along the Hyde Park Road frontage, and all main floor commercial unit entrances shall be oriented towards Hyde Park Road.
  - The ground-floor street façade shall be primarily comprised of vision glazing for views into and out of the building. Where vision glazing is not used, alternative urban design measures that contribute to an animated streetscape shall be required.”;
  - Overhead protection from natural elements shall be provided above the first floor in locations such as pedestrian entrances where appropriate to architecturally differentiate the building base from the upper levels;
- Buildings along the Street ‘A’ frontage:
  - The development shall provide street-oriented features for residential buildings, including individual entrances to ground-floor residential units with operable and lockable doors, and pedestrian-scale features such as lighting and weather protection. Private amenity areas shall also be provided and may include enclosed courtyards with a combination of planters and low decorative fencing.
  - The design for buildings facing Street ‘A’ shall be defined by complementary changes in building articulation and design above the 4<sup>th</sup> floor that will contribute to the provision of a human-scaled environment along the street. Potential design solutions may include a step-back, balconies or outdoor areas, which would provide proportionate step-backs from the front face of the building.
- a) Regulations:
  - i) Height (maximum) 22 metres (72 feet) (6-storeys)
  - ii) Ground Floor Height (minimum) 4.5 metres (14.76 feet)
  - iii) Dwelling units shall be permitted on the first floor of internal apartment buildings, continuum-of-care facilities, retirement lodge or retirement homes and nursing homes, this does not include apartment buildings, continuum-of-care facilities, retirement lodge or retirement homes and nursing homes fronting Hyde Park Road which shall be mixed-use buildings. (Z.-1-202869)

**B-69 122 Base Line Road West**

The Bonus Zone shall be implemented through a development agreement to facilitate the development of a low-rise apartment building with a maximum height of 4-storeys, 61 dwelling units and density of 100 units per hectare and provides:

- i) Affordable Housing
 

The provision of a minimum 30% of each unit type (i.e. type by number of bedrooms) set aside at affordable rent of approximately 70% of Average Market Rent. Affordable rental units will be established by an agreement entered into with the Corporation of the City of London, which will secure those units for a minimum twenty (20) year term.
- ii) Design Principles
 

Implementation of a site development concept, to be implemented through a future development agreement, which substantially achieves design principles that include:

  1. Building footprint and spatial orientation that: serves to activate the street, is pedestrian in scale; and, establishes safe, direct and

barrier-free accessible pedestrian connections throughout the Site and from the Site to the public realm;

2. A principle building entrance that further serves to activate the streetscape and reinforce the “front facing” built form;
3. A building footprint that mitigates impacts, noting an enhanced rear yard setback and enhanced interior side yard setback are identified in the Bonus Zone;
4. A parking area that provides for safe, direct and barrier-free accessible pedestrian connections; is suitably sized to accommodate projected demand; and, is strategically located to minimize impacts on the public realm;
5. An outdoor amenity area that is sufficiently-sized and strategically located to provide for privacy and additional buffering opportunities and plantings, and also serves to mitigate overland flows and other potential stormwater management (SWM) impacts; and
6. Maintain, to the greatest extent possible, on-site green infrastructure in a manner consistent with the findings of the preliminary Tree Preservation Plan.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

a)	Regulations	
iii)	Density (Maximum)	100 units per hectare (247 units per acre)
iv)	Parking (Minimum)	61 Parking Spaces
v)	Bicycle Parking (Minimum)	15 Parking Spaces
vi)	Rear Yard Depth (Minimum)	15.0 metres (49.2 feet)
vii)	Interior Side Yard Depth (Minimum)	3.0 metres (9.8 feet) for building walls containing no windows to habitable rooms or 8.0 metres (26.2 feet) minimum for building walls with windows to habitable rooms. (Z.-1-202874)

#### **B-70 1150 Fanshawe Park Road East**

The Bonus Zone shall be implemented through one or more agreements to facilitate a high quality development comprised of stacked townhouses and an apartment building, with a maximum height of 6 storeys (21m), and density of 133 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law and provides for enhanced urban design and affordable housing.

- i) The provision of affordable housing shall consist of:
  - A total of six (6) stacked townhouse affordable rental units;
  - Rents not exceeding 90% of the Average Market Rent (AMR) for the London Census Metropolitan Area as determined by the CMHC at the time of building occupancy;
  - The duration of affordability shall be set at 20 years from the point of initial occupancy of all affordable units.

The following special regulations apply within the bonus zone:

- a) Regulations
  - i) Frontage (Minimum) 22 metres (72 feet)
  - ii) Front yard depth (Minimum) 3 metres (9.8feet)
  - iii) Exterior yard depth (Minimum) 2 metres (6.6 feet)
  - iv) Interior side yard depth (Minimum) 4.5 metres (14.7 feet)
  - v) Density (Maximum) 133 units per hectare
  - vi) Height (Maximum) 21 metres (68.9 feet)
  - vii) Parking Spaces (Minimum) 110 spaces
  - viii) West Parking area depth (Minimum) 9.5 metres (31.2 feet)
  - ix) South Parking area depth (Minimum) 15 metres (49.2 feet)
  - x) Any permitted convenience commercial and/or restricted office uses may be located within the apartment building (Z.-1-202891)

#### **B-71 1761 Wonderland Road North**

The Bonus Zone shall be implemented through one or more agreements to facilitate the development of a high quality mixed-use commercial/residential apartment building with a maximum density of 226 units per hectare and a maximum height of 63 metres (17-storeys) which substantially implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law in return for the following facilities, services and matters:

- (a) Provision of Affordable Housing including:
  - A total of twelve (12) one-bedroom units, including a maximum of two (2) accessible one-bedroom units, established by agreement not exceeding 85% of the CMHC Average Market Rent (AMR) for a duration for affordability set at 50 years from initial occupancy.

- (b) A high quality development which substantially implements the site plan and elevations as attached in Schedule “1” to the amending by-law:

##### **Building Design**

- i) High quality architectural design (building/landscaping) including a common design theme for residential and commercial elements; and provision of structure parking facilities and screening for surface parking areas.

##### **Underground Parking**

- i) Underground Parking Structure parking provided to reduce surface parking areas (a minimum 189 subsurface spaces provided).

##### **Outdoor Amenity and Landscaping**

- i) Common outdoor amenity area to be provided in the northeast quadrant of the site; and rooftop terraces above the 4th, 16th and 17th floors.

- ii) Landscape enhancements beyond City design standards, including theme lighting and public seating at strategic locations.
- iii) Large caliper boulevard trees planted with a minimum 100mm caliper and a minimum distance of 10m between tree planting for the extent of the Wonderland Road North frontage.
- iv) Landscape plans for common outdoor amenity areas to incorporate hard landscape elements and drought resistant landscaping to reduce water consumption.

Sustainability

- i) Four electric vehicle charging stations within the publically accessible surface parking area, as well as 16 charging stations within the parking garage.
  - ii) Dedicated areas for bicycle parking along the Wonderland Road North frontage (with convenient access to building entrances). Secure bicycle storage within the structured parking facility. Walkway connections from the tower podium and surface parking filed to provide connectivity to Wonderland Road North bike lanes.
- (c) Public Transit
- i) The financial contribution of funding towards construction of transit shelters in close proximity to Wonderland Road North/Fanshawe Park Road West intersection in the amount of \$10,000 to promote bus ridership.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
  - i) Density (maximum) 226 uph
  - ii) Height (maximum) 63 metres
  - iii) Off Street Parking (minimum) 322 spaces
  - iv) Non-residential space within an apartment building on the first and second floor (maximum) 1,200 m<sup>2</sup>
  - v) Gross floor area individual permitted non-residential use (maximum) 600m<sup>2</sup>
  - vi) *Notwithstanding the compound zoning permissions of Section 3.9.1) of the Zoning By-law this zone variation is excluded*
  - vii) Additional Permitted Use: Pharmacy (Z.-1-212894)

**B-73 3080 Bostwick Road (Site 2)**

The Bonus Zone applying to Block 2 in the proposed plan of subdivision shall be enabled through one or more agreements to facilitate the development of a 189 unit residential apartment building with a maximum height of 18 storeys, and sixteen (16) stacked townhouse dwelling units with a maximum height of 15.0 metres, and a maximum overall density of 205 units per hectare, which generally implements in principle the site concept and elevation plans attached as Schedule “1” to the amending by-law, with further refinements to occur through



the site plan approval process, in return for the following facilities, services and matters:

- i) High quality architectural design (building/landscaping) including a common design theme applied to street boulevards. Design elements are to have regard for the Urban Design Guidelines prepared for 3080 Bostwick Road;
- ii) Underground parking to reduce surface parking requirements. Surface parking spaces are to be largely dedicated for visitor parking;
- iii) Large caliper boulevard tree planting with a minimum 100 mm caliper and a minimum distance of 10 m between tree planting for the extent of the site frontage for Bostwick Road and both sides of Street A as early as site construction allows;
- iv) Construction of one accessible electric vehicle charging station located on the Bostwick Community Centre lands or in a publicly accessible location of Block 2;
- v) Construction of one transit shelter along the Bostwick Road frontage, or the commensurate financial equivalent for the feature;
- vi) Construction of ten (10) publicly accessible bicycle share facilities/spaces.

The following special regulations apply within the bonus zone:

- a) Regulations:
  - i) Density (Maximum): 205 units per hectare
  - ii) Height (Maximum):
    - Apartment Building 75.0 metres (18 storeys)
    - Stacked Townhouses 15.0 metres
  - iii) Front Yard Depth (Minimum) 5.5 metres
  - iv) Exterior Side Yard Depth (Minimum): 1.0 metre
  - v) Rear Yard Depth (Minimum): 22.0 metres  
(Z.-1-212921)

**B-74 3080 Bostwick Road (Site 6)**

The Bonus Zone applying to Block 6 in the proposed plan of subdivision shall be enabled through one or more agreements to facilitate the development of two (2) residential apartment buildings having a total of 387 dwelling units, with a maximum height of 17 storeys, and a maximum density of 320 units per hectare, which generally implements in principle the site concept and elevation plans attached as Schedule “2” to the amending by-law, with further refinements to occur through the site plan approval process, in return for the following facilities, services and matters:

- 1) Provision of Affordable Housing
  - i) The affordable housing shall consist of a total of thirty (30) rental apartment dwelling units, which shall include nineteen (19) one-bedroom units and eleven (11) two-bedroom units;
  - ii) Rents shall be set at 85% of the CMHC Average Market Rent (AMR) for the London CMA at the time of occupancy;
  - iii) The period of affordability will be identified as being thirty (30) years from the point of initial occupancy;
  - iv) The Proponent shall enter into a Tenant Placement Agreement

(TPA) with the City of London to align the nineteen (19) one-bedroom units and eleven (11) two-bedroom units with priority populations.

- v) These conditions shall be secured through an agreement registered on title with associated compliance requirements and remedies
- 2) High quality architectural design (building/landscaping) including a common design theme applied to street boulevards. Design elements are to have regard for the Urban Design Guidelines prepared for 3080 Bostwick Road. Underground parking to reduce surface parking requirements.

The following special regulations apply within the bonus zone:

- a) Regulations:
  - i) Density (Maximum): 320 units per hectare
  - ii) Height (Maximum): 75.0 metres (17 storeys)
  - iii) Front Yard Depth (Minimum): 3.0 metres
  - iv) Interior Side Yard Depth (Minimum): 6.0 metre
  - v) Rear Yard Depth (Minimum): 7.5 metres  
(Z.-1-212921)

#### **B-75 611-165 Third Street**

The Bonus Zone shall be implemented through one or more agreements to facilitate the development of a residential apartment building, with a maximum height of 4-storeys and a maximum density of 96 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law, and provides for affordable housing. The affordable housing component shall consist of:

- A total of three (3), three-bedroom units and one (1), one-bedroom unit, including one (1) accessible three-bedroom unit and one (1) accessible one-bedroom unit;
- Rents for the three (3), three-bedroom units and one (1), one bedroom unit be set at 80% of the CMHC Average Market Rent (AMR) for the London CMA at the time of occupancy;
- That the period of affordability be identified as being thirty (30) years from the point of initial occupancy; and,
- That the Proponent enter into a Tenant Placement Agreement (TPA) with the City of London to align the three (3), three-bedroom units and one (1), one-bedroom unit with priority populations.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
  - i) Height (Maximum): 15.8 metres (51.8 feet)
  - ii) Density (Maximum): 96 units per hectare  
(Z.-1-212922)

**B-76 1634 – 1656 Hyde Park Road, 1480 North Routledge Park and Part of 1069 Gainsborough Road**

The Bonus Zone shall be implemented through one or more agreements to facilitate the development of a mixed-use apartment building, with a maximum height of 8-storeys or 29 metres and a maximum density of 169 units per hectare, in general conformity with the Site Plan, Renderings, Elevations and Cross Sections attached as Schedule “1” to the amending by-law, and provides for the following:

- a) Exceptional Building Design:
  - i) providing an ‘L’-shaped mixed-use building that is generally in keeping with the vision of the current Official Plan as well as the London Plan by providing for continuous street walls along the Hyde Park Road and North Routledge Park frontages;
  - ii) providing a 7-storey massing along Hyde Park Road that includes a significant step-back above the second storey and 8-story massing along North Routledge Park;
  - iii) providing for appropriate scale/ rhythm/ materials/ fenestration;
  - iv) incorporating all of parking in the rear yard and underground, away from the adjacent street frontages;
  - v) providing ground floor commercial space with transparent glazing and principal entrances facing the Hyde Park Road creating an active edge;
  - vi) providing ground floor residential units with individual entrances and patio spaces along the North Routledge Park frontage;
  - vii) providing a rooftop patio;
  - viii) providing a parking lot layout that accommodates appropriate driveway alignments across North Routledge Park; and
  - ix) relocating the existing heritage structure and providing a glass link between the heritage structure and the new building along the North Routledge Park frontage and a recessed courtyard immediately south of the heritage structure.
- b) Provision of Affordable Housing:
  - i) A total of five (5) one-bedroom units will be provided for affordable housing. Subject to the concurrence of the City, some or all of these five (5) one-bedroom units may be allocated from the adjacent development owned and/or managed by the Proponent, noting the bonus zone requirement and encumbrance would remain specific to the Subject Lands;
  - ii) Rents not exceeding 80% of the Average Market Rent (AMR) for the London Census Metropolitan Area as determined by the CMHC at the time of building occupancy;
  - iii) The duration of affordability set at 50 years from the point of initial occupancy;
  - iv) The proponent enters into a Tenant Placement Agreement (TPA) with the City of London to align the affordable units with priority populations;
  - v) These conditions to be secured through an agreement registered on title with associated compliance requirements and remedies.

- c) Relocation, conservation, and adaptive re-use of the existing heritage designated structure at 1656 Hyde Park Road:
  - i) The owner shall enter into a Heritage Easement Agreement with the City of London.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- c) Additional Permitted Use:
  - i) Apartment buildings, including dwelling units in the front portion of the ground floor adjacent to North Routledge Park.
- d) Regulations:
  - i) Density (Maximum): 169 units per hectare
  - ii) Building Height (Maximum): 29 metres
  - iii) Front Yard Depth from North Routledge Park to relocated heritage structure (Maximum): 6.0 metres
  - iv) Parking – All commercial uses (Minimum): 1 space per 20m<sup>2</sup>
  - v) Parking – All residential uses (Minimum): 1 space per unit (Z.-1-212941)

**B-78 400 Southdale Road East**

The Bonus Zone shall be implemented through one or more agreements to facilitate the development of a residential apartment building, with a maximum height of 7-storeys plus mechanical (29.2m) and a maximum density of 462 units per hectare, which substantively implements the Site Plan and Elevations attached as Schedule “1” to the amending by-law, and provides for affordable housing in return for the following facilities, services and matters:

1. Exceptional Building Design

The building design shown in the various illustrations contained in Schedule “1” of the amending by-law is being bonused for features which serve to support the City’s objectives of promoting a high standard of design.

- The building oriented to the corner of Southdale Road East and Dundalk Street providing a well-defined built edge and creating a positive public interface and human scale at street level;
  - The inclusion of building step backs, from 7-storeys to 6-storeys and 5-storeys with a variety of building materials and building articulation to break up the massing of the building;
  - Purpose-designed amenity space on top of the 7-storey apartment building and parking structure;
2. Underground parking
3. Provision of Affordable Housing by requiring that LJM Developments enter into an agreement with the Corporation of the City of London (“the City”) to facilitate the transfer of ownership at no cost of four (4) new one-bedroom condominium units constructed within the development for the purposes of affordable housing, in a form prescribed by the City.

It being noted that a future development agreement will provide for the four new one-bedroom units and will include the following through further agreements as necessary:

- Assurances of the specific location, size, fixtures, and features of the bonus units are defined as to the City's satisfaction. This includes any common and general attributes, (such as storage lockers, parking, or other building resident amenities) for each bonus unit.
- A purchase agreement, inclusive of securities as applicable, reflecting the process for the no-cost transfer of the 4 new one-bedroom units and any associated services and features upon condominium plan registration, in a form satisfactory to the City.
- Confirmation that the associated condominium declaration and by-laws shall in no way limit the use and function of the units for affordable rental housing in accordance with applicable residential rental laws.

It is further recognized that, upon ownership, the City will retain and maintain the units within the function and business of affordable rental housing as managed through the City's Housing Stability Services. The City, as owner, would therefore be required to address costs associated with condominium and other standard fees. These factors have been considered within the bonus provisions and will be subject to separate reporting and details. The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

b) Permitted Uses:

- i) Apartment building

c) Regulations:

- |       |  |                       |
|-------|--|-----------------------|
| i)    | Height<br>(Maximum):   | 29.2 metres           |
| ii)   | Density<br>(Maximum):  | 462 units per hectare |
| iii)  | Front Yard Setback<br>(Minimum):   | 1.0 metres            |
| iv)   | Exterior Side Yard Setback<br>(Minimum):   | 1.0 metres            |
| v)    | Interior Side Yard Setback<br>(Minimum):   | 9.84 metres           |
| vi)   | Rear Yard Setback<br>(Minimum):  | 0.65 metres           |
| vii)  | Landscaped Open Space<br>(Minimum):  | 15%                   |
| viii) | Aisle Width for Access and Driveway<br>(Minimum):  | 6.5 metres            |
| ix)   | Parking<br>(Minimum):  | 1.06 spaces per unit  |
| x)    | Accessible Parking<br>(Minimum):   | 7 spaces              |
| xi)   | Bicycle Parking 0.45m (width) x 1.1m (height) x 1.85m (length) where as 06.m (width) x 1.5m (height) x 1.9m (length) is required   |                       |
| xii)  | Balcony yard encroachment of 1.8m in all yards, no closer than 1.05m to the lot line whereas 1.5m balcony yard encroachment in all yards, no closer than 3.0m to the lot line is required<br>(Z.-1-212954) |                       |

**B-79                    1047 – 1055 Dearness Drive**

The Bonus Zone shall be implemented through one or more agreements to facilitate the development of a residential apartment building, with a maximum height of 6-storeys measuring up to 26 metres and a maximum density of 134 units per hectare, in general conformity with the Site Plan, Renderings, Elevations and Views attached as Schedule “1” to the amending by-law, and provides for the following:

- a) Exceptional Building and Site Design:
  - i) a built form located along Bradley Avenue and Dearness Drive that establishes a built edge with street-oriented units and active uses along those frontages;
  - ii) an architectural feature/massing/building articulation that addresses and emphasizes the intersection of Bradley Avenue and Dearness Drive;
  - iii) an active edge along the Bradley Avenue frontage including a well-defined principal entrance and individual front entrances to the ground floor apartment units;
  - iv) lockable “front door” style ground floor doors that open into ground floor private amenity spaces designed to extend into setbacks as front porches or courtyards;
  - v) ground floor patio enclosures using semi-transparent materials with a height of no more than 1 metre to provide views and passive surveillance into the public streetscape;
  - vi) a step-back above the 5<sup>th</sup> storey for a portion of the building along both street frontages providing a human-scale along the streets;
  - vii) articulated facades including recesses, projections, balconies and terraces to provide depth and variation in the built form to enhance the pedestrian environment;
  - viii) a variety of materials, textures and articulation along building façade(s) to highlight different architectural elements and provide interest and human-scale rhythm along the street frontages;
  - ix) a significant setback from the property to the north aiding smooth transition from the mid-rise building to the low-rise residential to the north;
  - x) common outdoor amenity space at ground level and using rooftop terraces located to protect the privacy of adjacent properties.
  - xi) an enhanced landscape buffer for the length of the north property line, between the parking ramp and the properties to the north, as well as an enhanced buffer to screen parking where it is visible from the street, noting that the Access Management Guidelines will require that the location of the proposed driveway, parking area and ramp will deviate from the locations shown on Schedule “1”; and,
  - xii) limited surface parking located away from the major street frontage and providing most of the parking within an underground structure.
- b) Provision of Affordable Housing
  - i) A total of two (2) one-bedroom units and two (2) two-bedroom units will be provided for affordable housing;
  - ii) Rents not exceeding 85% of the Average Market Rent (AMR) for the London Census Metropolitan Area as determined by the CMHC at the time of building occupancy;
  - iii) The duration of affordability set at 50 years from the point of initial occupancy;
  - iv) The proponent enter into a Tenant Placement Agreement (TPA) with the City of London to align the affordable units with priority

populations;

- v) These conditions to be secured through an agreement registered on title with associated compliance requirements and remedies.

The following special regulations apply within the bonus zone upon the execution and registration of the required development agreement(s):

- a) Regulations:
  - i) Density (Maximum): 134 units per hectare
  - ii) Building Height (Maximum): 6 storeys up to 26 metres (85.3 feet)
  - iii) Interior Side Yard Depth (Minimum): 2.9 metres (9.5 feet)
  - iv) Exterior Side Yard Depth (Minimum): 4.0 metres (13.1 feet)
  - v) The as-of-right bonusing permitted in Table 13.3, Row 16 shall not apply (Z.-1-212956)

#### 4.4 BUILDING ADDITIONS

Where the erection of one or more additional storeys is proposed on a building where the existing yards appurtenant to such building would not conform to the requirements of this By-Law subsequent to such erection, no such erection shall be permitted except where:

- 1) the addition is located so as to comply with the yard requirements of this by-law at the level of the base of such addition; and,
- 2) the addition complies in all other respects with the provisions of this by-law.

Where a shipping container or trailer (both not exceeding 12.1 metres (40 feet) in length x 2.4 metres (8 feet) in width x 2.9 metres (9.5 feet) in height) is to be used as the sole base of operations for a non-residential business, its installation shall be subject to all regulations of the underlying zone and to Site Plan Control.

Where a shipping container or trailer (not exceeding 12.1 metres (40 feet) in length x 2.4 metres (8 feet) in width x 2.9 metres (9.5 feet) in height) is to be used as additional floor space (or permanent storage) to an existing establishment in any multi-family (3 or more units) or non-residential zone, the following regulations shall apply:

- 1) the location, floor area, setbacks and all other regulations of the underlying zone shall apply;
- 2) it shall only be permitted in the rear yard;
- 3) the installation is subject to Site Plan Control(except for Agricultural uses which are not subject to Site Plan Control); and,
- 4) no more than two (2) shipping containers or trailers not exceeding 6 metres (20 feet) in length x 2.4 metres (8 feet) in width x 2.6 metres (8.5 feet) in height; or one (1) shipping container or trailer not exceeding 12.1 metres (40 feet) in length x 2.4 metres (8 feet) in width x 2.9 metres (9.5 feet) in height shall be permitted per property.

Shipping containers (as permanent structures) shall be prohibited on any property designated under Part IV or Part V of the Ontario Heritage Act.

## 4.5 TEMPORARY STRUCTURES

### 1) CONSTRUCTION USE

The following construction uses shall be permitted only so long as the same are necessary for construction work in progress which has neither been finished nor abandoned: construction camps, model home sales office, tool sheds, scaffolds, or any other building or structure incidental to and necessary for the construction work on the lot. No construction use, however, shall be located within the sight triangle of any lot.

### 2) SHIPPING CONTAINERS AND TRAILERS

Shipping Containers and Trailers may be used as temporary uses subject to the following regulations:

#### All Residential Zones

Time Period: Maximum of 2 months within a calendar year for the purposes of moving/relocating. Maximum of 4 months within a calendar year for the purposes of building renovation.

A maximum of two (2) (in total) shipping containers or trailers not exceeding 6 metres (20 feet) in length x 2.4 metres (8 feet) in width x 2.6 metres (8.5 feet) in height shall be permitted per property.

#### All Non Residential Zones

Time Period: Maximum of 4 months within a calendar year

Other Regulations: Yard setbacks as per the underlying zone. Gross Floor Area relating to the temporary use does not require additional on-site parking. A temporary reduction in the required parking may be permitted in association with the establishment of the temporary use.

No more than two (2) shipping containers or trailers not exceeding 6 metres (20 feet) in length x 2.4 metres (8 feet) in width x 2.6 metres (8.5 feet) in height; or one (1) shipping container or trailer not exceeding 12.1 metres (40 feet) in length x 2.4 metres (8 feet) in width x 2.9 metres (9.5 feet) in height shall be permitted per property.

### 3) MODEL HOMES

Model homes shall be permitted in all R1, R2, R3, R4, R5, and R6 (excluding apartment buildings) Zones subject to the following:

- (a) the lands on which the model homes are to be constructed have received draft plan approval under the Planning Act and the subdivision agreement has been executed;
- (b) site servicing may be permitted for model homes in accordance with the requirements of the Chief Building Official prior to the registration of the plan of subdivision;
- (c) the number of model homes for any phase of a draft approved plan of subdivision shall not exceed the lesser of ten (10) dwelling units or ten per cent (10%) of the total number of lots of the phase; and,
- (d) the number of model homes for any draft approved plan of subdivision shall not exceed the lesser of ten (10) dwelling units or ten per cent (10%) of the total number of lots.

### 4) FARM MARKET

A farm market, in which nothing may be sold or displayed except seasonal products from the farm upon which the market is located, is permitted in an agricultural zone subject to a maximum size of 18.6m (200 sq.ft.), a maximum height of 3.6m (12 ft.) and a minimum front yard of 7.5m (25 ft.). (Z.-1-172583)



#### **4.6 DWELLING UNITS**

##### **1) LOCATION WITHIN BASEMENT**

A dwelling unit or part thereof is permitted in a basement provided the finished floor level of such basement is not below the level of any sanitary sewer servicing the building or structure in which such basement is located.

##### **2) DWELLING UNIT AREA (MINIMUMS)**

No dwelling unit shall be erected or used unless it has the following minimum gross floor area:

- a) a dwelling unit constituting a single detached dwelling - 70.0 square metres (753 sq. ft.) or as specified within a zone;
- b) converted dwelling unit and additional residential unit - 25 square metres (269 square feet); (Z.-1-96417)(Z.-1-212896)
- c) any other dwelling unit - 37 square metres (398 square feet) for a bachelor dwelling unit plus an additional 10.0 square metres (108 square feet) for each bedroom.

#### **4.7 FOSTER HOMES**

A foster home is permitted in all Residential Zones and in any dwelling unit in an Agricultural (AG) zone. (Z-1-051390)

#### **4.8 GROUP HOMES**

Notwithstanding any other provision of this By-law to the contrary, a Group Home Type 1 is permitted to occupy the whole of an individual dwelling unit, other than dwelling units located within farmland areas, including a single detached dwelling, a semi-detached dwelling, a duplex dwelling, and a triplex dwelling provided the total number of residents within the building structure does not exceed eight, The dwelling must have 20.0 square metres (216 sq. ft.) of gross floor area per person residing within the unit. (Z.-1-202871)

Group Homes Type 2 and Supervised Residences are permitted only within the zones where specified as a permitted use. In order to prevent concentrations of Group Home Type 2 uses within specific areas, and notwithstanding their being listed as a permitted use, a minimum separation between such facilities shall be 450.0 metres (1,476 ft.) measured in any one direction.(Z.-1-172602)

#### **4.9 HEIGHT EXEMPTION**

Any height limitations of this By-Law shall not apply to place of worship spires, belfries, cupolas, mechanical penthouses and domes which are not used for human occupancy; nor to chimneys, ventilators, skylights, water tanks, solar collectors, windmills, bulkheads, hydro, radio, television or microwave towers and antenna and similar features or necessary mechanical appurtenances or electrical supply facilities usually situated above the roof level; nor to any industrial apparatus such as silos, cracking towers, or conveyors; nor any main agricultural buildings or structures, such as barns, grain elevators and storage bins, grain dryers or windmills. Such features, however, shall be erected only to such height as is necessary to accomplish the purpose they are to serve. (Z.-1-051390)

#### **4.10 HOME OCCUPATION**

For home occupations the character of the dwelling as a private residence shall not change and a nuisance such as noise, fumes, dust, odour, traffic or parking shall not be created which would interfere with the enjoyment of the residential

amenities of the neighbourhood. Home occupations are permitted in any dwelling unit within a single detached dwelling, semi-detached dwelling, duplex dwelling, converted dwelling, triplex dwelling, fourplex dwelling, townhouse dwelling and accessory farm dwellings subject to the following provisions: (Z.-1-94293) (Z.-1-051390)

- 1) The home occupation shall be clearly ancillary to the use of the dwelling unit and carried out by a resident of the unit.  
(O.M.B. File #R 910387 - Appeal #9002 June 4, 1993)
- 2) The floor area taken up by all home occupations shall be no more than 25 percent (25%) of the total floor area of the dwelling unit and shall be confined to one area and shall not exceed 28.0 square metres (301.4 square feet).
- 3) In no case shall any outdoor use or outdoor storage be permitted.  
(Z.-1-97466)
- 4) The home occupation shall not involve the use or employment within the dwelling unit of more than one person who does not reside in the dwelling unit. The function of such non-resident person shall be restricted to a secretarial, accounting, clerical or support role.
- 5) The home occupation shall not involve the use of the premises as a base of operations for persons who are employed by or associated with the home occupation, nor shall the premises be used to assemble or rally such persons for transportation to a work site.
- 6) No more than five persons shall be present in the dwelling unit at any time, or six persons in a provincially licensed Private Home Day Care located outside of the flood plain, to receive treatment, services or instructions. (Z.-1-162500)
- 7) No retail use of the dwelling unit shall be permitted as a home occupation.
- 8) No manufacturing, assembly or repair process shall be carried on except for the fabrication of hand made articles of clothing, arts or crafts.
- 9) No institutional use of the dwelling unit shall be permitted as a home occupation.
- 10) The use of a swimming pool shall not be permitted as a home occupation.  
(O.M.B. File #R910387 - Appeal #9002 June 4, 1993)
- 11) No food preparation or food service shall be permitted as a home occupation.
- 12) No animal, corpse bacteria or virus shall be brought into the dwelling in connection with a home occupation.
  - a) Notwithstanding Subsection 4.10 (12), a dog and domestic cat grooming business is permitted as a home occupation, subject to the following:
    - i) A maximum of three (3) dogs and two (2) domestic cats be allowed on the premises at any one time in conformity with the City of London Animal Control By-law (PH-3) and Dog Licensing and Control By-law (PH-4);
    - ii) No veterinary services be provided on the premises;
    - iii) No breeding, boarding or overnight accommodation of dogs or domestic cats for grooming be permitted on the premises;
    - iv) Only dogs and domestic cats that are licensed or identified as per the City of London Animal Control By-law (PH-3) and Dog Licensing and Control By-law (PH-4), or dogs and domestic cats that are legally licensed by any other municipality in the Province of Ontario, be

- v) allowed on the premises at any one time;
  - v) No dogs or domestic cats that are brought into the premises for grooming shall be permitted outside of the dwelling; and
  - vi) All other provisions of Section 4- General Provisions, Subsection 4.10, Home Occupation, shall apply. (Z.-1-061498)
- 13) No additional commercial vehicles shall be permitted in connection with a home occupation. (Z.-1-97466)
- 14) No lodging shall be provided in the dwelling unit in connection with a home occupation.
- 15) No delivery of materials to the dwelling unit shall be permitted from a vehicle which has a registered gross weight exceeding 5 000 kilograms (11,023 pounds).
- 16) No additional outdoor mechanical equipment, other than that normally associated with a residential use, shall be used, the operation of which would result in any undue noise, fumes, dust or odor escaping to any adjacent or neighbouring lot. (Z-1-051390)
- 17) The home occupation shall not require the creation of additional on-site parking spaces, except in accordance with this By-law. (Z.-1-051390)
- 18) A home occupation shall not be permitted in association with an additional residential unit. (Z.-1-212896)

#### **4.11 HOUSEHOLD SALES**

No household sale shall be permitted except where a premises has a dwelling unit, and there shall not be more than two household sales per annum at one location and no such sale shall exceed two consecutive days in duration. The sale of agricultural products grown on the premises shall be permitted where the premises has a dwelling unit and there shall be not more than 20 household sales per annum at one location and no such sale shall exceed two consecutive days in duration. (Z.-1-97466) (Z.-1-93173) (Z.-1-202857)

#### **4.12 LANDSCAPED OPEN SPACE**

Landscaped open space shall be provided in accordance with the zone provisions set out herein for each zone and the following general provisions:

- 1) Any part of a lot which is not occupied by buildings, structures, parking areas, loading spaces, driveways, excavations, agricultural uses or permitted outdoor storage areas shall be maintained as landscaped open space.
- 2) Except as otherwise specifically provided herein, no part of any required front yard or required exterior side yard shall be used for any purpose other than landscaped open space.
- 3) Where landscaped open space of any kind, including a planting strip, is required adjacent to any lot line or elsewhere on a lot, nothing in this By-Law shall apply to prevent such landscaped open space from being traversed by pedestrian walkways or permitted driveways.
- 4) No part of any driveway, parking area, loading space, stoop, roof-top terrace, balcony, swimming pool or space enclosed within a building, other than a landscaped area located immediately above an underground parking area, shall be considered part of the landscaped open space on a lot.

#### 4.13 LOADING SPACE REQUIREMENTS

##### 1) LOADING SPACES REQUIRED

The owner or occupant of any lot, building or structure within any RSA, CSA, NSA, ASA, HS, RSC, AGC, RRC or any industrial zone used or erected for any purpose involving the receiving, shipping, loading or unloading of animals, goods, wares, merchandise or raw materials, shall provide and maintain, on the same lot, facilities comprising one or more loading spaces in accordance with the provisions of this Subsection. (Z.-1-051390)

##### 2) LOADING SPACE REQUIREMENTS (MINIMUMS)

The number of loading spaces required on a lot shall be based on the total gross floor area of all the uses on the said lot for which loading spaces are required by Clause (1) of this Subsection, in accordance with the following:

- a) less than 250.0 square metres (2,691 square feet) of total gross floor area - 0 loading spaces;
- b) over 250.0 square metres (2,691 square feet) up to and including 2500.0 square metres (26,910 square feet) of gross floor area - 1 loading space;
- c) over 2500.0 square metres (26,910 square feet) up to and including 7500.0 square metres (80,729 square feet) of gross floor area - 2 loading spaces;
- d) over 7500.0 square metres (80,729 square feet) up to and including 14 000.0 square metres (150,695 square feet) of gross floor area - 3 loading spaces;
- e) over 14 000.0 square metres (150,695 square feet) of gross floor area - 3 loading spaces plus 1 additional loading space for each 10 000.0 square metres (107,639 square feet) or part thereof of total gross floor area in excess of 14 000.0 square metres (150,695 square feet).

##### 3) DIMENSIONS OF LOADING SPACES

A loading space required hereby shall have minimum dimensions of 3.6 metres (11.8 feet) by 20.0 metres (65.0 feet) and a minimum vertical clearance of 4.5 metres (15.0 feet) for Industrial Zones and 3.6 metres (11.8 feet) by 9.0 metres (30.0 feet) and a minimum vertical clearance of 4.25 metres (14.0 feet) for all other zones.

##### 4) LOCATION OF LOADING SPACES

Required loading spaces shall be provided on the same lot occupied by the building or structure for which the said loading spaces are required within the side or rear yard and shall not form a part of any street or lane.

##### 5) ACCESS TO LOADING SPACES

Access to loading spaces shall be provided by means of one or more unobstructed driveways which have a minimum unobstructed width of at least 3.6 metres (11.8 feet), regardless of the direction of traffic flow thereupon and provide sufficient space to permit the maneuvering of vehicles on the lot so as not to obstruct or otherwise cause a hazard on adjacent streets.

Vehicles may be permitted to manoeuvre into loading areas on Neptune Crescent, Sovereign Crescent, Sovereign Crescent between Gore Road and Trafalgar Street, and Tartan Drive between Sovereign Road and Neptune Crescent using the road allowance. (Z.-1-101963)

Vehicles may be permitted to manoeuvre into loading areas at 45

ArtisansCrescent using the Artisans Crescent road allowance. (Z.-1-122063)

On roads identified as “secondary collector” or “local street” in the Zoning By-law, and where the existing premises are zoned to permit Industrial use(s) vehicles may be permitted to manoeuvre into loading areas using the road allowance where necessary. Where a new building is proposed, the development shall provide sufficient space for the manoeuvring of vehicles on the lot so as not to obstruct or otherwise cause a hazard on adjacent streets. (Z.-1-172594)

On roads identified as “secondary collector” or “local street” in the Zoning By-law, and where the existing premises are zoned to permit Industrial use(s) vehicles may be permitted to manoeuvre into loading areas using the road allowance where necessary. Where a new building is proposed, the development shall provide sufficient space for the manoeuvring of vehicles on the lot so as not to obstruct or otherwise cause a hazard on adjacent streets.  
(Z.-1-172594)

#### **4.14 LOTS REDUCED BY PUBLIC ACQUISITION**

Where the area of a lot is reduced by means of an acquisition of part of the lot by any authority having power of expropriation, and where such acquisition causes the lot as reduced, or any building or structure existing lawfully on the lot on the date of such acquisition, to have a lot area, lot frontage, lot coverage, parking, area of landscaped open space, setback, front yard depth, side yard depth or rear yard depth that does not conform to the requirements hereof for the zone in which such lot is located, then nothing in this By-Law shall apply to prevent the continued use of the lot as reduced as if no such acquisition had taken place, provided that:

- 1) no further change is made in the dimensions, area or any other characteristics of the lot as reduced, subsequent to the date of such acquisition, that would increase the extent of the said non-conformity; and,
- 2) no building or structure or addition thereto is erected on the lot as reduced, subsequent to the date of such acquisition, except in accordance with all the provisions hereof for the zone in which such lot is located.

In the case of a road widening dedication, the land that will be dedicated shall be included in any calculation for the purpose of determining lot area, coverage, height, parking, landscaped open space, floor area, floor area ratio and density provided any building or structure is located wholly within the boundary of the land remaining after the dedication. (Z.-1-12101)

#### **4.15 SERVICES REQUIREMENT**

##### **1) MUNICIPAL SANITARY FACILITIES**

Notwithstanding any other provisions of this By-Law, including Section 4.15(2), Individual Sanitary Facilities, no land shall be used nor any buildings or structures erected or used unless, pursuant to an agreement made or condition imposed under Sections 41, 45, 50 or 53 of the *Planning Act, R.S.O. 1990, c. P.13*, or Section 50 of the *Condominium Act, R.S.O. 1990, c. C.26*, dealing with the provision of any of the municipal services as are set out in this By-Law, the municipal services to be provided are available to service the land, buildings or structures, as the case may be. (Z.-1-94236) (Z-1-051390)

##### **2) INDIVIDUAL SANITARY FACILITIES**

Individual sanitary facilities are permitted for the erection or enlargement of any building or structure not serviced by municipal sanitary sewerage facilities within the Agricultural Class of Zones, the Rural Settlement Commercial (RRC) Zone, some Residential R1 Zone variations including

R1-14 to R1-17 and Urban Reserve (UR) Zones or where the h-17 holding provision is applied. Issuance of a building permit shall be subject to the following provisions;

- a) The applicable requirements for the wastewater treatment system can be met and all approvals are obtained in writing from the authorities having jurisdiction; and,
- b) If the quantity of effluent is projected to exceed 4500 litres per day, a hydrological study is required to demonstrate that the system can operate satisfactorily on the site; and,
- c) For lands within an Urban Reserve (UR) Zone, development of a new dwelling unit shall also be subject to the following provision;
  - i) Municipal sanitary sewer service is not available within 150m of the property.  
(Z.-1-051390)

#### 4.16 EXISTING USES CONTINUED

- 1) Nothing in this By-law shall prevent the rebuilding or repair of a building or structure lawfully used on the 26th day of June, 2005 for a purpose that, were it not for Section 1.3 (Non-Conforming Uses), is prohibited by this By-law, except where,
  - a) the dimensions of the building or structure are not increased and the yards appurtenant thereto are not reduced except in accordance with the provisions of this By-law; and,
  - b) the building or structure is used for a use which is contained in the list of permitted uses for the zone in which the building or structure is located.  
(Z.-1-051390)
- 2) Nothing in this By-law shall prevent an extension of or addition to a building or structure lawfully used on the 26th day of June, 2005 for a purpose that, were it not for Section 1.3, is prohibited by the By-law, except where,
  - a) the extension or addition and any existing floor area devoted to the same purpose complies with the regulations of this By-law, pertaining to the zone in which the use is located and any other applicable regulations of this By-law; and,
  - b) the minimum yard or setback required for the extension or addition shall be equal to the minimum yard or setback prescribed in the regulations of this By-law; and,
  - c) the building or structure is used for a use which is contained in the list of permitted uses for the zone in which the building or structure is located.  
(Z.-1-051390)
- 3) A lot which was vacant and could be separately conveyed in accordance with the provisions of the *Planning Act* on the 26th day of June 2005 which has an area or frontage less than that required by the applicable regulations of this By-law is hereby deemed to be in conformity with those regulations if the area and frontage of that lot is not less than 80 percent of the minimum area and frontage otherwise required by the regulations. (Z-1-051390)
- 4) Where the development of any land is approved under Section 41 of the *Planning Act* and, pursuant to a consent subsequently given under Section 53 of the Act or pursuant to an approval or exemption subsequently given under Section 50 of the *Condominium Act*, any part of the land is conveyed or otherwise dealt with, the regulations of this By-law

are hereby deemed to apply to the land as a whole and to any building or structure thereon in the same manner and to the same extent as if the consent, approval or exemption was not given, so long as the land and any building or structure thereon are used and continue to be used for the purpose for which the development was approved.  
(Z.-1-95377)

#### **4.17 OPEN STORAGE**

##### **1) OPEN STORAGE REGULATIONS**

Except as otherwise provided in Clause (2) of this Subsection, no open storage shall be permitted on any lot in any zone, except in accordance with the following provisions:  
(Z.-1-051390)

- a) No open storage area shall be permitted in any front or exterior side yard, except for the outside display and sales area for goods and materials that are in conjunction with a permitted commercial or industrial use.
- b) No open storage area shall be visible from any street or from any adjacent lot, where such adjacent lot is located in a zone other than a Commercial Zone or an Industrial Zone, and to this end any open storage area shall be screened, wherever necessary in order to comply with this provision, by a planting strip containing an opaque fence, wall or other opaque barrier not less than 2.0 metres (6.6 ft.) in height. (Z-1-051390)
- c) Any open storage area shall be maintained as landscaped open space or provided and maintained with a stable surface, treated so as to prevent the raising of dust or loose particles and drained in accordance with the requirements of the Corporation.
- d) Notwithstanding Paragraph (c) of this Clause, no open storage area shall be considered part of any required landscaped open space.
- e) No parking spaces or loading spaces required by this By-Law shall be used for open storage purposes.

##### **2) SPECIAL EXCEPTIONS**

Nothing in Clause (1) of this Subsection shall apply to prevent or otherwise restrict the use as an open storage area of any part of:

- a) the front and/or exterior side yard of a lot containing an agricultural use for a farm market for produce grown on the said lot or other open storage accessory to the agricultural use; or  
(Z.-1-051390)
- b) a lot containing a single dwelling, for a special temporary sale, by auction or otherwise, of personal possessions belonging to the occupant thereof.

#### **4.18 OUTDOOR PATIO ASSOCIATED WITH A RESTAURANT OR TAVERN (Z.-1-051390)**

Notwithstanding any other provisions of this By-Law, the following shall apply to an outdoor patio associated with a permitted restaurant:

##### **1) CAPACITY**

No outdoor patio shall accommodate more than 50 percent (50%) of the licenced capacity of the restaurant with which the patio is associated, or 50 persons, whichever is the greater.

2) LOCATION

- a) No outdoor patio shall be permitted where any lot line adjoins lands which are in a residential zone class which is not in combination with another zone, or is separate therefrom by a lane.
- b) Notwithstanding the provisions of Paragraph (a), where only the rear lot line adjoins a residential zone class which is not in combination with another zone, or is separated therefrom by a lane, an outdoor patio shall be permitted in the front yard.
- c) No outdoor patio shall be located above the elevation of the floor of the first storey of the principal building where the lot adjoins a residential zone class which is not in combination with another zone, or is separated therefrom by a lane.

3) LIGHTING

Any outdoor lighting shall be directed toward or onto the patio area and away from adjoining properties and streets.

4) LOADING

Notwithstanding Subsection 4.13 (Loading Space Requirements), no loading space shall be required for an outdoor patio restaurant.

5) PARKING

Parking spaces shall be required for the gross floor area associated with the outdoor patio at the same ratio as for restaurants.

There is no parking requirement for those outdoor patios associated with an adjacent restaurant zoned Downtown Area (DA).  
(Z.-1-97534)

6) SEASONAL OUTDOOR PATIOS

- a) No seasonal outdoor patio shall be permitted between November 16 and March 14, inclusive;
- b) All structures and appurtenances associated with a seasonal outdoor patio must be removed between November 16 and March 14, inclusive;
- c) Notwithstanding Section 4.18(2), seasonal outdoor patios shall be setback a minimum of 6.0 metres from any residential zone which is not in combination with another zone;
- d) Notwithstanding Section 4.18(5), there is no parking requirement for seasonal outdoor patios;
- e) Notwithstanding Section 4.19, seasonal outdoor patios are permitted within required parking spaces for commercial uses; and,
- f) No seasonal outdoor patio shall be located within required parking spaces for residential dwelling units.  
(Z.-1-212919)

**4.19 PARKING**

1) CALCULATION OF PARKING REQUIREMENTS

- a) Where a part of a parking space is required in accordance with this By-Law, such part shall be considered one parking space for the purpose of calculating the total parking requirements for the said use.
- b) Parking spaces required in accordance with this By-Law shall not include any parking spaces used or intended to be used primarily for the storage or parking of vehicles for hire or gain, display or sale.



2) DIMENSIONS OF PARKING SPACES

- a) A parking space required hereby shall have minimum dimensions of 2.7 metres by 5.5 metres, except as required for parking spaces for the use of persons with disabilities.
- b) A Type 'A' parking space for the use of persons with disabilities shall be a minimum width of 3.4 metres by at least 5.5 metres in depth.
- c) A Type 'B' standard parking space for the use of persons with disabilities shall be a minimum width of 2.4 metres by at least 5.5 metres in depth.  
(Z.-1-142353)

2.1) ACCESS AISLES

Accessible aisles, that is, the space between parking spaces that allows persons with disabilities to get in and out of their vehicles, must be provided for all parking spaces for the use of persons with disabilities in off-street parking facilities. (Z.-1-142353)

3) LOCATION OF PARKING AREAS

With the exception of the Commercial Zones, all required parking spaces shall be provided on the same lot occupied by the building, structure or use for which such parking spaces are required, and shall not form a part of any street or lane. Within the Commercial Zones, the required parking spaces may be supplied within 150.0 metres (490 feet) of the main pedestrian access of the building, structure or use for which the parking spaces are required, provided a Site Plan Agreement is registered on title of the lands used for parking committing said parking spaces to the related commercial site.

4) YARDS WHERE PARKING AREAS PERMITTED

- a) No person shall use any land or cause or permit the use of any land situated in any zone for the purpose of parking or storage of a vehicle in any front yard or exterior side yard.
- b) No person shall use any land or cause or permit the use of any land situated in any zone for the purposes of uncovered surface parking areas in any front yard or exterior side yard.
- c) Notwithstanding the yard and setback provisions of this By-law to the contrary, uncovered surface parking areas that conform to the provisions of Subsection 4.19(7) (Surface and Drainage of parking Areas and Driveways) of this By-law, shall be permitted in the yards or in the area between the required road allowance and the required setback as follows:

**Zone Class**

Office,  
 Commercial,  
 Institutional,  
 Open Space,  
 Recreational ,  
 Agricultural,  
 Agricultural Commercial &  
 Rural Settlement  
 Commercial Zone  
 and any Zone  
 Class not  
 specified  
 (Z-1-051390)

**Yard in Which Required Parking Area is Permitted**

All yards provided that no part of any parking area, other than a driveway, is located closer than 3.0 metres (9.8 feet) to any required road allowance except in the DA Zones, the BDC Zones or the OC Zones where parking shall not be permitted in the front yard.

Residential  
- R1, R2, R3, R4  
and R11 Zones.

- (a) The interior side yard and rear yard, provided that no part of any parking area is located closer than 1.0 metres (3.3 feet) to any required road allowance and provided that no part of any rear yard parking area shall be located closer than 3.0 metres (9.8 feet) from the rear lot line and 3.0 metres (9.8 feet) from any one side lot line except where access to a rear yard parking area is obtained by a lane in which case no part of any rear yard parking area shall be located closer than 3.0 metres (9.8 feet) from each side lot line; and. For the purposes of this By-law, where a Private Garage is an accessory use to a permitted use on a lot, such Private Garages shall be exempt from this regulation but shall comply with the provisions for accessory uses set out in Subsection 4.1, (Z.-1-122125, OMB Order PL121003, July 22, 2013) (z.-1-132233)
- (b) Driveways in the front yard or driveways in the exterior side yard.

Residential  
- R5, R6, R7, R8, R9  
and R10 Zones.

Deleted by Z.-1-97552

Industrial

All yards provided that no part of any parking area, other than a driveway, is located closer than 1.0 metre (3.3 feet) to any required road allowance. (Z.-1-95366)

## 5) PARKING STRUCTURES

Where a parking area located in a structure is accessory to a permitted use on a lot, then such structure shall not be considered an accessory building and shall conform to all the provisions for the zone as a main building.

## 6) ACCESS AND DRIVEWAYS TO PARKING AREAS AND SPACES

- a) For single, semi-detached, duplex, or converted two unit dwellings, a parking area/private garage can only be accessed by driveways leading to all or a portion of a parking area/private garage which is provided from an improved street by means of one or more unobstructed driveways not less than 2.7 metre (8.8 feet) in width. For the purposes of this section, a driveway includes a walkway connected and parallel to a driveway and shall form part of the driveway leading to all or a portion of a parking area/private garage as part of the driveway widths.

Notwithstanding Section 4.19 (4), driveways and/or walkways connected to and parallel to a driveway may project no greater than 2.0 metres in the front yard of a single detached, semi-detached,

duplex or converted two unit dwelling. That part of the required front yard not used for a driveway or walkway, or where permitted by this by-law, a parking area/space, shall be used exclusively for landscaped open space.

The driveway width shall be measured parallel to the said street at the street line. It shall be further provided that no lot shall have more than two driveways for the first 30.0 metres. (98.7 feet) of street line.

For agricultural uses, driveways to parking areas shall be provided from an improved street by means of one or more unobstructed driveways not less than 3.0m (9.8 ft.) in width and not more than 10.0 m (32.8 ft.) in width. The maximum number of driveways shall be not more than two driveways for the first 100m (328 ft.) of lot frontage plus one driveway for each additional 100m (300 ft.) of lot frontage thereafter. (Z.-1-142314)

- b) For uses subject to site plan control, the driveway widths shall be determined in the site plan approval process and agreement. For uses other than those described in Subsection 4.19(6)(a) and not subject to site plan approval, driveways and parking aisles shall have a minimum unobstructed width of 6.7 metres (21.9 feet) where two-way traffic is permitted and 3.0 metres (9.8 feet) where only one-way direction of traffic flow is permitted and is clearly indicated by signs, pavement markings or both but does not apply to stacked parking. (Z.-1-132179)  
(Z.-1-95366)
- c) Each required parking space shall be readily accessible at all times for the parking or removal of a vehicle and vehicular access to any such parking space shall not be impeded by any obstruction except as provided in Paragraph (d) of this Clause, except that this provision shall not apply to prevent the use as a parking space of any part of a driveway accessory to a single dwelling, semi-detached dwelling, duplex dwelling townhouse dwelling or stacked parking provided that no parking space shall obstruct access to a parking area on any other lot or of any other unit and for the use of stacked parking, to provide required parking, that the following shall be required:
  - i) stacked parking be located wholly within a structure
  - ii) a development agreement be entered into which includes:
    - i. implementation of all mitigation measures recommended in a Traffic Impact Analysis which includes a functional parking study to the satisfaction of the City;
    - ii. implementation of all mitigation measures recommended in a noise and vibration study to the satisfaction of the City; and
    - iii. a qualified operator, certified by TSSA, in the operation of a stacked parking device be provided at all times.  
(Z.-1-132179)
- d) Nothing in this By-Law shall prevent the obstruction of a driveway by a gate, a temporary barrier stacked parking or similar obstruction used solely to restrict access to the said driveway and designed to be easily raised, swung aside or otherwise opened or removed when necessary to permit passage of a vehicle but does not include the use of any fee to remove these temporary barriers to any driveway or entrance required to access a required parking space excluding institutional uses and legally established commercial parking structures or lots. (Z.-1-132180)
- e) Nothing in this Subsection shall apply to prevent the use of a right-

of-way as a means of obtaining access to a parking area, provided the said right-of-way has been specifically established for such purpose, or to prevent the establishment of abutting driveways or parking aisles along a common lot line.

f) No person shall use any land or cause or permit the use of any land situated in any zone for the purposes of parking or storage of a vehicle or; for the purposes of uncovered surface parking areas or; for the purpose of access driveways, on any corner lot, within the triangular area bounded by the street lines and a line joining points on the street lines as defined in Section 4.24 (Sight Triangle) of this By-law. (Z.-1-95366)

g) Residential Garage Widths for Small Residential Lots

For single detached dwellings permitted in Residential Zones with a lot frontage of less than 12 metres (39.4 feet), the maximum residential garage width shall not exceed 53% of the lot frontage.(Z.-1-00759)

h) Residential Driveway Widths for Residential Lots

(I) For a single detached, semi-detached, duplex or converted two unit dwelling, driveways and/or walkways connected to and parallel to a driveway cannot exceed:

- i. for lots with a width of up to 12.0 metres: 50% of the required lot frontage to a maximum 6.0 metres, whichever is less;
- ii. for lots with a width greater than 12.0 metres: 50% of the required lot frontage to a maximum of 8 metres, whichever is less

(II) Where more than one driveway leads to a parking area/private garage for a single detached, semi-detached, duplex or converted two-unit dwelling is provided, the combined total of all driveways shall not exceed the provision of 4.19 6) (h) (I).

(III)Notwithstanding Section 4.19 (4) and 4.19 (6) (I) (II) driveways and/or walkways connected to and parallel to a driveway may increase to the width of the private garage and/or the legal parking area of the residential property. In addition, a driveway and/or walkway connected to and parallel to a driveway may project into a front yard of a single detached, semi-detached, duplex or converted two unit dwelling, provided that the projection into the front yard is no greater than 2.0 metres. (Z.-1-142314)

i) Notwithstanding Section 4.19 b), street oriented townhouse dwellings in a cluster form, the maximum driveway width leading to a parking area shall be no greater than 3 metres for a single car garage and no greater than 6m when leading to a two car garage. For freehold street townhouse dwellings, the maximum driveway width leading to a parking area shall be no greater than 50% of the lot frontage or a maximum of 6.0 metres, whichever is less and in no case shall it be less than 3m.” (Z.-1-142314)

j) Access aisles for parking spaces for persons with disabilities

Access aisles may be shared by two parking spaces for the use of persons with disabilities in an off-street parking facility and shall meet the following requirements:

- 1) They shall have a minimum width of 1,500 mm.
- 2) They shall extend the full length of the parking space.

They shall be marked with high tonal contrast diagonal lines, which discourages parking in them, where the surface is asphalt, concrete or some other hard surface. (Z.-1-142353)

7) SURFACE AND DRAINAGE OF PARKING AREAS AND DRIVEWAYS

- a) All parking areas and driveways shall be provided and maintained with a stable surface, treated so as to prevent the raising of dust or loose particles, and including, but not so as to limit the generality of the foregoing, the following surface materials:
  - i) treated crushed stone or gravel; and,
  - ii) any asphalt, concrete or other hard-surfaced material.
- b) All parking areas and driveways shall be drained so as to control the pooling of surface water and prevent the flow of surface water onto adjacent lots.

8) ADDITION TO EXISTING USE

Where a building or structure has insufficient parking spaces on the date of passing of this By-Law to conform to the requirements herein, this By-Law shall not be interpreted to require that the deficiency be made up prior to the construction of any addition or a change of use provided, however, that any additional parking spaces required by this By-Law for such addition or change of use are provided in accordance with all provisions hereof respecting parking spaces and parking areas.

9) PARKING STANDARD AREAS

The City of London has been divided into Parking Standard Areas. The limit and location of Parking Standard Areas 1 and 2 are as shown on Schedule "B" to this By-Law. All remaining lands are within Parking Standard Area 3.

10) PARKING STANDARDS

Except as otherwise provided herein, the owner or occupant of any lot, building or structure used or erected for any of the purposes set forth in this Subsection, shall provide and maintain, for the sole use of the owner, occupant or other persons entering upon or making use of the said lot, building or structure from time to time, parking spaces in accordance with the provisions of this Subsection. Unless otherwise specified, the standards are expressed in one space per square metre of Gross Floor Area (GFA).

- a) Parking Standard Area 1 parking requirements are as follows:

- i) Non-Residential Development

- Within Parking Standard Area 1, parking shall be provided for all new non-residential development or redevelopment at one parking space per 45 square metres (484 square feet). Within Parking Standard Area 1, existing square metres of uses shall be subtracted from the total new or redeveloped square metres when calculating the required number of parking spaces. For the purpose of this Section, existing shall mean as of January 1, 1987.

- Notwithstanding this section, CLINIC, METHADONE and PHARMACY, METHADONE shall be calculated at the ratio provided for in Section 4.19. 10) b). (Z.-1-122090)

**Example Application of Section 4.19 10) Parking Standard Area 1**

Existing site - 4000 square metres (43,056 square feet); redeveloped to 6000 square metres (64,583 square feet) - 2000 square metres (21,528 square feet) at one space per 45 square metres (484 square feet) equals 44 spaces required

For all lands zoned Downtown Area (DA) within Parking Standard Area 1, parking shall be provided for all new non-residential development or redevelopment at one parking space per 90 square metres (968 square feet). Within Parking Standard Area 1,

existing square metres of uses shall be subtracted from the total new or redeveloped square metres when calculating the required number of parking spaces. For the purpose of this Section, existing shall mean as of June 3, 1991. (Z.-1-94271)

There is no requirement for additional parking for outdoor patios associated with adjacent restaurants. (Z.-1-97534)

ii) Residential Development

Parking Standard Area 1 provisions for residential development or redevelopment shall be as follows:

- a) Single Detached and Semi-Detached 2 per unit
- b) Duplex, Triplex, Townhouse and Apartment 1 per unit
- c) Converted Dwelling or Conversions of Existing Space to Residential Units No additional parking required
- d) Senior Citizen Apartment Building 0.25 per unit
- e) Handicapped Persons Apartment Building 0.25 per unit
- f) Lodging House 0.33 per unit

For all lands zoned Downtown Area (DA) within Parking Standard Area 1 there shall be no parking required for all existing and new residential development. (Z.-1-95343)

b) Parking Standard Areas 2 and 3 parking requirements are as follows:

	<b>Parking Standard Area 2</b>	<b>Parking Standard Area 3</b>
Abattoir	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Advanced Manufacturing Industrial Uses (Z.-1-172594)	1 per 200 m <sup>2</sup>	1 per 200m <sup>2</sup>
Advanced Manufacturing Educational Uses (Z.-1-172594)	1 per 200m <sup>2</sup>	1 per 200m <sup>2</sup>
Agricultural Service Establishment (Z.-1-051390)	1 per 65 m <sup>2</sup> (700 sq. ft.)	1 per 65 m <sup>2</sup> (700 sq. ft.)
Agricultural Supply Establishment	1 per 30 m <sup>2</sup> (323 sq. ft.) for retail show room plus 1 per 200 m <sup>2</sup> (2,153 sq. ft.) for warehousing/wholesaling	
Amusement Game Establishment	1 per 20 m <sup>2</sup> (215 sq.ft.)	1 per 20 m <sup>2</sup> (215 sq.ft.)
Animal Hospital/ Animal Clinic (Z.-1-051390)	1 per 45m <sup>2</sup> (484 sq. ft.)	1 per 45m <sup>2</sup> (484 sq. ft.)
Apartment Building	1 per unit	1.25 per unit
Apartment Hotel	1.25 per unit	1.25 per unit

	<b>Parking Standard Area 2</b>	<b>Parking Standard Area 3</b>
Arena (No Seats)	1 per 35 m <sup>2</sup> (376 Sq. ft.)	1 per 35 m <sup>2</sup> (376 Sq. ft.)
Arena (with Seats)	1 per 8 seats	1 per 7 seats
Art Gallery	See - Gallery	
Artisan Workshop (Z.-1-172561)	1 per 100m <sup>2</sup> (1,076 sq ft) for processing/ manufacturing, plus 1 per 25m <sup>2</sup> (269 sq ft) for retail area/restaurant	1 per 100m <sup>2</sup> (1,076 sq ft) for processing/ manufacturing, plus 1 per 15m <sup>2</sup> (161 sq ft) for retail area/restaurant
Assembly Hall	1 per 8 seats or 1 per 35 m <sup>2</sup> (376 sq ft) whichever is greater	1 per 7 seats or 1 per 25 m <sup>2</sup> (269 sq ft) whichever is greater
Auction Establishment	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Auditorium	1 per 8 seats	1 per 6 seats (Z.-1-97532)
Automobile Body Shop	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Automobile Rental Establishment	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Automobile Repair Garage (Z.-1-93150)	1 per 10 m <sup>2</sup> (108 sq ft)	1 per 10 m <sup>2</sup> (108 sq ft)
Automobile Sales, Ancillary to Automobile Repair Garage (Z.-1-93150)	1 per automobile store, kept or displayed for sale	
Automobile Sales & Service Establishment	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Automobile Service Station	6 per bay	6 per bay
Automobile Supply Store	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 25 m <sup>2</sup> (269 sq ft)
Bakery	1 per 100 m <sup>2</sup> (1,076 sq ft)	1 per 100 m <sup>2</sup> (1,076 sq ft)
Bake Shop	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 25 m <sup>2</sup> (269 sq ft)
Bed and Breakfast Establishment (Z-1-051390)	1 per bedroom plus 2 spaces	
Banquet Hall	See - Assembly Hall	
Boarding House	See – Lodging House	
Bowling Alley	See - Commercial Recreation Establishment	
Brewing on Premises Establishment (Z.-1- 95390)	1 per 40 m <sup>2</sup> (430 sq ft) processing Area	1 per 40 m <sup>2</sup> (430 sq ft) processing Area
Building or Contracting Establishment	See- Manufacturing Establishment	

	<b>Parking Standard Area 2</b>	<b>Parking Standard Area 3</b>
Building Supply Outlet	1 per 30 m <sup>2</sup> (323 sq ft) for retail/showroom plus 1 per 200 m <sup>2</sup> (2,153 sq ft) warehouse/wholesaling	
Bulk Beverage Outlet	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Bulk Sales Establishment (Z.-1-051390)	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Business Service Establishment	1 per 50 m <sup>2</sup> (538 sq ft)	1 per 50 m <sup>2</sup> (538 sq ft)
Campground (Z.-1-051390)	N/A	2 spaces per camp site
Car Wash	3 spaces	3 spaces
Catalogue Store	See – retail store	
Caterer's Establishment	1 per 200 m <sup>2</sup> (2,153 sq ft)	1 per 100 m <sup>2</sup> (1,076 sq ft)
Chronic Care Facility	See – Nursing Home	
Clinic and Out Patient Clinic	1 per 15 m <sup>2</sup> (161 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Clinic, Methadone (Z.-1-122090)	1 per 15 m <sup>2</sup> (161 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Cluster Housing	See - Specific Dwelling Type	
Commercial Outdoor Recreation Facility	1 per 1,000 m <sup>2</sup> (10,764 sq ft)	1 per 1,000 m <sup>2</sup> (10,764 sq ft)
Commercial Recreation Establishment	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Community Centre and Hall	1 per 8 seats or per 1 per 35 m <sup>2</sup> (376 sq ft) whichever is greater	1 per 7 seats or per 1 per 25 m <sup>2</sup> (269 sq ft) whichever is greater
Continuum-of-Care Facility	See – appropriate components as proportioned	
Convenience Service Establishment	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Convenience Store	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Converted Dwelling	1 per unit	1 per unit
Craft Brewery (Z.-1-172561)	1 per 100m <sup>2</sup> (1,076 sq ft) for processing, plus 1 per 25m <sup>2</sup> (269 sq ft) for retail area/restaurant	1 per 100m <sup>2</sup> (1,076 sq ft) for processing, plus 1 per 15m <sup>2</sup> (161 sq ft) for retail area/restaurant
Custom Workshop (Z.-1-051390)	5 spaces	5 spaces
Data Processing Establishment	1 per 40 m <sup>2</sup> (430 sq ft)	1 per 40 m <sup>2</sup> (430 sq ft)
Day Care Centre	1 per 40 m <sup>2</sup> (430 sq ft)	1 per 40 m <sup>2</sup> (430 sq ft)



	<b>Parking Standard Area 2</b>	<b>Parking Standard Area 3</b>
Department Store	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Driving Range (Golf)	1.5 per tee	1.5 per tee
Drug Store	See - pharmacy	
Dry Cleaning and Laundry Depot	1 per 15 m <sup>2</sup> (161 sq ft)	1 per 10 m <sup>2</sup> (108 sq ft)
Dry Cleaning and Laundry Plant	1 per 200 m <sup>2</sup> (2,153 sq ft)	1 per 100 m <sup>2</sup> (1,076 sq ft)
Duplex Dwelling (O.M.B. File #R 910387 - Appeal #9008 June 4, 1993)	1 per unit	1 per unit
Duplicating Shop	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Emergency Care Establishment	1 per 40 m <sup>2</sup> (430 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Farm Equipment Sales and Service Establishment (Z.-1-051390)	N/A	1 per 100 m <sup>2</sup> (1,076 sq ft)
Farm food and products market (Z.-1- 98573)	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Farm Market (Z.-1-051390)	N/A	5 spaces
Film Processing Depot	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Financial Institution	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Fire Station	1 per 1.5 employees	1 per 1.5 employees
Florist Shop	See – retail store	
Food Store	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Food, Tobacco and Beverage Processing	See – manufacturing establishment	
Fourplex (O.M.B. File #R 910387 - Appeal #9008 June 4, 1993)	1 per unit	1 per unit
Fraternity House	See – Lodging House	
Funeral Home	1 per 1 per 20 m <sup>2</sup> (215 sq ft) or 20 spaces, whichever is greater	
Gallery	1 per 50 m <sup>2</sup> (538 sq ft)	1 per 40 m <sup>2</sup> (430 sq ft)
Garden Store (Z.-1-051390)	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Gas Bar	3 spaces	3 spaces
Greenhouse Farm (Z.-1-152403)	1 per 20,000 m <sup>2</sup>	

	<b>Parking Standard Area 2</b>	<b>Parking Standard Area 3</b>
Golf Course	8 per tee	8 per tee
Golf Course, Recreational (Z.-1-051390)	See appropriate components as proportioned	
Grocery Store	See- Food Store	
Group Home	2 spaces	2 spaces
Handicapped Persons Apartment Building	0.25 per unit	0.5 per unit
Hardware Store	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Home and Auto Supply Store	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 25 m <sup>2</sup> (269 sq ft)
Home Appliance Store	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Home Furnishings Store	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Home for the Aged	See – Rest Home	
Home Improvement Store	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Hospital	1.25 per bed	3 per bed
Hotel	1.25 per unit	1.25 per unit
Household Appliance Sales and Service	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Industrial and Agricultural Equipment Sales and Service	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
Industrial Mall (Z.1-051390)	1 per 65 m <sup>2</sup> (700 sq ft)	1 per 65 m <sup>2</sup> (700 sq ft)
Kennel	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Laboratory	1 per 45 m <sup>2</sup> (484 sq ft)	1 per 45 m <sup>2</sup> (484 sq ft)
Laboratory, Patient Testing	See – patient testing laboratory	
Laundromat	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Library	1 per 50 m <sup>2</sup> (538 sq ft)	1 per 40 m <sup>2</sup> (438 sq ft)
Liquor, Beer and Wine Store	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Lodging House	0.33 per unit	0.33 per unit
Manufacturing and Assembly	See – Manufacturing Establishment	
Manufacturing Establishment (Z.-1-172594)	1 per 200 m <sup>2</sup>	1 per 200 m <sup>2</sup>

	<b>Parking Standard Area 2</b>	<b>Parking Standard Area 3</b>
Miniature Golf Course	1.5 per tee	1.5 per tee
Mobile Home	1 per unit	1 per unit
Motel	1.25 per unit	1.25 per unit
Multiple Dwelling (Z.-1-95353)	1.25 per unit	1.25 per unit
Museum	1 per 50 m <sup>2</sup> (538 sq ft)	1 per 40 m <sup>2</sup> (430 sq ft)
Music School	1 per 45 m <sup>2</sup> (484 sq ft)	1 per 45 m <sup>2</sup> (484 sq ft)
Nursery(Z.-1-051390)	N/A	2 spaces
Nursing Home	1 per 3 beds	1 per 3 beds
Office-Apartment Building	See – Office and Apartment Building as proportioned	
Office - Business, Service and Professional	1 per 40 m <sup>2</sup> (430 sq ft)	1 per 40 m <sup>2</sup> (430 sq ft)
Office - Business, Service and Professional – Converted	1 per 45 m <sup>2</sup> (484 sq ft)	1 per 40 m <sup>2</sup> (430 sq ft)
Office, Medical/Dental	1 per 15 m <sup>2</sup> (161 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Office, Medical/Dental – Converted	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Office, Support	1 per 45 m <sup>2</sup> (484 sq ft)	1 per 45 m <sup>2</sup> (484 sq ft)
Open Storage	1 per ha (2.5 ac)	1 per ha (2.5 ac)
Paper and Allied Products Industry	See – Manufacturing Establishment	
Patient Testing Laboratory	1 per 15 m <sup>2</sup> (161 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Place of Worship (Z.-1-142287)	1 per each 4 persons place of worship capacity	
Personal Service Establishment	1 per 15 m <sup>2</sup> (161 sq ft)	1 per 10 m <sup>2</sup> (108 sq ft)
Pharmaceutical and Medical Products Industry	See – Manufacturing Establishment	
Pharmacy (Z.-1- 051390)	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 25 m <sup>2</sup> (269 sq ft)
Pharmacy, Methadone (Z.-1-122090)	1 per 15 m <sup>2</sup> (161 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Police Station	1 per 1.5 employees	1 per 1.5 employees
Post Office	1 per 30 m <sup>2</sup> (323 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)

	<b>Parking Standard Area 2</b>	<b>Parking Standard Area 3</b>
Primary Metals Industry	See – manufacturing establishment	
Printing Establishment	1 per 200 m <sup>2</sup> (2,153 sq ft)	1 per 100 m <sup>2</sup> (1,076 sq ft)
Printing, Reproduction and Data Processing Industries	See – manufacturing establishment	
Private Club	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Private Outdoor Recreation Club	1 per 700m <sup>2</sup> (7,535 sq ft) of lot area for the first 10 ha (25ac), plus 1 per 5000 m <sup>2</sup> (53,820 sq ft) of lot area over 10 ha (25 ac)	
Private Zoo (Z.-1-051390)	N/A	1 per 700 m <sup>2</sup> (7535 sq ft) of lot area used as a private zoo
Processed Goods Industry	See – manufacturing establishment	
Public Hall	See – Assembly hall	
Public Recreation Facility	1 per 35 m <sup>2</sup> (376 sq ft)	1 per 35 m <sup>2</sup> (376 sq ft)
Public Use	1 per 45 m <sup>2</sup> (484 sq ft)	1 per 45 m <sup>2</sup> (484 sq ft)
Public Utility	See – Public Use	
Racquet Facility	2 per court	2 per court
Raw Materials Processing	See – manufacturing establishment	
Recreational Vehicle Sales and Service Establishment (Z.-1-051390)	1 per 20 m <sup>2</sup> (215 sq ft) plus 1 per 10 vehicles on display	
Repair and Rental Establishment	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Research and Development Establishment	See- manufacturing establishment	
Resource Extraction Operation	2 spaces	2 spaces
Restaurant (Z.-1-97532)	1 per 15 m <sup>2</sup> (161 sq ft)	1 per 10 m <sup>2</sup> (108 sq ft)
Restaurant, Eat-In and Outdoor Patio	See - Resaurant	
Restaurant, Fast-Food, Drive-In, Take-Out	1 per 8 m <sup>2</sup> (86 sq ft)	1 per 8 m <sup>2</sup> (86 sq ft)
Rest Home	1 per 3 beds	1 per 3 beds
Restricted Manufacturing and Assembly	See – Manufacturing Establishment	

	<b>Parking Standard Area 2</b>	<b>Parking Standard Area 3</b>
Retail Store (Greater Than 2 000 m <sup>2</sup> )	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Retail Store (Less Than 2 000 m <sup>2</sup> )	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Retail Warehousing	1 per 1 per 30 m <sup>2</sup> (323 sq ft) retail/showroom plus 1 per 200 m <sup>2</sup> (2,153 sq ft) for warehousing/ wholesaling	
Retirement Lodge	1 per 3 beds	1 per 3 beds
Rooming House	See – lodging house	
Salvage Yard (With No Structures)	1 hectare (2.47 acres)	1 hectare (2.47 acres)
Salvage Yard (With Structures)	1 per 30 m <sup>2</sup> (323 sq ft) for retail and showroom plus 1 per 200 m <sup>2</sup> (2,153 sq ft) for warehousing and wholesaling	
School, Adult Secondary	See – School, Community College	
School, Commercial (Z.-1-97532)	1 per 45 m <sup>2</sup> (484 sq ft)	1 per 30 m <sup>2</sup> (323 sq ft)
School, Community College	1 per 100 m <sup>2</sup> (1,076 sq ft) plus 1 per 15 students	1 per 100 m <sup>2</sup> (1,076 sq ft) plus 1 per 15 students
School, Elementary	3 plus 1 per classroom	3 plus 1 per classroom
School, Private	3 per classroom	3 per classroom
School, Secondary	3 per classroom	3 per classroom
School, University	1 per 100 m <sup>2</sup> (1,076 sq ft) plus 1 per 15 students	1 per 100 m <sup>2</sup> (1,076 sq ft) plus 1 per 15 students
Self-storage Establishments	1 per 1000 m <sup>2</sup>	1 per 1000 m <sup>2</sup>
Semi-Detached Dwelling	2 per unit	2 per unit
Senior Citizens Apartment Building	0.25 per unit	0.5 per unit
Service and Repair Establishment	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)
Service Industrial Use	1 per 65 m <sup>2</sup> (700 sq ft)	1 per 65 m <sup>2</sup> (700 sq ft)
Service Trade	1 per 20 m <sup>2</sup> (215 sq ft)	1 per 20 m <sup>2</sup> (215 sq ft)

	<b>Parking Standard Area 2</b>	<b>Parking Standard Area 3</b>
Shopping Centre (Greater than 2 000 m <sup>2</sup> and with 4 or more individual business establishments excluding theatres and cinemas which are calculated separately) Where thirty percent (30%) of the gross leasable floor area is occupied by restaurants, the required parking is calculated separately for the restaurant use(s). (Z.-1-97532)	1 per 30 m <sup>2</sup> (323 sq ft) gross leasable floor area	1 per 20 m <sup>2</sup> (215 sq ft) gross leasable floor area
Shopping Centre (Less Than 2 000 m <sup>2</sup> )	1 per 15 m <sup>2</sup> (161 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Single Detached Dwelling	2 per unit	2 per unit
Sorority House	See – Lodging House	
Stadium	See - Auditorium	
Studio	1 per 45 m <sup>2</sup> (484 sq ft)	1 per 45 m <sup>2</sup> (484 sq ft)
Supermarket	See – Food Store	
Supervised Residence	1 per 4 residents	1 per 4 residents
Tavern	1 per 6 m <sup>2</sup> (65 sq ft)	1 per 6 m <sup>2</sup> (65 sq ft)
Taxi Establishment	1 per 6 m <sup>2</sup> (65 sq ft)	1 per 6 m <sup>2</sup> (65 sq ft)
Tennis Club	2 per court	2 per court
Tennis Court (Outdoors)	2 per court	2 per court
Terminal Centre	1 per 100 m <sup>2</sup> (1,076 sq ft)	1 per 100 m <sup>2</sup> (1,076 sq ft)
Textile Processing	See – Manufacturing Establishment	
Theatre and Cinema	See - Auditorium	
Townhouse, Cluster	1.25 per unit	1.5 per unit
Townhouse, Stacked	1.25 per unit	1.5 per unit
Townhouse, Street	2 per unit	2 per unit
Travel Plaza/Truck Stop (Z.-1-051390)	See appropriate components as proportioned	
Triplex Dwelling (O.M.B. File #R 910387 - Appeal #9008 June 4, 1993)	1 per unit	1 per unit
Vehicle Sales and Service Establishment	1 per 100 m <sup>2</sup> (1,076 sq ft)	1 per 100 m <sup>2</sup> (1,076 sq ft)

	<b>Parking Standard Area 2</b>	<b>Parking Standard Area 3</b>
Video Rental Establishment	1 per 25 m <sup>2</sup> (269 sq ft)	1 per 15 m <sup>2</sup> (161 sq ft)
Warehouse Establishment (Z.-1-94236)	1 per 200 m <sup>2</sup> (2,153 sq ft)	1 per 200 m <sup>2</sup> (2,153 sq ft)
Wholesale Establishment	1 per 150 m <sup>2</sup> (1,614 sq ft)	1 per 125 m <sup>2</sup> (1,345 sq ft)

c) Accessible parking spaces

Where parking spaces are required, in any development, accessible parking spaces shall be provided. Off street parking areas shall have a minimum number of accessible parking spaces as follows:

- i) One parking space for the use of persons with disabilities, which meets the requirements of a Type A parking space, where there are 12 parking spaces or fewer; or
- ii) Four per cent of the total number of parking spaces for the use of persons with disabilities, where there are between 13 and 100 parking spaces in accordance with the following ratio, rounding up to the nearest whole number:
  1. Where an even number of parking spaces for the use of persons with disabilities are provided in accordance with the requirements of this paragraph, an equal number of parking spaces that meet the requirements of a Type A parking space and a Type B parking space must be provided; or
  2. Where an odd number of parking spaces for the use of persons with disabilities are provided in accordance with the requirements of this paragraph, the number of parking spaces must be divided equally between parking spaces that meet the requirements of a Type A parking space and a Type B parking space, but the additional parking space, the odd-numbered space, may be a Type B parking space; or
- iii) One parking space for the use of persons with disabilities and an additional three per cent of parking spaces for the use of persons with disabilities, where there are between 101 and 200 parking spaces must be parking spaces for the use of persons with disabilities, calculated in accordance with the ratios set out in subparagraphs ii) 1 and 2), rounding up to the nearest whole number; or
- iv) Two parking spaces for the use of persons with disabilities and an additional two per cent of parking spaces for the use of persons with disabilities, where there are between 201 and 1,000 parking spaces must be parking spaces for the use of persons with disabilities in accordance with the ratio in subparagraphs ii) 1 and 2 rounding up to the nearest whole number; or
- v) Eleven parking spaces for the use of persons with disabilities and an additional one per cent of parking spaces for the use of persons with disabilities, where more than 1,000 parking spaces are provided must be parking spaces for the use of persons with disabilities in accordance with the ratio in subparagraphs ii) 1 and 2 rounding up to the nearest whole number; and
- vi) The number of parking spaces for persons with disabilities shall be included as part of the total parking required for the site. (Z.-1-142353)

11) OTHER PARKING REGULATIONS

- a) Nothing in this By-Law shall prevent the erection of a shelter for use solely by parking attendants or security personnel in any part of a parking area, except within a sight triangle, provided such shelter is not more than 4.5 metres (14.8 feet) in height and has a floor area of not more than 5.0 square metres (53.8 square feet).
- b) No recreational vehicle, trailer or boat shall be parked or stored on any part of a lot in a Residential Zone, except for one recreational vehicle which:
  - i) is owned by the occupant of the said lot; and,
  - ii) where it has dimensions not greater than 2.4 metres (7.9 feet) of width, 2.4 metres (7.9 feet) of height above ground, nor 6.0 metres (20.0 feet) of length, may be parked or stored on a permitted driveway, or parking area, or within a yard other than a front yard or exterior side yard; or
  - iii) where it has dimensions greater than 2.4 metres (7.9 feet) of width, 2.4 metres (7.9 feet) of height above ground, or 6.0 metres (20.0 feet) of length, may be parked or stored within a rear yard.

Any recreational vehicle owned by the occupant and parked or stored within an enclosed building shall not be subject to these provisions. (Z.-1-97542)

- c) No commercial vehicle or motorized construction equipment shall be parked or stored on any part of a lot in a Residential Zone, except for one commercial vehicle which:
  - i) is owned or operated by the occupant of the said lot; and
  - ii) has dimensions not greater than 2.4 metres (7.9 feet) of width, 2.4 metres (7.9 feet) of height above ground, nor 6.0 metres (20.0 feet) of length; and,
  - iii) is parked or stored within a building or carport or on a permitted driveway or parking area or within a yard other than a front yard, or exterior side yard; or  
(Z.-1-97542)
  - iv) is parked for the purpose of making deliveries or otherwise providing services, on a temporary basis, to the said lot.  
(Z.-1-97542).
- d) Not more than one vehicle or recreational vehicle lacking current valid license plates shall be parked or stored on a lot, excluding permitted vehicles parked or stored within a private garage or other buildings in accordance with the applicable parking/storage location, in a Residential Zone, except that this provision shall not apply to a permitted vehicle sales or permitted rental establishment or permitted salvage yard.  
(Z.1.97542)
- e) No person shall park or store, or permit to be parked or stored, a vehicle on any part of a lot in a residential zone, for the purpose of providing a transfer location, where a person or persons travel to the said lot from another location, to remove or return a vehicle, from or to the said lot.  
(Z.-1-97542)
- f) Service areas are included when calculating parking rates for Automobile Repair Garage. A service area is defined as all areas utilized to service and repair vehicles, and may include lifts, joists, service pits and tool and part storage. (Z.-1-091844)



12) BUS PARKING AREA SUBSTITUTION

A bus parking area or areas may be substituted for vehicle parking spaces at the maximum rate of 1 bus parking area for every twenty-five (25) vehicle parking spaces for any lands zoned Regional Shopping Area (RSA) or Community Shopping Area (CSA), where required by City Council for use by public transit and shown on a registered site plan. (Z.-1-97465)

13) DIMENSIONS OF BUS PARKING AREA

A bus parking area permitted hereby shall be a minimum 3.75 metres (12.3 feet) wide by a minimum 15 metres (49.2 feet) long excluding entrance and/or exit tapers. (Z.-1-97465)

14) BICYCLE PARKING REQUIREMENTS

All required bicycle parking spaces shall be provided at the time of the erection of a building or addition thereto, expansion of a use, or when there is a change of use of a lot or a building. Bicycle parking spaces shall be maintained exclusively for the use for which they are required for as long as the use is in operation.

Where part of a bicycle parking space is required in accordance with this By-law, such part shall be considered one parking space for the purpose of calculating the total bicycle parking requirement for the use. (Z.-1-051420)

15) DESIGN CHARACTERISTICS FOR BICYCLE PARKING

For the purpose of this By-law, associated design elements shall be provided in accordance with those provisions set forth under Section ( ) of the City's Site Plan Control By-law. (Z.-1-051420)

16) NUMBER OF BICYCLE PARKING SPACES

1) Residential Development:

Apartment buildings and lodging houses (with five or more residential units) shall be required to provide 0.75 long-term bicycle parking space per residential unit.

2) Residential Care Facilities:

Short-term bicycle parking spaces shall be provided at a rate of 7% of the required number of automobile parking spaces, as specified in the Zoning By-law, for the following residential care facilities:

- a) senior citizen apartment buildings
- b) nursing homes;
- c) rest homes;
- d) retirement lodges;
- e) retirement homes;
- f) handicapped persons apartment buildings;
- g) continuum-of-care facility;
- h) chronic care facility;
- i) foster homes;
- j) group home type 1 and type 2;
- k) supervised residence;
- l) correctional and detention centre;
- m) emergency care establishment.

3) Residential Development Exemptions:

Notwithstanding clause "( )" to the contrary, bicycle parking shall not be required for:

- a) Conversions of existing space to residential units;
- b) Single detached dwellings; semi-detached dwellings; duplex dwellings; triplex dwellings; fourplex dwellings; townhouse dwellings; stacked townhouse dwellings; street townhouses; cluster townhouses; farm dwellings.  
(Z.-1-051420)

4) Non-Residential Development:

Short-term bicycle parking spaces shall be provided at a rate of 7% of the required number of automobile parking spaces, as specified in the Zoning By-law, for all non-residential development except as specified below:

5) Non-Residential Development Exemptions:

- a) where the required number of automobile parking spaces specified in the Zoning By-law is 9 or fewer spaces, no bicycle parking is required;
- b) No bicycle parking requirement applies for the following uses specified in the Zoning By-law:

Abattoir; aggregate reprocessing; aggregate storage area; agricultural service establishment; agricultural supply establishment; agricultural use; agricultural use, intensive; agricultural use, non-intensive; agriculturally related commercial use; agriculturally related industrial use; batching plant, asphalt; batching plant, concrete; channel composting facility; construction and demolition recycling facility; crushing plant; driving range; drive-through facility; farm; farm cluster; farm equipment sales and service; farm foods and products market; farm market; feedlot; forestry use; grain elevator; greenhouse, commercial; in-vessel composting facility; kennel; landing strip; livestock; livestock facilities; managed woodlot; manure storage facilities; pit; propane transfer facility; quarry; resource excavation; residential and other source recycling facility; resource extraction operation; salvage yard; specialized recycling facility; stockpiling; travel plaza/truck stop; truck stop; theatre, drive-in; wayside pit or wayside quarry; windrow composting facility

- c) No bicycle parking requirement will apply to the conversion of existing buildings for residential or non-residential uses in all Downtown Area 1 and 2 Zones. Major redevelopment involving property consolidation and new construction is required to provide for bicycle parking facilities at the mandated standard.
- d) No bicycle parking requirement will apply to the conversion of existing buildings for residential or non-residential uses in all Business District Commercial 1 and 2 Zones. Major redevelopment involving property consolidation and new construction is required to provide for bicycle parking facilities at the mandated standard.
- e) For CLINIC, METHADONE or PHARMACY, METHADONE uses, notwithstanding any provisions of this by-law, the number of bicycle parking spaces provided shall be no less than 5 spaces.

6) Municipally-owned Parking lots and structures:

Municipally-owned parking lots in the Downtown Area zones and defined Business District Commercial Area zones shall provide for short-term bicycle parking facilities equal to 7% of the total vehicular parking spaces provide.

7) Bicycle Parking Incentives:

Notwithstanding Section 4.19 of this By-law to the contrary, the required number of motor vehicle parking spaces for non-residential uses may be reduced to provide for additional short or long-term bicycle parking spaces beyond those mandated by this by-law provided, however, the reduction in motor vehicle parking spaces shall not exceed 10% of the required motor vehicle parking spaces. Individual vehicular parking stalls shall be required to provide for a minimum of five bicycle parking spaces. This incentive shall not apply to CLNIC, METHADONE or PHARMACY, METHADONE uses. (Z.-1-051420) (Z.-1-122090)

#### 4.20 PUBLIC USES

1) PARKS, FACILITIES AND STREETS (Z-1-051390)

Nothing in this By-law shall apply to prevent or otherwise restrict the use of any land for a public park, a public street, a bicycle pathway, or a public memorial such as a monument, cenotaph or fountain.  
(O.M.B. File #R910387 - Appeal #9006-2 June 4, 1993) (Z-1-051390)

2) UTILITIES

This By-Law permits the use of land or the use or erection of any building or structure in any zone for the purpose of the provision of essential services or utilities or drainage work by the City or The Hydro-Electric Commission of the City of London or of any telephone, telegraph, cable television or natural gas company, including any transportation or public utility owned, operated or franchised by or for the City or The Hydro-Electric Commission of the City of London, any department of the Federal or Provincial governments, including Ontario Hydro, or any Conservation Authority established by the Government of Ontario, provided that:

(Z.-1-94236) (Z.-1-051390)

- a) such building or structure is designed and maintained in general harmony with the buildings and structures permitted within the zone in which it is located;
- b) all buildings shall otherwise comply with the regulations of that zone; and
- c) there shall be no exterior storage of goods, materials or equipment in any Residential Zone.
- d) a minimum setback of 20m (65 ft) will be provided from the centre of a high pressure pipeline to the nearest wall of a building intended for human occupancy. (Z-1-051390)

3) MUNICIPAL DRAINS AND NATURAL WATERCOURSES

The following Municipal Drains and Natural Watercourses regulations will apply only to lands within the Agricultural and Urban Reserve zone variations and, within these zone variations, will apply to all lands including existing lots of record.

No permanent buildings or structures shall be erected or used:

- a) closer than 7.5m (25 ft.) from the top-of-bank of a municipal drain having the width of less than 4.5m (15 ft.) from top-of-bank to top-of-bank;
- b) closer than 18.5m (61 ft.) from the top-of-bank of a municipal drain having a width of between 4.5m (15 ft.) and 7.5m (25 ft.) from top-of-bank to top-of-bank;
- c) closer than 30.5m (100 ft.) from the top-of-bank of a municipal drain which is 7.5m (25 ft.) or more from top-of-bank to top-of-bank;

d) closer than 7.5m (25ft.) from the centre line of a municipal tile drain.

The regulations of the *Drainage Act* will continue to apply to all municipal drains and natural watercourses in all zone variations in the City.  
(Z.-1-051390)(deleted and replaced by Z.-1-112031)

#### 4.21 ROAD ALLOWANCE REQUIREMENTS - SPECIFIC ROADS

Notwithstanding any other provisions of this By-Law, yard requirements adjacent to the Arterial and Collector roads listed below shall be measured from the limit of the required or the existing road allowance, whichever is the greater.

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Street "A" 39T-03505 (Z.-1-051330)	Medway Park Drive	East limit of Plan (39T-02503)	Secondary Collector	10.75m (35.3 ft)
Street "A" (39T-00512) (Z.-1-01869) (Deleted by Z.-1-102986)				
Street "A" (39T-07503) (Z.-1-081703)	Cranbrook Road	Colonel Talbot Road	Secondary Collector	10.75 m (35.3ft)
Street "A" (Z.-1-112006)	Blakie Road	Street "C"	Secondary Collector	13m (42.6 ft)
Street "A" (Z.-1-112006)	Street "C"	Dingman Drive	Primary Collector	10.75 m (35.3 ft)
Street "A" (Z.-1-112007)	Dingman Drive	Street "C"	Primary Collector	13 m (42.6 ft)
Street "A" (Z.-1-112007)	Street "C"	Blakie Road	Secondary Collector	10.75 m (35.3 ft)
Street "A" (39T-09502) (Z.-1-112020)	Commissioners Road East	Street "E" (39T-09502)	Primary Collector	14 m (45.9 ft) at Commissioners Road East tapering to 13m (42.6 ft) at Street "E"
Street "A" (39T-09502) (Z.-1-112020)	Street "E"	Street "B" (39T-09502)	Primary Collector	10.75 m (35.3 ft)
Street "A" (Z.-1-122099)	Sunningdale Road	Callingham Drive	Secondary Collector	10.75 m (35 ft)
Street "A" (west of Crumlin Road and east of Veteran's Memorial Parkway) (39T-09502)	North of Sovereign Road	South of Dundas	Proposed Secondary Collector	10.75m (35.3 ft)
Aberdeen Drive	North of Gore Road	Tartan Drive	Secondary Collector	10.75 m (35.3ft)
Adelaide Street South	Commissioners Road East	Thames River South Branch	Arterial	18 m (59.1 ft)
Adelaide Street North (Z.-1-132184)	Thames River South Branch	Hamilton Road	Arterial	18 m (59.1 ft)
	Hamilton Road	Horton Street East	Arterial	19.5 m (63.98 ft)
	Horton Street East	Dundas Street	Arterial	As Existing

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
	West Side Dundas Street	Queens Avenue	Arterial	26.5 m (87 ft) @ Dundas Street tapering to 19.5 m (63.98 ft) @ Queens Avenue
	East Side of Dundas Street	Queens Avenue	Arterial	As Existing @ Dundas St tapering to 19.5m (63.98 ft) @ Queens Avenue
Adelaide Street North (Z.-1-132184)	Queens Avenue	Fanshawe Park Rd E	Arterial	19.5 m (63.98 ft)
	Fanshawe Park Rd E	N. City Limits	Arterial	18m (59.1 ft)
Adelaide Street South (Z.-1-94277)	Southdale Road	Bradley Ave	Arterial	18 m (59.1 ft)
Adelaide Street South (Z.-1-95277)	Bradley Ave	Newbold Street	Primary Collector	As Existing
Admiral Dr (Z.-1-061546)	Trafalgar Street	Veteran's Memorial Parkway	Secondary Collector	10.75 m (35.3ft)
Albert Street	Ridout Street N	Talbot Street	Secondary Collector	10.75 m (35.3 ft)
Aldersbrook gate	Aldersbrook Road	Fanshawe Park Road West	Secondary Collector	As Existing
Aldersbrook Road	Sarnia Road	Wonderland Road North	Secondary Collector	10.75 m (35.3 ft)
Ambelside Drive	Western Road	Ambelside Drive	Secondary Collector	10.75 m (35.3 ft)
Andover Drive	Southdale Road East	Commissioners Road West	Secondary Collector	10.75 m (35.3ft)
Angle St (Z.-1-132184)	Clarence St	Richmond St	Primary Collector	As Existing
Ashland Avenue	Brydges Street	Dundas Street	Secondary Collector	10.75 m (35.3 ft)
Atkinson Boulevard	Trafalgar Street	Whitehall Drive	Secondary Collector	10.75 m (35.3 ft)
Street "B" (39T-09502) (Z.-1-112020)	Street "A"	East Limit of Plan	Primary Collector	10.75 m (35.3 ft)
Barclay Road	Cranbrook Road	Farnham Road	Secondary Collector	10.75 m (35.3 ft)
Barker Street	Oxford Street East	Kipps Lane	Secondary Collector	10.75 m (35.3 ft)
Base Line Road East	Wharncliffe Road	Wellington Road	Secondary Collector	10.75 m (35.3 ft)
Base Line Road East	Wellington Road	Westminster Road	Primary Collector	13 m (42.7 ft)
Base Line Road West	Southcrest Drive	Wharncliffe Road	Secondary Collector	10.75 m (35.3 ft)
Bateman Trail (Z.-1-061452) (Z.-1-041270)	White Oak Road (south leg)	White Oak Road (north leg)	Secondary Collector	10.75 m (35.3 ft)
Beachwood Ave	Commissioners Road West	Ridgewood Cres.	Secondary Collector	10.75 m (35.3 ft)
Beaverbrook Ave. (Z.-1-02986)	Riverside Drive	Oxford Street West	Secondary Collector	10.75 m (35.3 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Beaverbrook Avenue	Sarnia Road	East limit of Plan	Secondary Collector	10.75 m (35.3 ft)
Beaverbrook Avenue (Z.-1-061478)	Oak Crossing Road	Westerly limit of plan 39T-06501	Secondary Collector	10.75 m (35.3 ft)
Beckwork Avenue	Sanford Street	Sorrel Road	Secondary Collector	10.75 m (35.3 ft)
Belfield Street	Huron St	Kipps Lane	Secondary Collector	10.75 m (35.3 ft)
Belmont Drive	Notre Dame Drive	Whancliffe Road South	Secondary Collector	10.75 m (35.3 ft)
Bentley Drive	Webster Street	Sandford Street	Secondary Collector	10.75 m (35.3 ft)
Berkshire Drive	Teepie Terrace	Springbank Drive	Secondary Collector	10.75 m (35.3 ft)
Berryhill Drive (Z.-1-041203)	Elderberry Avenue	Chambers Avenue	Secondary Collector	10.75 m (35.3 ft)
Bessemer Road	Exeter Road	Newbold Street	Primary Collector	13 m (42.7 ft)
Blackacres Blvd	Dalmagerry Road	Wonderland Road North	Secondary Collector	10.75 m (35.3 ft)
Blackfriars Street	Whancliffe Road North	Ridout Street North	Secondary Collector	10.75 m (35.3 ft)
Blackmaple Drive (Z.-1-02986)	Edgevalley Road	Townsend Drive	Secondary Collector	10.75 m (35.3 ft)
Blackwater Road (Z.-1-02986)	Grenfell Drive	Sunningdale Road East	Secondary Collector	10.75 m (35.3 ft)
Blackwell Boulevard (Z.-1-051314)	Highbury Avenue	East limit of plan 39T-01509	Secondary Collector	10.75 m (35.3 ft)
Blakie Road (Z.-1-112000)	West Limit of Plan 39T-06508	East Limit of Plan 39T-06508	Secondary Collector	10.75 m (35.3 ft)
Blakie Road (Z.-1-112006)	West Limit of Plan 39T-06504	East Limit of Plan 39T-06504	Secondary Collector	10.75 m (35.3 ft)
Blakie Road (Z.-1-112007)	White Oak Road	Street "A"	Primary Collector	13m (42.6 ft)
Blakie Road (Z.-1-112007)	Street "A"	Meg Drive	Secondary Collector	10.75 m (35.3 ft)
Bluebell Road (Z.-1-1041203)	Skyline Avenue	Sunningdale Road East	Secondary Collector	10.75 m (35.3 ft)
Bluegrass Drive	Beaverbrook Avenue	West limit of plan	Secondary Collector	10.75 m (35.3 ft)
Boler Road	Southdale Road West	Sanatorium Road	Arterial	18 m (59.1 ft)
Bostwick Road (Z.-1-051390)	Southdale Road West	Exeter Road	Arterial	18 m (59.1 ft)
Bow Street	Marconi Blvd	Hudson Drive	Secondary Collector	10.75 m (35.3 ft)
Bradish Road (Z.-1-051390)	Wellington Road South	Regan Bourne	Arterial	18m (59.1 ft)
Bradley Avenue (Z.-1-051390)	Bostwick Road	Eastern City Limits	Arterial	18 m (59.1 ft)
Brady Drive (Z.-1-051390)	Bradish Road	Webber Bourne	Arterial	18 m (59.1 ft)
Bramwell Drive	Riverside Drive	80.0 m N/E of Riverside Drive	Secondary Collector	10.75 m (35.3 ft)
Briarhill Avenue	Huron Street	Kipps Lane	Secondary Collector	10.75 m (35.3 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Bruce Street	Wharncliffe Road South	Ridout Street	Secondary Collector	10.75 m (35.3 ft)
Brunswick Avenue	Aldersbrook Road	Healy Road	Secondary Collector	10.75 m (35.3 ft)
Brydges Street	Egerton Street	Wavell Street	Primary Collector	13 m (42.7 ft)
Byron Baseline Road (Z.-1-051390)	Westdel Bourne	Commissioners Road West	Arterial	18 m (59.1 ft)
Byron Baseline Road	Western City Limits	Westdel Bourne	Arterial	18 m (59.1 ft)
Street "C" (Z.-1-01911)	Street "A"	North Limit of Street "C"	Secondary Collector	10.75 m (35.3 ft)
Street "C" (Z.-1-11007)	White Oak Road	Street "A"	Primary Collector	13 m (42.6 ft)
Street "C" (west of Veteran's Memorial Parkway) (Z.-1-13224)	North of Tartan Drive	South of Trafalgar Street	Proposed Secondary Collector	10.75 m (35.3 ft)
Callingham Drive (Z.-1-122099)	Meadowlands Way	East Limit of Plan 39T-11504	Secondary Collector	10.75 m (35.3 ft)
Campbell Street (Z.-1-051390)	Whancliffe Road South	Sunray Avenue	Secondary Collector	10.75 m (35.3 ft)
Capulet Lane	Oxford Street West	Blythwood Road	Secondary Collector	10.75 m (35.3 ft)
Carfrae Crescent	Thames River	Grand Avenue	Primary Collector	13 m (42.7 ft)
Castle Drive	Royal Crescent	Wavell Street	Secondary Collector	10.75 m (35.3 ft)
Castlegrove Blvd	Sarnia Road	Sleightholme Avenue	Secondary Collector	10.75 m (35.3 ft)
Castleton Road (Z.-1-051390)	Roxburgh Road	Dingman Drive	Secondary Collector	10.75 m (35.3 ft)
Cedarhallow Blvd	Killarney	Fanshawe Park Road East	Secondary Collector	10.75 m (35.3 ft)
Central Avenue	Talbot Street	Richmond Street	Primary Collector	13 m (42.7 ft)
Central Avenue	Richmond Street	Adelaide Street North	Primary Collector	As Existing
Central Avenue	Adelaide Street North	Ontario Street	Secondary Collector	10.75 m (35.3 ft)
Chalkstone Drive	Exeter Road	Jalna Blvd	Secondary Collector	10.75 m (35.3 ft)
Chalkstone Drive (Z.-1-112000)	Exeter Road	Blakie Road	Secondary Collector	10.75 m (35.3 ft)
Chambers Avenue	Nanette Drive	Skyline Avenue	Secondary Collector	10.75 m (35.3 ft)
Cheapside Street	St George Street	Richmond Street	Secondary Collector	As Existing
Cheapside Street	Richmond Street	Clarke Road	Primary Collector	13 m (42.7 ft)
Chelton Road (Z.-1-95310) (Z.-1-99716)	Commissioners Road East	Reardon Blvd	Secondary Collector	As Existing
Chelton Road (deleted by Z.-1-041203)				

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Chelton Road (Z.-1-041203)	Reardon Blvd	Meadowgate Blvd	Secondary Collector	10.75 m (35.3 ft)
Cherryhill Blvd	Oxford Street West	Cherryhill Place	Secondary Collector	As Existing
Cherryhill Circle (west) (Z.-1-00800)	Westfield Drive	Cherryhill Blvd	Secondary Collector	10.75 m (35.3 ft)
Cherryhill Place	Cherryhill Blvd	Platts Lane	Secondary Collector	10.75 m (35.3 ft)
Cherrywood Trail	Bluegrass Drive	Sarnia Road	Secondary Collector	10.75 m (35.3 ft)
Chiddington Avenue	Upper Queen Street	Chiddington Gate	Secondary Collector	10.75 m (35.3 ft)
Chiddington Gate	Chiddington Avenue	Wilkins Street	Secondary Collector	10.75 m (35.3 ft)
Chippewa Drive	Sanford Street	Oakville Avenue	Secondary Collector	10.75 m (35.3 ft)
Clarence Street (Z.-1-132184)	York Street	Queens Avenue	Primary Collector	11.6 m (38.1 ft)
	Queens Avenue	Dufferin Avenue	Primary Collector	As Existing
	Dufferin Ave	Angel St	Primary Collector	As Existing
Clarke Road (Z.-1-02986) (Z.-1-132184)	Hamilton Road	VMP Extension	Arterial	18 m (59.1 ft)
	VMP Extension	Fanshawe Park Rd E	Arterial	19.5 m (63.98ft)
	Fanshawe Prk Rd E	North City Limits	Arterial	18 m (59.1 ft)
Clarke Road (Z.-1-132224)	Veteran's Memorial Parkway	200 m east of Veteran's Memorial Parkway	Proposed Arterial	18 m (59.1 ft)
Clarke Road (Z.-1-132224)	Clarke Road	Veteran's Memorial Parkway	Proposed Arterial	18 m (59.1 ft)
Colborne Street	South Street	Huron Street	Primary Collector	As Existing
Colonel Talbot Road (Z.-1-051390)	Southdale Road West	South City Limits	Arterial	18 m (59.1 ft)
Commissioners Road East (Z.-1-051390)	Wharncliffe Road	East City Limits	Arterial	18 m (59.1 ft)
Commissioners Road West (Z.-1-01933)	West City Limits	200 metres east of Springbank Drive	Arterial	18 m (59.1 ft)
Commissioners Road West (Z.-1-0933)	200 metres east of Springbank Drive	Cranbrook Road	Arterial	10 m (32.8 ft)
Commissioners Road West (Z.-1-0933)	Cranbrook Road	Wharncliffe Road South	Arterial	18 m (59.1 ft)
Coombs Avenue	Trott Drive	Sarnia Road	Secondary Collector	10.75 m (35.3 ft)
Cook Road (Z.-1-051390)	Westminster Drive	South City Limits	Arterial	18 m (59.1 ft)
Corley Drive	Windermere Road	Ambelside Drive	Secondary Collector	10.75 m (35.3 ft)



STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Coronation Drive	Gainsborough Road	North of South Carriage Road	Secondary Collector	10.75 m (35.3 ft)
Cranbrook Road	Commissioners Road West	Tillman Road	Secondary Collector	10.75 m (35.3 ft)
Cranbrook Road (Z.-1-142255)	Tilman Road	Longworth Road	Secondary Collector	10.75m (35.3 ft)
Crumlin Road (Z.-1-09133)	Gore Road	Oxford Street East	Arterial	18 m (59.1 ft)
Crumlin Side Road (Z.-1-01887)	Oxford Street East	Robin's Hill Road	Secondary Collector	10.75 m (35.3 ft)
Crumlin Road	200 m south of the CNR line	Gore Road	Secondary Collector	10.75 m (35.3 ft)
Cuddy Blvd	Page Street	Oxford Street	Secondary Collector	10.75 m (35.3 ft)
Street "D" (West of Veteran's Memorial Parkway)	North of Scanlan Street	South of Gore Road	Proposed Secondary Collector	10.75 m (35.3 ft)
Dalmagarry Road	Fanshawe Park Road	Coronation Drive	Secondary Collector	10.75 m (35.3 ft)
Darnley Blvd. (Z.-1-95310)	Jackson Road	Lighthouse Drive	Secondary Collector	17m (55.8 ft)
Darnley Blvd.	Lighthouse Drive	Meadowgate Blvd	Secondary Collector	10.75 m (35.3 ft)
Dearness Drive	Bradley Avenue	Southdale Road	Secondary Collector	10.75 m (35.3 ft)
Decker Driver (Z.-1-051390)	West City Limits	Wonderland Road South	Arterial	18 m (59.1 ft)
Deer Park Circle	Valetta Street	Juniper Street	Secondary Collector	10.75 m (35.3 ft)
Denview Ave	Sunningdale Road West	Silverfox Cres	Secondary Collector	10.75 m (35.3 ft)
Derwent Road	Fanshawe Park Road	Ambleside Drive	Secondary Collector	As Existing
Deveron Crescent	Pond Mills Road (North Intersection)	Pond Mills Road (south Intersection)	Secondary Collector	10.75 m (35.3 ft)
Deveron Crescent	Pond Mills Road	Commissioners Road East	Secondary Collector	10.75 m (35.3 ft)
Deveron Crescent	Commissioners Road East	Pond Mills Road	Secondary Collector	10.75 m (35.3 ft)
Dingman Drive (Z.-1-051390)	Wonderland Road South	East City Limits	Arterial	18 m (59.1 ft)
Doon Drive	Windermere Road	Windermere Road	Secondary Collector	10.75 m (35.3 ft)
Driver Lane (Z.-1-142254)	North of Dundas Street	Auto Mall Avenue	Secondary Collector	10.75 m (35.3 ft)
Dufferin Ave	Richmond Street	Colborne Street	Primary Collector	As existing
	Ridout street	Richmond Street	Primary Collector	13 m (42.7 ft) or as existing
Dundalk Drive	Southdale Road	Ferndale Avenue	Secondary Collector	10.75 m (35.3 ft)
Dundas Street (Z.-1-132184)	Riverside Dr	Wellington St	Primary Collector	As Existing
	Wellington St	Burwell St	Primary Collector	As Existing

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
	Burwell St	Ontario St	Secondary Collector	10.75 m (35.3 ft)
	Ontario St	Highbury Ave	Arterial	20 m (65.6 ft)
	Highbury Ave	E. City Limits	Arterial	18 m (59.1 ft)
Dyer Drive	Hyde Park Road	Street "B" in Plan 39T-11503	Secondary Collector	10.75 m (35.3 ft)
Street "E" 39T-04505 (Z.-1-051330)	Street "A"	West limit of Plan	Secondary Collector	10.75 m (35.3 ft)
Edgevalley Road (Z.-1-95369) (Z.-1-02986)	Highbury Avenue North	Blackmaple Drive	Secondary Collector	10.75 m (35.3 ft)
Egerton Street	Thames River	Dundas Street	Primary Collector	13 m (42.7 ft)
Elias Street	Ontario Street	Nightingale Avenue	Secondary Collector	10.75 m (35.3 ft)
Elliot Street	Oxford Street East	Victoria Street	Secondary Collector	10.75 m (35.3 ft)
Elderberry Ave (Z.-1-041203)	Skyline Avenue	Berryhill Drive	Secondary Collector	10.75 m (35.3 ft)
Elmwood Avenue	Wharncliffe Road	Ridout Street	Secondary Collector	10.75 m (35.3 ft)
Elviage (Z.-1-051390)	West City Limits	Westdel Bourne	Arterial	18 m (59.1 ft)
Emery Street West	Southcrest Drive	Wharncliffe Rd South	Secondary Collector	10.75 m (35.5 ft)
Emery Street East	Wharncliffe Road South	Wellington Road	Secondary Collector	10.75 m (35.5 ft)
Ernest Avenue	Jalna Blvd. South	Southdale Road	Primary Collector	13 m (42.7 ft)
Epworth Avenue	Richmond Street	Waterloo Street	Secondary Collector	10.75 m (35.5 ft)
Exeter Road (Z.-1-051390)	Bostwick Road	Highway 401	Arterial	18 m (59.1 ft)
Street "F" (deleted by Z.-1-051389)				
Fairview Avenue	Weston Street	Whetter Avenue	Secondary Collector	10.75 m (35.5 ft)
Fanshawe Park Road (Z.-1-051390) (Z.-1-132184)	W.City Limits	Wonderland Road N	Arterial	18 m (59.1 ft)
	Wonderland Rd N	Clarke Rd	Arterial	19.5 m (63.98 ft)
Farnham Road	Southdale Road West	Viscount Road	Secondary Collector	10.75 m (35.5 ft)
Ferndale Avenue	Wharncliffe Road	Nixon Avenue	Secondary Collector	10.75 m (35.5 ft)
First Street	Dundas Street	Oxford Street East	Secondary Collector	10.75 m (35.5 ft)
Fitzwilliam Blvd (Z.-1-95369)	Manchestser Road	Hyde Park Road	Secondary Collector	As Existing
Florence Street	Rectory Street	Dundas Street	Arterial	13 m (42.7 ft)
Foxwood Avenue	Fanshawe Park Road West	Medway Park	Secondary Collector	10.75 m (35.5 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Frontenac Road	Commissioners Road East	Deveron Crescent	Secondary Collector	10.75 m (35.3 ft)
Fullerton Street	Ridout Street	Richmond Street	Primary Collector	As Existing
Fuller Street	McNay Street	Highbury Avenue	Secondary Collector	10.75 m (35.3 ft)
Street "G" (Z.-1-111993)	Sunningdale Road	Quarrier Road	Secondary Collector	10.75 m (35.3 ft)
Gainsborough Road	West City Limits	Wonderland Road North	Arterial	18 m (59.1 ft)
Gardenwood Drive	Berkshire Drive	Ridgewood Crescent	Secondary Collector	10.75 m (35.3 ft)
Gideon Drive (Z.-1-051390)	West City Limits	Commissioners Road West	Arterial	18 m (59.1 ft)
Glanworth Drive (Z.-1-051390)	West City Limits	East City Limits	Arterial	18 m (59.1 ft)
Glengarry Ave	Fanshawe Park Road	Glenora Drive	Secondary Collector	10.75 m (35.3 ft)
Glenora Drive	Adelaide Street North	Fanshawe Park Road	Secondary Collector	10.75 m (35.3 ft)
Glenroy Road	Pond View Road	Deveron Crescent	Secondary Collector	10.75 m (35.3 ft)
Gore Road	Hamilton Road	East City Limits	Arterial	18 m (59.1 ft)
Gough Avenue (Z.-1-041203)	Sunningdale Road East	North Wenige Drive	Secondary Collector	10.75 m (35.3 ft)
Grand Avenue	Ridout Street	Wellington Road	Secondary Collector	10.75 m (35.3 ft)
Green Valley Drive (Z.-1-051390)	Sise Road	Highbury Avenue South	Primary Collector	13 m (42.7 ft)
Grenfell Drive (Z.-1-02986)	Adelaide Street North	Stackhouse Drive	Secondary Collector	10.75 m (35.3 ft)
Grey Street	75 m East of Colborne Street	Adelaide Street North	Secondary Collector	10.75 m (35.3 ft)
Grey Street	Wellington Street	75 m East of Colborne Street	Secondary Collector	10.75 m (35.3 ft)
Griffith Street	Commissioners Road West	Boler Road	Secondary Collector	10.75 m (35.3 ft)
Griffith Street	Boler Road	Baseline Road	Secondary Collector	10.75 m (35.3 ft)
Guildwood Blvd (Z.-1-02896)	Oxford Street West	Prince Phillip Drive	Secondary Collector	10.75 m (35.3 ft)
Guildwood Gate	Oxford Street West	Guildwood Blvd	Secondary Collector	10.75 m (35.3 ft)
Hale Street	Hamilton Road	Dundas Street	Secondary Collector	10.75 m (35.3 ft)
Hall's Mill Road	Commissioners Road West	Boler Road	Primary Collector	10.75 m (35.3 ft)
Hamilton Road (Z.-1-132184)	Horton Street	Adelaide St N	Arterial	18 m (59.1 ft)
Hamilton Road (Z.-1-132184)	Adelaide St N	Highbury Ave	Arterial	19.5 m (63.98 ft)
	Highbury Ave	E. City Limits	Arterial	18 m (59.1 ft)
Hamlyn Street (Z.-1-051390)	Wharncliffe Road South	Wonderland Road South	Secondary Collector	10.75 m (35.3 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Harry White Drive (Z.-1-051390)	Wonderland Road South	Wellington Road South	Arterial	18 m (59.1 ft)
Hartson Road	Aldergrove Crescent	Bramwell Drive	Secondary Collector	10.75 m (35.3 ft)
Hartson Road	Bramwell Drive	Riverside Drive	Secondary Collector	10.75 m (35.3 ft)
Hastings Drive	Fanshawe Park Road East	Fanshawe Park Road East	Secondary Collector	10.75 m (35.3 ft)
Hastings Drive (Deleted by Z.-1-041203)				
Hastings Drive	Fanshawe Park Road East	Chambers Avenue	Secondary Collector	10.75 m (35.3 ft)
Hastings Gate	Hastings Drive	Stoneybrook Crescent	Secondary Collector	10.75 m (35.3 ft)
Hawthorne Road	Coronation Drive	Blackacres Blvd	Secondary Collector	10.75 m (35.3 ft)
Hazeldon Lane	Hartson Road	Hyde Park Road	Secondary Collector	10.75 m (35.3 ft)
Headley Drive	Santa Monica Road	Oxford Street West	Secondary Collector	10.75 m (35.3 ft)
Hickory Road	Oak Park Drive	Pinetree Drive	Secondary Collector	10.75 m (35.3 ft)
High Street	Commissioners Road East	Wellington Road	Secondary Collector	10.75 m (35.3 ft)
Highbury Avenue North (Z.-1-051390) (Z.-1-132184)	Power Street	Dundas St	Arterial	18 m (59.1 ft)
	Dundas St	Fanshawe Park Rd E	Arterial	20 m (65.6 ft)
	Fanshawe Prk Rd E	N. City Limit	Arterial	18 m (59.1 ft)
Highbury Avenue South (Z.-1-051390)	Highway 401	South City Limits	Arterial	18 m (59.1 ft)
Highview Avenue East	Ferndale Avenue	Wharncliffe Road South	Secondary Collector	10.75 m (35.3 ft)
Homestead Crescent	Limberlost Crescent	Blackacres Blvd	Secondary Collector	10.75 m (35.3 ft)
Homeview Road	Southdale Road East	Ferndale Avenue	Secondary Collector	10.75 m (35.3 ft)
Homewood Line (Z.-1-051390)	Longwoods Road	Pack Road	Arterial	18 m (59.1 ft)
Horton Street	Springbank Drive	Wellington Street	Arterial	18 m (59.1 ft)
Horton Street	Wellington Street	Burwell Street`	Arterial	As Existing
Horton Street	Burwell Street	Adelaide Street North	Arterial	18 m (59.1 ft)
Horton Street	Adelaide Street South	York/Florence	Arterial	18 m (59.1 ft)
Hubrey Road (Z.-1-051390)	Wilton Grove Road	Green Valley Road	Primary Collector	13 m (42.7 ft)
Hudson Drive	Bow Street	Trafalgar Street	Secondary Collector	10.75 m (35.3 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Hume Street	Trafalgar Street	Pine Street	Secondary Collector	10.75 m (35.3 ft)
Hunt Club Drive	Royal York Road	Manchester Road	Secondary Collector	10.75 m (35.3 ft)
Huntington Drive	Chiddington Ave	Commissioners Road East	Secondary Collector	10.75 m (35.3 ft)
Huron Street	Richmond Street	Maitland Street	Primary Collector	13 m (42.7 ft)
Huron Street	William Street	Adelaide Street North	Primary Collector	13 m (42.7 ft)
Huron Street	Adelaide Street North	Wager Road	Arterial	18 m (59.1 ft)
Huron Street	CN Rail Right of Way	Robin's Hill Road	Arterial	18 m (59.1 ft)
Hyde Park Road	Riverside Drive	Oxford Street West	Primary Collector	13 m (42.7 ft)
Hyde Park Road	Oxford Street West	North City Limits	Arterial	18 m (59.1 ft)
Street "A"	Hyde Park Road	Street B (Plan 39T-11503)	Secondary Collector	10.75 m (35.3 ft)
Street "I" (Z.-1-02986) Deleted by (Z.-1-041203)				
Industrial Road	Dundas Street	700.0 m North of Oxford Street East	Secondary Collector	10.75 m (35.3 ft)
Ironwood Road Deleted by Z.-1-041203				
Ironwood Road (Z.-1-041203)	Wickerson Road	Southerly limit of existing Road	Secondary Collector	10.75 m (35.3 ft)
Jackson Road (Z.-1-95310) (Z.-1-02986)	Commissioners Road East	Bradley Avenue	Primary Collector	As Existing
Jalna Blvd. West	Ernest Avenue	Southdale Road	Secondary Collector	10.75 m (35.3 ft)
Jalna Blvd East	Jalna Blvd West	Meg Drive	Secondary Collector	10.75 m (35.3 ft)
Jalna Blvd	Ernest Avenue	Meg Drive	Primary Collector	13 m (42.7 ft)
James Street (Z.-1-051390)	Colonel Talbot Road	Main Street	Secondary Collector	10.75 m (35.3 ft)
Jennifer Road	Fanshawe Park Road	Hastings Drive	Secondary Collector	As Existing
Jensen Road (Z.-1-95369)	Bentley Drive	30 m West of Stonehenge Road	Secondary Collector	As Existing
Jensen Road	Highbury Ave N	500.0 m E of Webster St	Secondary Collector	10.75 m (35.3 ft)
	500.0 m E of Webster St	Bentley Dr	Secondary Collector	10.75 m (35.3 ft)
Juniper St	Dear Park Circle	Oxford St W	Secondary Collector	10.75 m (35.3 ft)
Street "K" 39T-02505 (Z.-1-041251)	Sunningdale Road West	Street "L" 39T-02505	Secondary Collector	10.75 m (35.3 ft)
Kains Road (Z.-1-00834) (deleted by Z.-1-051389)				

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Kains Road (Z.-1-051389)	Oxford Street West	Westdel Bourne	Primary Collector	13 m (42.7 ft)
Kains Road (Z.-1-061542)	Westdel Bourne	South-west limit of 39T-05514	Secondary Collector	10.75 m (35.3 ft)
Kains Road (39T-17501) (Z.-1-172596)	North limit of plan	Oxford Street W	Secondary Collector	10.75 m (35.3 ft)
Kernohan Pkwy	Ridgewood Cres.	Springbank Drive	Secondary Collector	As Existing
Kerr Rd (Z.-1-051390)	Brady Dr	Webber Bourne	Arterial	18 m (59.1 ft)
Kilally Rd (Z.-1-071667)	Thames River	Edgevalley Road	Secondary Collector	10.75 m (35.3 ft)
	Edgevalley Road (Z.-1-071667)	Highbury Ave	Primary Collector	18 m (59.1 ft)
	Highbury Ave N (Z.-1-051390)	Clarke Rd	Arterial	18 m (59.1 ft)
Kilally Rd	Clarke Rd	200 m east of Clarke Road	Proposed Arterial	18 m (59.1 ft)
Killarney Road	Highbury Avenue North	Cedarhallow Boulevard	Secondary Collector	10.75 m (35.3 ft)
King St (Z.-1-99631)	Thames River	Wellington St	Arterial	10 m (32.8 ft)
	Wellington St	Burwell St	Arterial	As Existing
	Burwell St	Ontario St	Arterial	13 m (42.7 ft)
King St (Ext)	Ridout St N	Riverside Dr	Arterial	13 m (42.7 ft)
King Edward Ave	Commissioners Road E	Thompson Rd	Secondary Collector	10.75 m (35.3 ft)
Kingsway Ave	Oakridge Dr	Wonderland Rd N	Secondary Collector	10.75 m (35.3 ft)
Kipps Lane	Adelaide St N	Briarhill Ave	Secondary Collector	10.75 m (35.3 ft)
Knightsbridge Rd	North St	Cranbrook Rd	Secondary Collector	10.75 m (35.3 ft)
Street "L" 39T-02505 (Z.-1-041251)	East Limit of Plan	West limit of plan	Secondary Collector	10.75 m (35.3 ft)
Lawson Road (Z.-1-152382)	Coronation Dr	Wychwood Park (E. Intersection)	Secondary Collector	10.75 m (35.3 ft)
Legendary Drive (Z.-1-061452)	Wharncliffe Road South	Paulpeel Ave	Secondary Collector	10.75 m (35.3 ft)
Lighthouse Rd (added by Z.-1-95310) (Amended by Z.-1-02986)	Reardon Blvd	120 m south of Darnley Blvd	Secondary Collector	10.75 m (35.3 ft)
Little Simcoe St	Adelaide St N	Rectory St	Secondary Collector	10.75 m (35.3 ft)
Limberlost Cres	Limberlost Rd	Homestead Cres.	Secondary Collector	10.75 m (35.3 ft)
Lindisfrne Rd (Z.-1-041203)	Skyline Ave	Sunningdale Rd E	Secondary Collector	10.75 m (35.3 ft)
Longwoods Rd (Z.-1-051390)	W. City Limits	Colonel Talbot Rd	Arterial	18 m (59.1 ft)
Longworth Rd (Z.-1-99705)	Commissioners Rd West	Colonel Talbot Rd	Secondary Collector	10.75 m (35.3 ft)

<b>STREET</b>	<b>FROM</b>	<b>TO</b>	<b>STREET CLASSIFICATION</b>	<b>LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE</b>
Longworth Rd (Z.-1-081703)	Commissioners Rd West	Cranbrook Rd	Secondary Collector	10.75 m (35.3 ft)
Louise Blvd (Z.-1-99719)	Fanshawe Park Road	Plane Tree Drive	Secondary Collector	10.75 m (35.3 ft)
Main Street (Z.-1-051390)	Colonel Talbot Rd	Wharncliffe Rd S	Arterial	18 m (59.1 ft)
Maitland St	Huron St	Regent St	Primary Collector	13 m (42.7 ft)
Manchester Rd (deleted by Z.-1-02986)				
Manchester Rd (Z.-1-95369)	Royal York Rd	Fitzwilliam Blvd	Secondary Collector	10.75 m (35.3 ft)
Manitoulin Dr	Tweedsmuir Dr	Montebello Dr	Secondary Collector	10.75 m (35.3 ft)
Manning Dr (Z.-1-051390)	Wonderland Road South	E City Limits	Arterial	18 m (59.1 ft)
Marconi Blvd (added by Z.-1-95369) (amended by Z.-1-02986)	Marconi Gate	Trafalgar St	Secondary Collector	10.75 m (35.3 ft)
Marconi Gate (Z.-1-95369)	Gore Rd	Marconi Blvd	Secondary Collector	10.75 m (35.3 ft)
McGarrell Drive (Z.-1-99719) (Z.-1-01869) (Z.-1-02986)	Louise Blvd	Limits of traffic circle at Pinnacle Parkway and McGarrell PI	Secondary Collector	10.75 m (35.3 ft)
McGarrell Drive (Z.-1-041203)	Plane Tree Dr	Louise Blvd	Secondary Collector	10.75 m (35.3 ft)
McLean Dr	Glenora Dr	Fanshawe Park Rd	Secondary Collector	10.75 m (35.3 ft)
McMaster Dr	Farnham Rd	Wonderland Rd S	Secondary Collector	10.75 m (35.3 ft)
McNay St	Oxford St E	Fuller St	Secondary Collector	10.75 m (35.3 ft)
Meg Dr	Exeter Rd	Jalna Blvd	Primary Collector	13 m (42.7 ft)
Medway Park 39T-02505 (Z.-1-041251)	East limit of plan	West limit of Plan	Secondary Collector	10.75 m (35.3 ft)
Medway Park Drive (Z.-1-051330)	West limit of plan	East limit of plan	Secondary Collector	10.75 m (35.3 ft)
Melsandra Ave	Briarhill Ave	McNay St	Secondary Collector	10.75 m (35.3 ft)
Merlin Cres	Whitehall dr	Wavell St	Secondary Collector	10.75 m (35.3 ft)
Milestone Rd (deleted by Z.-1-01852)				
Millbank Dr (N. Leg)	Southdale Rd E (West Intersection)	Southdale Rd E (East Intersection)	Secondary Collector	10.75 m (35.3 ft)
	Southdale Rd E	Bradley Ave	Secondary Collector	10.75 m (35.3 ft)
Montebello Dr	Gore Rd	Manitoulin Dr	Secondary Collector	10.75 m (35.3 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Mornington Ave	Quebec St	Oxford E	Secondary Collector	10.75 m (35.3 ft)
Morrison Rd (Z.-1-051390)	Scotland Dr	Wonderland Rd S	Arterial	18m (59.1 ft)
Murray Rd (Z.-1-051390)	Sharon Rd	Westdel Bourne	Arterial	18m (59.1 ft)
Nanette Dr (deleted by Z.-1-041203)				
Nanette Dr (Z.-1-041203)	Jennifer Rd	Chambers Ave	Secondary Collector	As Existing
Newbold St	Bessemer Rd	Adelaide St S	Primary Collector	13 m (42.7 ft)
Nicole Ave (Z.-1-041203)	South Wenige Dr	Sunningdale Rd	Secondary Collector	10.75 m (35.3 ft)
Nightingale Ave	Dundas St	Eliast St	Secondary Collector	10.75 m (35.3 ft)
Nixon Ave	Southdale Rd E	Wilkins St	Primary Collector	13 m (42.7 ft)
North Street	Byron Baseline Rd	Commissioners Rd W	Primary Collector	13 m (42.7 ft)
North Centre Rd (deleted by Z.-1-041203)				
North Centre Rd (Z.-1-041203)	Sunnyside Dr	Richmond St	Secondary Collector	10.75 m (35.3 ft)
North Routledge Park	Hyde Park Rd	Dalmagrry Rd	Secondary Collector	10.75 m (35.3 ft)
North Talbot Community Plan Area (Z.-1-01875)			Secondary Collector	As Existing
North Wenige Dr (Z.-1-02986)	Adelaide St N	Sunningdale Rd N	Secondary Collector	10.75 m (35.3 ft)
Notre Dame Dr	Southdale Rd	Viscount Rd	Secondary Collector	10.75 m (35.3 ft)
Nottingham Rd	Village Green Ave	Commissioners Rd W	Secondary Collector	10.75 m (35.3 ft)
Oakcrossing Gate (Z.-1-061478)	Sarnia Road	Oakcrossing Road	Secondary Collector	10.75 m (35.3 ft)
Oakcrossing Road (Z.-1-061478)	Beaverbrook Ave	Oakcrossing Gate	Secondary Collector	10.75 m (35.3 ft)
Oakridge Dr	Valetta St	Kingsway Ave	Secondary Collector	10.75 m (35.3 ft)
Oak Park Dr	Valetta St	Hickory Rd	Secondary Collector	10.75 m (35.3 ft)
Old Hunt Rd	Oxford St W	Hunt Club Dr	Secondary Collector	10.75 m (35.3 ft)
Old Victoria Rd (Z.-1-051390)	Hamilton Rd	S City Limits	Arterial	18 m (59.1 ft)
Ontario St	Dundas St	King St	Arterial	13 m (42.7 ft)
	Central Ave	Elias St	Secondary Collector	10.75 m (35.3 ft)
Osgoode Dr (W. Leg)	Adelaide St S	Adelaide St S	Secondary Collector	10.75 m (35.3 ft)



STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Outer Dr (Z.-1-051390)	Colonel Talbot Rd	Sunray Ave	Secondary Collector	10.75 m (35.3 ft)
Oxford St E (Z.-1-132184)	Thames River North Branch	Richmond St	Arterial	20 m (65.6 ft)
	Richmond St	Adelaide St N	Arterial	As Existing
	Adelaide St N	Highbury Ave	Arterial	18 m (59.1 ft)
	Highbury Ave	Clarke Rd	Arterial	20 m (65.6ft)
Oxford St W (Z.-1-132184)	Clarke Rd	Crumlin Rd	Arterial	18 m (59.1 ft)
	W City Limits	Hyde Park Rd	Arterial	18 m (59.1 ft)
Pack Road (Z.-1-051390)	Hyde Park Rd	Thames River North Branch	Arterial	20 m (65.6 ft)
	Homewood Lane	Bostwick Rd	Arterial	18 m (59.1 ft)
Page St	Industrial Rd	Crumlin SDRD	Secondary Collector	10.75 m (35.3 ft)
Pall Mall St	Richmond St	Wellington St	Arterial	18 m (59.1 ft)
Parkhurst Ave	Third St	Clarke Side Rd	Secondary Collector	10.75 m (35.3 ft)
Paulpeel Ave (Z.-1-061452) (Z.-1-041270)	Bradley Ave	Bateman Trail	Secondary Collector	10.75 m (35.3 ft)
Pelkey Rd (Z.-1-99719)	Plane Tree Dr	Northerly limit of existing road	Secondary Collector	10.75 m (35.3 ft)
Phillbrook Dr	Fanshawe Park Rd E	Adelaide St N	Secondary Collector	10.75 m (35.3 ft)
Pine St	Egerton St	Hume St	Secondary Collector	10.75 m (35.3 ft)
Pinetree Dr	Hickory Rd	Riverside Dr	Secondary Collector	10.75 m (35.3 ft)
Pine Valley Dr	Viscount Rd	Andover Dr	Secondary Collector	10.75 m (35.3 ft)
Pine Valley Gate	Wonderland Rd S	Pine Valley Dr	Secondary Collector	10.75 m (35.3 ft)
Pinnacle Pkwy (Z.-1-02986)	Ambleside Dr	South limit of traffic circle at McGarrell Dr	Secondary Collector	10.75 m (35.3 ft)
Pinnacle Pkwy (Z.-1-02986)	North limit of traffic circle at McGarrell Dr	South limit of traffic circle at Plane Tree Dr	Secondary Collector	10.75 m (35.3 ft)
Piper Dr (Z.-1-051390)	Westdel Bourne	Murray Rd	Arterial	18 m (59.1 ft)
Platt's Lane	Oxford St W	Western Rd	Primary Collector	13 m (42.7 ft)
Plane Tree Drive (Z.-1-99719) (Z.-1-01869) (Z.-1-02986)	Richmond St	Limits of traffic circle at Plane tree Dr (west limit) and Pinnacle Pkwy	Secondary Collector	10.75 m (35.3 ft)
Plymouth Ave	Sanatorium Rd	Santa Monica Rd	Secondary Collector	10.75 m (35.3 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Pond Mills Rd ( deleted by Z.-1-041203) (Z.-1-98630) (Z.-1-99662)				
Pond Mills Rd (Z.-1-99662)	Pond View Rd	Commissioners Rd E	Arterial	15.5 m (50.9 ft)
	Bradley Ave	Pond View Rd	Arterial	18 m (59.1 ft)
	Commissioners Rd E	Thompson Rd	Primary Collector	13 m (42.7 ft)
Pond Mills Rd (Z.-1-041203)	Wilton Grove Rd	Bradley Ave	Arterial	18 m (59.1 ft)
Pond View Rd	Pond Mills Rd	Glenroy Rd	Secondary Collector	10.75 m (35.3 ft)
Prince Philip Dr	Guildwood Blvd	Hyde Park Rd	Secondary Collector	10.75 m (35.3 ft)
Proudfoot Lane	Beaverbrook Ave	Beaverbrook Ave	Secondary Collector	10.75 m (35.3 ft)
Quarrier Rd (Z.-1-99719) (Z.-1-02986)	Plane Tree Dr	Pelkey Rd	Secondary Collector	10.75 m (35.3 ft)
Quebec St	Dundas St	Queens Ave	Arterial	18 m (59.1 ft)
	Queens Ave	Oxford St E	Primary Collector	13 m (42.7 ft)
Queens Ave	Thames River North Branch	Burwell St	Arterial	As Existing
	Burwell St	Quebec St	Arterial	13 m (42.7 ft) or existing
Quinella Dr	Rosecliffe Ter	Springbank Dr	Secondary Collector	10.75 m (35.3 ft)
Regan Bourne (Z.-1-051390)	Wellington Rd S	Bradish Rd	Arterial	18 m (59.1 ft)
Reardon Blvd (Z.-1-02986)	Lighthouse Rd	Meadowgate Blvd	Secondary Collector	10.75 m (35.3 ft)
Rectory St	Hamilton Rd	Dundas St	Secondary Collector	10.75 m (35.3 ft)
Rectory St (Ext)	Dundas St	Queens Ave	Secondary Collector	10.75 m (35.3 ft)
Regent St	Maitland St	William St	Primary Collector	13 m (42.7 ft)
Repton Ave (Z.-1-041203)	Sandybrook Dr	Phillbrook Dr	Secondary Collector	10.75 m (35.3 ft)
Richmond St (Z.-1-132184)	York St	Fullarton St	Arterial	12.2 m (40 ft)
	Fullarton St	Oxford St	Arterial	13 m (42.7 ft)
	Oxford St	Huron St	Arterial	As Existing
	Huron St	Fanshawe Pk Rd	Arterial	20 m (65.6 ft)
	Fanshawe Pk Rd	N. City Limits	Arterial	18m (59.1 ft)
Ridgewood Cres	Gardenwood Dr	Beachwood Ave	Secondary Collector	10.75 m (35.3 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Ridout St S	Commissioners Rd E	Thames River South Branch	Primary Collector	13 m (42.7 ft)
Ridout St N	Thames River South Branch	Dufferin Ave	Primary Collector	13 m (42.7 ft)
Ridout St N	Dufferin Ave	Blackfriars St	Secondary Collector	10.75 m (35.3 ft)
River Road (OMB R980212 Issued April 15, 1999) (Z.-1-98612)	Gore Road	E City Limits	Arterial	18 m (59.1 ft)  (Measured from centerline which is to be established by an Environmental Assessment Process)
Riverbend Rd (Z.-1-051389)	Kains Road	Shore Road	Secondary Collector	10.75 m (35.3 ft)
Riverbend Road (Z.-1-061483)	Oxford St West	Sumac Way	Secondary Collector	10.75 m (35.3 ft)
Riverside Dr	Sanitorium Rd	Thames River North Branch	Arterial	13 m (42.7 ft) or
Robin's Hill Road (Z.-1-01887)	Crumlin Sideroad	Huron Street	Secondary Collector	10.75 m (35.3 ft)
Robin's Hill Road (Z.-1-01887) (Z.-1-051390)	Huron St	E City Limits	Arterial	18 m (59.1 ft)
Rosecliffe Terrace	Commissioners Rd W	Wonderland Road S	Secondary Collector	10.75 m (35.3 ft)
Roxburgh Rd (Z.-1-051390)	Wilton Grove Rd	Sise Rd	Primary Collector	13 m (42.7 ft)
	Wellington Rd S	Wilton Grove Rd	Arterial	18 m (59.1 ft)
Royal Cres	Wexford Ave	Castle Dr	Secondary Collector	10.75 m (35.3 ft)
Royal York Rd	Oxford St W	Hyde Park Rd	Secondary Collector	10.75 m (35.3 ft)
	Hyde Park Rd	Guildwood Blvd	Secondary Collector	10.75 m (35.3 ft)
St George St	Oxford St W	Cheapside St	Secondary Collector	10.75 m (35.3 ft)
St James St	Talbot St	St George St	Secondary Collector	10.75 m (35.3 ft)
Sandford St	Cheapside St	Kilally Rd	Primary Collector	13 m (42.7 ft)
Sandybrook Dr (Z.-1-041203)	Chambers Ave	Repton Ave	Secondary Collector	10.75 m (35.3 ft)
Sanitorium Road	Boler Rd	Oxford St W	Arterial	18 m (59.1 ft)
Santa Monica Rd	Plymouth Ave	Headley Dr	Secondary Collector	10.75 m (35.3 ft)
Sarnia Rd (Z.-1-051390) (Z.-1-132184)	Hyde Park Rd	Wonderland Rd N	Arterial	18 m (59.1 ft)
	Wonderland Rd N	Western Rd	Arterial	19.5 m (63.98 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Saskatoon St	Wavell St	Dundas St	Primary Collector	13 m (42.7 ft)
Savoy St	Wharncliffe Rd S	North limit of existing road	Secondary Collector	10.75 m (35.3 ft)
Scotland Dr (Z.-1-051390)	Wonderland Rd S	E City Limits	Arterial	18 m (59.1 ft)
Second St	Dundas St	Cheapside St	Primary Collector	13 m (42.7 ft)
Settlement Trail (Z.-1-041203)	Raleigh Blvd	Southerly limit of existing road	Secondary Collector	10.75 m (35.3 ft)
Scanlon St (Z.-1-98612) (OMB File No R98212 – Issue Date April 15, 1999) (amended by Z.-1-02986) (deleted by Z.-1-041203)				
Scanlan St (Z.-1-041203)	River Road (north side, west intersection)	River Road (east intersection)	Secondary Collector	10.75 m (35.3 ft)
Sise Road (Z.-1-051390)	Green Valley Rd	Wilton Grove Rd	Primary Collector	13 m (42.7 ft)
Sharon Rd (Z.-1-051390)	W. City Limits	Longwoods Rd	Arterial	18 m (59.1 ft)
Sholto Dr	Exeter Rd	Jalna Blvd	Secondary Collector	10.75 m (35.3 ft)
Shore Road (Z.-1-031072) (deleted by Z.-1-051389)				
Shore Road (Z.-1-041233)	Kains Road	Westdel Bourne	Secondary Collector	10.75 m (35.3 ft)
Shore Road (Z.-1-051389)	Westdel Bourne	225 metres east of Kains Road	Secondary Collector	10.75 m (35.3 ft)
Skyline Ave (Z.-1-041203)	Chambers Ave	Sunningdale Rd E	Secondary Collector	10.75 m (35.3 ft)
Sleightholme Ave	Sarnia Rd	Wychwood Pk	Secondary Collector	10.75 m (35.3 ft)
Sorrel Rd	Cheapside St	Huron St	Secondary Collector	10.75 m (35.3 ft)
South St	75 E of Colborne St	Adelaide St N	Secondary Collector	10.75 m (35.3 ft)
	Wellington St	75 E of Colborne St	Secondary Collector	As existing
Southcrest Dr	Gradenwood Dr	Emery St W	Secondary Collector	10.75 m (35.3 ft)
Southdale Rd W	Westdel Bourne	Wharncliffe Rd S	Arterial	18 m (59.1 ft)
Southdale Rd E	Wharncliffe Rd S	Pond Mills Rd	Arterial	18 m (59.1 ft)
Southminster Bourne (Z.-1-051390)	W. city limits	Wonderland Rd S	Arterial	18 m (59.1 ft)
South Wenige Dr	Sunningdale Rd E (east intersection)	North Weinge Dr	Secondary Collector	10.75 m (35.3 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Sovereign Road	north of the CPR line	existing limit of Sovereign Road	Secondary Collector	10.75 m (35.3 ft)
Springbank Dr	Commissioners Rd W	Horton St	Arterial	18 m (59.1 ft)
Springmeadow Road	Southdale Rd W	Singleton Ave	Secondary Collector	10.75 m (35.3 ft)
Stackhouse Ave (Z.-1-02986) (deleted by Z.-1-041203)				
Stanley St	Wharnccliffe Rd S	York St	Arterial	13 m (42.7 ft)
Stoneybrook Cres.	Fanshawe Park Rd	Fanshawe Park Rd	Secondary Collector	10.75 m (35.3 ft)
Sunningdale Rd W	W City Limits	Richmond St	Arterial	18 m (59.1 ft)
Sunningdale Rd E	Richmond St	Clarke Rd	Arterial	18 m (59.1 ft)
Street "G"	Sunningdale Rd W	Street "B" (in plan 39T-11503)	Secondary Collector	10.75 m (35.3 ft)
Sunnyside Dr	Richmond St	Doon Dr	Secondary Collector	10.75 m (35.3 ft)
Sunray Ave (Z.-1-051390)	Colonel Talbot Rd	Outer Dr	Secondary Collector	10.75 m (35.3 ft)
Talbot St	York St	Queens Ave	Primary Collector	10.67 m (35 ft)
	Queens Ave	Fullarton Street	Primary Collector	13 m (42.7 ft)
	Fullarton Street	Dufferin Ave	Primary Collector	As Existing
	Dufferin Ave	Oxford St W	Primary Collectore	13 m (42.7 ft)
	Oxford St W	St James St	Secondary Collector	10.75 m (35.3 ft)
Tartan Dr	Aberdeen Drive	Veterans Memorial Parkway	Secondary Collector	As Existing
	Veterans Memorial Parkway (Z.-1-061546)	Neptune Cres.	Secondary Collector	As Existing
Tartan Dr	Railton Ave	Veterans Memorial Parkway	Secondary Collector	As Existing
Tavistock Rd	Village Green Ave	Viscount Rd	Secondary Collector	10.75 m (35.3 ft)
Taylor St	Victoria St	Huron St	Secondary Collector	10.75 m (35.3 ft)
Tempo Rd (Z.-1-051390)	Colonel Talbot Rd	Glanworth Dr	Arterial	18 m (59.1 ft)
Thames St	Dundas St	Horton St	Primary Collector	13 m (42.7 ft)
Third St	Dundas St	Cheapside St	Secondary Collector	10.75 m (35.3 ft)
Thistlewood Dr (Z.-1-02986)	South Wenige Dr	Blackwater Rd	Secondary Collector	10.75 m (35.3 ft)
Thompson Rd	Whetter Ave	Thames River	Primary Collector	13 m (42.7 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Tillmann Rd (Z.-1-02986) (deleted by Z.-1-041203)				
Tillman Rd (Z.-1-041203)	Cranbrook Road	Southerly limit of existing road	Secondary Collector	10.75 m (35.3 ft)
Street "B"	Tokala Trail	North leg at the East limit of plan 39T-11503	Secondary Collector	10.75 m (35.3 ft)
Topping Lane	Commissioners Rd W	Berkshire Dr	Secondary Collector	10.75 m (35.3 ft)
Townsend Dr (Z.-1-02986)	Blackmaple Drive	Highbury Ave N	Secondary Collector	10.75 m (35.3 ft)
Trafalgar St	Hamilton Rd	E City Limits	Arterial	18 m (59.1 ft)
Trossack Ave	Fanshawe Pk Rd	Grenfell Dr	Secondary Collector	10.75 m (35.3 ft)
Trott Dr	Coombs Ave	Platts Lane	Secondary Collector	10.75 m (35.3 ft)
Tweedsmuir Ave	Hale St	Manitoulin Dr	Secondary Collector	10.75 m (35.3 ft)
Upper Queen St	Wilkins St	Commissioners Rd E	Primary Collector	13 m (42.7 ft)
Valetta St	Hyde Park Rd	Oakridge Dr	Secondary Collector	10.75 m (35.3 ft)
Vancouver St	Trafalgar St	Wavell St	Secondary Collector	10.75 m (35.3 ft)
Veterans Memorial Parkway	Highway 401	River Road	Freeway	37.5 m (123 ft)
Veterans Memorial Parkway	River Road	Huron St	Freeway	30 m (98.4 ft)
Veterans Memorial Parkway (Z.-1-13224)	Huron St	Clarke Road	Proposed Freeway	30 m (98.4 ft)
Veterans Memorial Parkway (Z.-1-13223)	Highway 401	Wilton Grove Road	Freeway	30 m (98.4 ft)
Victoria St	Taylor St	Elliot St	Secondary Collector	10.75 m (35.3 ft)
Village Green Ave	Viscount Rd	Tavistock Rd	Secondary Collector	10.75 m (35.3 ft)
Viscount Rd	Cranbrook Rd	Commissioners Rd W	Primary Collector	13 m (42.7 ft)
Waterloo St (Z.-1-132184)	York St	Dundas St	Primary Collector	As Existing
	Dundas St	Dufferin Ave	Primary Collector	As Existing
Wavell St	Brydges St	Dundas St	Primary Collector	13 m (42.7 ft)
Webber Bourne (Z.-1-051390)	Kerr Rd	Highbury Ave	Arterial	18m (59.1 ft)
Webster St	Huron St	Kilally Rd	Secondary Collector	10.75 m (35.3 ft)
Webster St (Z.-1-061473)	Kilally Road	Highbury Ave N	Secondary Collector	10.75 m (35.3 ft)

STREET	FROM	TO	STREET CLASSIFICATION	LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE
Wellington Rd (Z.-1-132184)	Thames River South Branch	Bradley Ave	Arterial	20 m (65.6 ft)
	Bradley Ave	Hwy 401	Arterial	18 m (59.1 ft)
Wellington Rd S	Hwy 401	S. City Limits	Arterial	18 m (59.1 ft)
Wellington St (Z.-1-132184)	Thames River South Branch	York St	Arterial	20 m (65.6 ft)
	York St	Pall Mall St	Arterial	As Existing
Westdel Bourne	Oxford St W	S. City Limits	Arterial	18 m (59.1 ft)
	Kains Rd (Z.-1-041233)	Oxford St W	Primary Collector	18 m (59.1 ft)
Westfield Dr	Beaverbrook Ave	137.5 East of Beaverbrook Ave	Secondary Collector	10.75 m (35.3 ft)
	137.5 East of Beaverbrook Ave	Cherryhill Circle	Secondary Collector	As Existing
	Oxford St E	Kenneth St	Primary Collector	As Existing
Western Rd (Z.-1-021036)	Wharncliffe Rd N	Richmond St	Arterial	15.5 m (50.9 ft)
Westminster Ave	Base Line Rd	Whetter Ave	Primary Collector	13 m (42.7 ft)
Westminster Dr (Z.-1-051390)	W. City Limits	E. City Limits	Arterial	18 m (59.1 ft)
Weston St	Wellington Rd	Fairview Ave	Secondary Collector	10.75 m (35.3 ft)
Wexford Ave	Royal Cres	Admiral Dr	Secondary Collector	10.75 m (35.3 ft)
Wharncliffe Rd S (Z.-1-051390) (Z.-1-132184)	Main St	Commissioners Rd	Arterial	18 m (59.1 ft)
Wharncliffe Rd S (Z.-1-132184)	Commissioners Rd	Riverview Ave	Arterial	19.5 m (63.98 ft)
	Riverview Ave	Thames River	Arterial	19.5 m (63.98 ft)
Wharncliffe Rd N	Thames River	Western Rd	Arterial	18 m (59.1 ft)
Whetter Ave	Wellington Rd	Thompson Rd	Secondary Collector	10.75 m (35.3 ft)
Whisperwood Ave	Griffith St (w. intersection)	Griffith St (e intersection)	Secondary Collector	10.75 m (35.3 ft)
Whitehall Dr	Atkinson Blvd	Merlin Cres	Secondary Collector	10.75 m (35.3 ft)
White Oak Rd	Southdale Rd E	Harry White Dr	Arterial	18 m (59.1 ft)
Wickerson Rd (Z.-1-01933)	Byron Baseline Road	Southdale Rd W	Secondary Collector	10.75 m (35.3 ft)
Wilkins St	<del>Upper Queens St</del>	Wellington Rd	Secondary Collector	10.75 m (35.3 ft)
Wilkins St	Upper Queens St	Wellington Rd	Secondary Collector	10.75 m (35.3 ft)
William St	Regent St	Huron St	Primary Collector	13 m (42.7 ft)

<b>STREET</b>	<b>FROM</b>	<b>TO</b>	<b>STREET CLASSIFICATION</b>	<b>LIMIT OF ROAD ALLOWANCE MEASURED FROM CENTRE LINE</b>
Willow Lane	Dearness Dr	Osgoode Dr	Secondary Collector	10.75 m (35.3 ft)
Willsie Bourne (Z.-1-051390)	Old Victoria Rd	E. City Limits	Arterial	18 m (59.1 ft)
Wilton Grove Rd (deleted by Z.-1-041203)				
Wilton Grove Rd	Roxburgh Rd	E. City Limits	Arterial	18 m (59.1 ft)
Windermere Rd	Ryersie Rd	Western Rd	Secondary Collector	10.75 m (35.3 ft) or as existing
	Western Rd	Adelaide St N	Arterial	18 m (59.1 ft)
Wonderland Rd N (Z.-1-132184)	Thames River	Fanshawe Pk Rd	Arterial	19.5 m (63.98 ft)
	Fanshawe Pk Rd	N. City Limit	Arterial	18 m (59.1 ft)
Wonderland Rd S	S. City Limits	Dingman Dr	Arterial	18 m (59.1 ft)
Wonderland Rd S	Dingman Dr	Southdale Rd W	Arterial	21 m (68.90 ft)
Wonderland Rd S	Southdale Rd W	Thames River	Arterial	19.5 m (63.98 ft)
Woodhull Rd	Oxford St W	S. City Limits	Arterial	18 m (59.1 ft)
Woodward Ave	Riverside Dr	Oxford St W	Secondary Collector	10.75 m (35.3 ft)
Wortley Rd	Commissioners Rd E	York St	Secondary Collector	10.75 m (35.3 ft)
Wychwood Park	Lawson Rd (W. Intersection)	Sleightholme Ave	Secondary Collector	10.75 m (35.3 ft)
York St (Z.-1-132184)	Thames River South Branch	Richmond St	Arterial	13 m (42.7 ft)
	Richmond St	Waterloo St	Arterial	13 m (42.7 ft)
	Waterloo St	Rectory St	Arterial	13 m (42.7 ft)
Street A	Sunningdale Rd	Richmond St	Primary Collector	12.25 m (40 ft)
Street B	Street C	West limit of Plan 39T-04513	Secondary Collector	10.75 m (35.3 ft)
Street C	Street A	Street B	Secondary Collector	10.75 m (35.3 ft)

(deleted and amended by Z.-1-091888)

#### **4.21.1 Road Allowance Requirements at Intersections**

The required minimum right-of-way widths shown in Section 4.21 are the minimum requirements for sections of streets. Additional right-of-way on arterial streets of up to 24 m from the centre line of the street will be required within 150 m of an intersection. (Z.-1-132184)



#### 4.21.2 Sight Triangle at Intersections

The required minimum right-of-way width on any corner lot will include a triangular area bounded by the street lines and a line joining points on the street lines at a distance of no greater than 6 m. (Z.-1-132184)

#### 4.22 LODGING HOUSES

Class 1 Lodging Houses, with a maximum of three lodging units are permitted in any dwelling unit in a detached, semi-detached, duplex, triplex, fourplex, townhouse, or apartment dwelling. (Z.-1-93172)

In any zone which permits a Lodging House Class 2 use, for the purpose of density calculations, a maximum of three (3) lodging units are permitted for each one dwelling unit permitted by density regulations set out in the relevant use zone. (Z.-1-96447)

#### 4.23 REGULATIONS FOR LOW-RISE RESIDENTIAL DEVELOPMENT IN THE PRIMARY TRANSIT AREA

##### a) Area

These regulations apply to development and additions in R1, R2, and R3 Zone variations within the Primary Transit Area identified in *Figure 4.23 Primary Transit Area*".

The Primary Transit Area is generally bounded by the following streets: Fanshawe Park Road to the north; Wonderland Road to the west; Southdale Road (west of White Oak Road) and Bradley Avenue (east of White Oak Road) to the south; Highbury Avenue to the east; Properties on both sides of each boundary street are included within this Primary Transit Area.

##### 4.23.1 Front and Exterior Side Yard Setback

- a) The Maximum Front and Exterior Side Yard setbacks shall be established as follows:
  - i. the average setback of the two (2) closest residential buildings to the subject site oriented to the same street, within the same block, on the same side of the street;
  - ii. where the setbacks of the two (2) closest buildings to the subject site from (i) above differ by 5.0 metres or greater - the average of the four (4) closest residential buildings oriented to the same street, within the same block, on the same side of the street;
  - iii. where the subject site is within a block with fewer than the required number of existing residential buildings from (i) or (ii) above, the average setback of all residential buildings oriented to the same street, within the same block, on the same side of the street;
  - iv. Subsection 4.23.1(a) i, ii and iii shall not apply to additions to existing buildings. (Z.-1-182669)
- b) The Minimum Front and Exterior Side Yard setbacks shall be established as follows:
  - i. The smallest Main Building setback that exists from (i), (ii) or (iii);
  - ii. The minimum setback for a Private Garage shall be 6.0 metres, or the setback of the Main Building, whichever is greater.
  - iii. Notwithstanding 4.23.1(b)i., where an existing building has a front yard setback and/or exterior side yard setback that is less than the adjacent buildings, the existing front and/or exterior side yard setback shall be regarded as the minimum setback that applies to the building. (Z.-1-182669)

#### 4.23.2 Interior Side Yard Setbacks

- a) 1.2 metres; for any portion of the side yard adjacent to a part of the building not exceeding two storeys in height, plus 0.6 metres for each storey or part thereof above two storeys; except that, where no private garage is attached to the dwelling, one side yard shall be 3.0 metres.
- b) Where parking is provided in the side or rear yard, the minimum setback of the opposite side yard may be reduced to a minimum of 0.6 metres for any portion of the side yard adjacent to a part of the building not exceeding two storeys in height, plus 0.6m for each storey or part thereof above two storeys.

#### 4.23.3 Building Depth

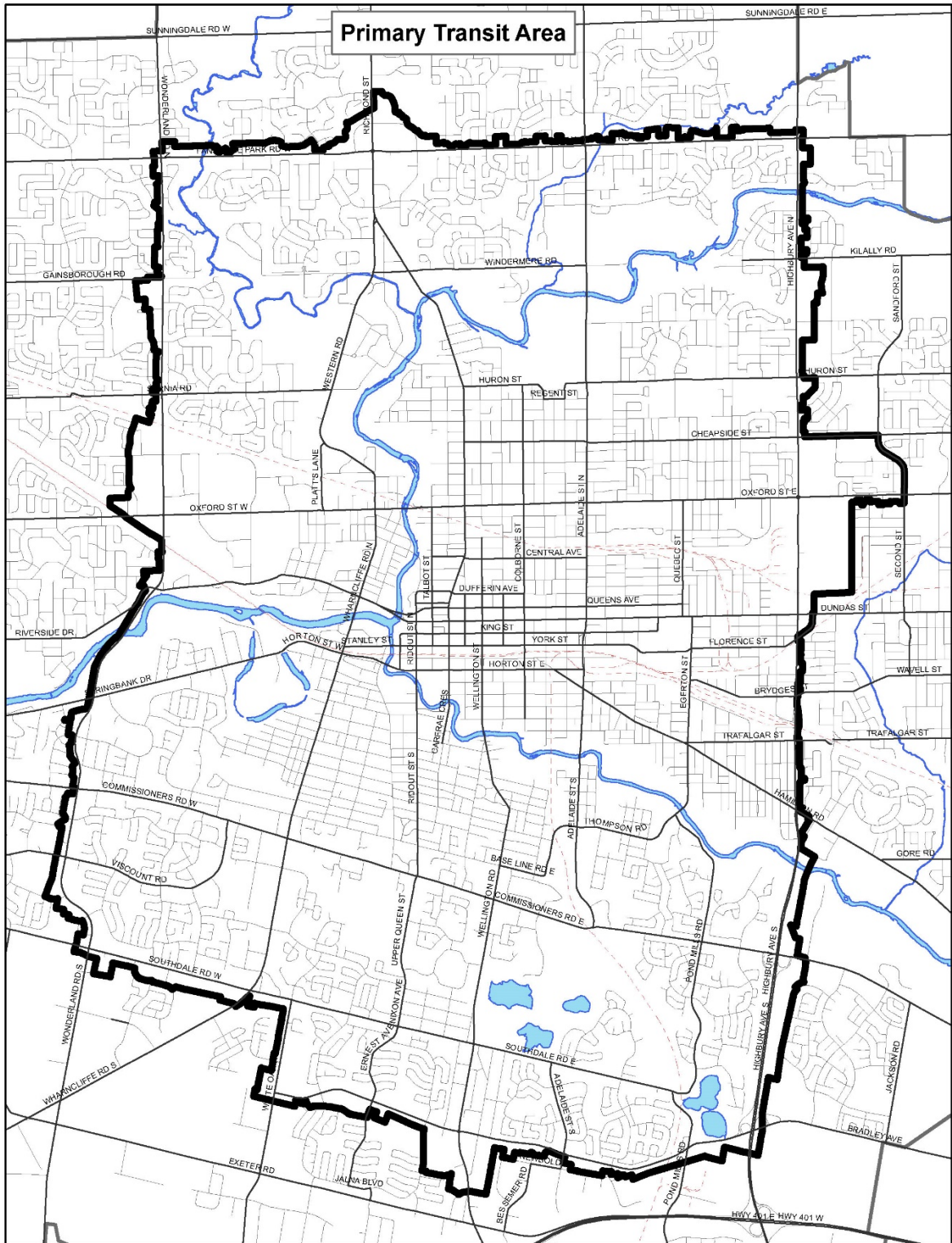
The maximum building depth shall not exceed 60% of the actual lot depth. Minimum rear yard setbacks outlined in Table 5.3, Table 6.3 and Table 7.3 still apply.

#### 4.23.4 Garage Width

The maximum residential garage width (interior walls) shall not exceed 50% of the building façade width.  
(Z.-1-041306) (Z-1-051390) (Z.-1-172575)

- 4.23.5. Notwithstanding 4.23.1, where buildings are constructed on lots fronting onto a new street, the minimum and maximum front yard setback and exterior side yard setback will be established by the underlying zone regulations.” (Z.-1-182669)

Figure 4.23 Primary Transit Area



#### 4.24 SIGHT TRIANGLE

On any corner lot in all zones except the Downtown Area (DA), Business District (BDC) and Arterial Commercial (AC) Zones, no fence or structure shall be erected to a height greater than 1.0 metre (3.3 ft.) above grade of the streets that abut the lot, within the triangular area bounded by the street lines and a line joining points on the street lines at a distance as established by the following:

<b>Interior Angle Formed By Intersection of Street Lines</b>	<b>Distance from Point of Intersection of Street Lines</b>
20 degrees or less	26.0 metres (85.3 feet)
over 20 degrees and up to 30 degrees	18.0 metres (59.1 feet)
over 30 degrees and up to 40 degrees	13.5 metres (44.3 feet)
over 40 degrees and up to 50 degrees	10.5 metres (34.4 feet)
over 50 degrees and up to 60 degrees	9.0 metres (29.5 feet)
over 60 degrees and up to 80 degrees	7.5 metres (24.6 feet)
over 80 degrees and up to 110 degrees	6.0 metres (19.7 feet)

In agricultural zones no driveway, buildings or vegetation greater than .3 m (1 ft) high shall be erected within 9m (29.5 ft) of the intersection of joined streets.

Where a road or street crosses a railway at the same grade then no building or structure shall be erected closer to the point of intersection of the centre line of both railway and the road or street than 45.5m (150 ft)  
(Z-1-051390)

#### 4.25 SWIMMING POOLS

##### 1) PRIVATE OPEN SWIMMING POOLS

Notwithstanding any other provision of this By-Law to the contrary, the following provision shall apply with respect to the erection or use of any private open swimming pool not enclosed or otherwise located within a building:

- a) No private open swimming pools or related structures shall be permitted anywhere within the zoned area except:
  - i) in an interior side yard or rear yard on a lot containing a permitted dwelling; or
  - ii) in any yard other than a required yard on a lot containing a permitted hotel, motel or private club.
- b) No interior wall surface of any open swimming pool shall be located closer than 1.5 metres (4.9 ft.) to any lot line or closer to any street line than the setback required therefrom.  
(Z.-1-98622)
- c) No part of any open swimming pool shall be greater than 2.0 metres (6.6 feet) in height, exclusive of related structures which shall not exceed 4.0 metres (13.1 feet) in height.
- d) Every open swimming pool shall be enclosed in accordance with the City's By-Law governing the erection and maintenance of fences and gates around private outdoor open swimming pools.
- e) No part of any related pool equipment such as diving boards and slides, but excluding pool mechanical equipment, shall be located closer than .6 metres (2.0 feet) to any lot line.  
(Z.-1-98622)

2) LOT COVERAGE EXEMPTION

Notwithstanding any other provisions of this By-Law to the contrary, no open swimming pools or any related structures shall be considered part of the lot coverage of a lot, but where a swimming pool is enclosed with a building or where a structure appurtenant to a swimming pool constitutes a building as defined herein, such building shall comply with any lot coverage requirements set out herein for the zone in which such building is located, but shall be exempt from the lot coverage requirements for accessory uses set out in Subsection 4.1 hereof.

3) INDOOR SWIMMING POOLS

Any swimming pool located within a building as defined herein shall comply with the provisions for accessory uses set out in Subsection 4.1 hereof, where such swimming pool is located within an accessory building, or with the zone requirements set out herein for the zone in which such swimming pool is located, where such swimming pool is located within a main building.

**4.26 USES PERMITTED IN LISTED ZONES**

Notwithstanding any other provisions of this By-Law, the following uses are permitted in the listed zones provided they can meet the specific provisions of this By-law:

<b>Use</b>	<b>Zones Permitted</b>	<b>Governing General Provisions Section</b>
Accessory Uses, Buildigns or Structures	All Zones	4.1
Construction Uses	All Zones	4.5
Foster Homes (Z.-1-051390)	All residential and agricultural zones	4.7
Group Homes Type 1	All residential zones	4.8
Home Occupation reference deleted by Z.-1-94293		
Household Sales	Deleted by Z.-1-97466	
Private Home Day Care	All R1, R2, R3, R4 and R5 Residential Zones, the Agricultural (AG) Zone and the OC1 and OC2 Zone variations	4.1
Z.-1-051390		
Public Uses (OMB Files #R 910387 – Appeal #9006-2 June 4, 1993)	All Zones	See Section 2, Definition for Public Uses
Model Homes (Z.-1-95317)	All R1, R2, R3, R4, R5 and R6 (excluding apartment building) zones	4.5

#### 4.27 YARD ENCROACHMENTS PERMITTED

In residential zones, institutional zones and in all zones which abut a residential zone, the following yard encroachments shall be permitted. In all other zones, there shall be no restrictions on the permitted encroachments.

	<b>Structure</b>	<b>Yard in Which Projection is Permitted</b>	<b>Maximum Projection Permitted into Required Yard Under Zone Regulations</b>
(1)	Steps, open, unenclosed ramps for physically handicapped persons	All	Unlimited
(2)	Window sills, cornices pilasters, cantilevers, eaves, gutters and similar non-structural architectural features	All	0.5 metres (1.6m feet)
(3)	Fire escapes	Side & Rear	1.5 metres (4.9 feet)
(4)	Bay windows, chimneys	All	1.3 metres (4.3 feet) and no closer to any corner of the building than extent of projection into the yard.
(5)	Open or covered but unenclosed decks or porches not exceeding one storey in height	All	3.0 metres (9.8 feet) provided projection is no closer than 1.2 metres (3.9 feet) to lot line, except that where the lot line abuts an OS4 or OS5 Zone the projection shall be no closer than 3.0 metres (9.8 feet) to the lot line. (Z.-1-93196)
(6)	Balconies on apartment buildings	All	1.5 metres (4.9 feet) provided the projection is no closer than 3.0 metres (9.8 feet) to the lot line
(7)	Balconies on other dwellings	Front & Rear	1.5 metres (4.9 feet)
(8)	Underground structure (parking, basements)	All	Unlimited
(9)	Heat pumps, air exchangers, air conditioners and/or pool mechanical equipment. (Z.-1-98622)	All	1.5 metres (4.9 feet) provided the projection is no closer than 0.9 metres (3.0 feet) to the lot line.

#### 4.28 YARD REQUIREMENTS ADJACENT TO STREETS GREATER THAN 40.0 METRES

Notwithstanding the minimum yard requirements of this By-Law, with the exception of garages, where a lot fronts onto a street having a right-of-way width of at least 40.0 metres (131.0 feet), there shall be no front yard requirement, and where a lot flanks such a street there shall be no exterior yard requirement. (Z.-1-97490)

#### 4.29 YARD REQUIREMENTS, EXTERIOR SIDE YARD CONDITION

Notwithstanding the minimum exterior side yard requirements of this By-Law, when a corner lot is sited so that its rear lot line abuts an adjacent interior side yard, the exterior side yard shall be subject to the regulations of a front yard. When a corner lot is sited so that its rear lot line abuts an adjacent rear lot line, the exterior side yard shall be subject to the regulations of an interior side yard.

#### 4.30 YARD REQUIREMENT, REAR YARD TO ARTERIAL ROAD

Notwithstanding the rear yard requirements of this By-Law, with the exception of accessory buildings or structures, where a rear yard abuts an Arterial Street as classified by Section 4.21 (Road Allowance Requirements - Specific Roads), the rear yard requirement shall be increased by 6.0 metres (19.68 feet) except where either a noise attenuation fence or wall is provided. (Z.-1-97490)(Z.-1-97552)

#### 4.31 ANCILLARY SALE OF AUTOMOBILES

Sales of automobiles ancillary to an automobile repair garage shall be limited to a maximum of six (6) automobiles being stored, kept or displayed for sale on the site at anytime. (Z.-1-93150)

#### 4.32 MINIMUM DISTANCE SEPARATION (MDS)

The Minimum Distance Separation guidelines are provided by the Ontario Ministry of Agriculture, Food and Rural Affairs (OMAFRA). The guidelines provide formulas which are used to calculate and establish separation distances between odour producing uses (livestock facilities, manure storages and anaerobic digestors) and urban and sensitive land uses. The setbacks are applied in the Agricultural (AG) Zone, and in other zones such as Rural Settlement Commercial (RRC), Agricultural Commercial (AGC) or Urban Reserve (UR) where there is the potential for agriculturally generate odour impacts. Lands to be rezoned from an Agricultural (AG) or Urban Reserve (UR) Zone to any other zone to permit a residential, facility, commercial, industrial or recreational use will comply with the MDS I formula.

Expansions to existing livestock facilities will need to comply with setbacks calculated under the MDS II formula when they are located close to zones permitting residential, commercial, or other urban and sensitive land uses.

The setbacks calculated under the MDS I formula will apply to development proposed through building permit on existing lots of record only in the following Zone variations: Agricultural (AG) Zone, Rural Settlement Commercial (RRC) zone or Urban Reserve (UR) Zone. The MDS II setbacks shall apply to all lots of record. (deleted and replaced by Z.-1-172557) (deleted and re placed by Z.-1-091877) (Z-1-051390)

#### 4.33 SETBACK REQUIREMENTS ADJACENT TO OIL AND GAS WELLS

The standard setback; consistent with the *Oil, Salt and Gas Resources Act*; for new wells next to existing development or new development next to existing wells is 75m (246 ft.). Variations to this setback can be made through consultation with the City and Province. (Z-1-051390)

#### 4.34 MINIMUM SETBACKS REQUIRED FOR DEVELOPMENT ADJACENT TO RAILWAY LINES ON LANDS ANNEXED TO THE CITY ON JANUARY 1, 1993.

The following regulations apply to all main buildings in the Residential (R1 to R11, OR); Regional (RF), Community (CF) and Neighbourhood Facility (NF); and Open Space (OS1 and OS2) Zone variations that abut a Rail Transportation (RT) Zone.

1. To address safety issues the following regulations apply:

<b>Classification of Track</b>	<b>Setback without Berm</b>	<b>Setback with Berm</b>	<b>Required Size of Berm</b>
Principal Main Line	120 m (394 ft.)	30 m (98.4 ft.)	2.5 m (8.2 ft.)
Secondary Main Line	120 m (394 ft.)	30 m (98.4 ft.)	2.0 m (6.6 ft.)
Principal Branch Line	60 m (197 ft.)	15 m (49 ft.)	2.0 m (6.6 ft.)

2. To address vibration issues, evaluation of ground-borne vibration from rail traffic will be required within 75m (246 ft.) of the railway line and shunting yards.
3. To address noise issues, evaluation of noise from rail traffic through a Noise Impact Study will be required within 120m (394 ft.) of the railway line and shunting yards in accordance with Ministry of the Environment (MOE) guidelines.

Variations to the noise and vibration standards can be made if an Environmental Noise & Vibration Study is submitted and accepted by the General Manager of Planning and Development in support of the request. These reports can be used as a basis for varying the standards where site specific barriers or topographical features are present which may warrant changes. The specific standards can be applied through the development, consent or subdivision agreement.

(Z.-1-051390)

#### **4.35 DRIVE-THROUGH FACILITIES**

The following regulations shall apply to a drive-through facility associated with a permitted use which is adjacent to a residential or facility/institutional use and/or zone:

##### **1) SEPARATION DISTANCE - INTERIOR AND REAR YARD**

The minimum separation distance, measured from the edge of the drive-through lane or speaker location, whichever is closer, to the closest residential/facility/institutional use lot line and/or zone line shall be 30 metres. This setback may be reduced to 15 metres if a 2.4 metre high noise attenuation barrier is installed between the residential/facility/institutional use and the drive-through lane. Further reductions to the setback may be considered upon the City's review and acceptance of mitigation measures identified by a noise study prepared by a qualified noise consultant. A minimum 3 metre wide landscaped strip is required consisting of new and/or existing vegetation immediately adjacent to any noise barrier.

##### **2) SETBACK FROM ULTIMATE ROAD ALLOWANCE**

The City's preferred location for drive-through facilities is in the rear and/or interior side yard. Drive-through facilities may only be permitted in the front and/or exterior side yard if there are no other design alternatives and/or to address safety considerations. If the drive-through facility is located in the front yard, a landscape plan and building elevation plan is required to illustrate a minimum 3 metre landscaped buffer between the edge of the drive-through lane and the ultimate road allowance all to the satisfaction of the Manager of Site Plan Control.

##### **3) STACKING SPACES**

The number of stacking spaces required for uses with drive-through facilities are;

Donut/Coffee Shop	15 spaces
Fast food/Eat-in Restaurant	12 spaces
Service station with convenience sales	9 spaces
Financial institution/Automated Teller	4 spaces

Uses that request a lower stacking lane capacity must submit a queuing study to the City to identify the stacking capacity required. A queuing study is also required when projected volumes are greater than or equal to 60 vehicles/hour.

A minimum of 3 spaces or 16m, whichever is greater, is required between the road right-of-way and the entrance to the stacking lane.



#### 4) PARKING SPACES

Stacking lane spaces are included in the overall parking requirement for the site. (Z.-1-081795)

#### 4.36 CLINIC, METHADONE AND PHARMACY, METHADONE

Notwithstanding any other provision of this by-law, CLINIC, METHADONE or PHARMACY, METHADONE uses shall be permitted solely through amendment to this by-law.

CLINIC, METHADONE or PHARMACY, METHADONE uses shall not be permitted within 300.0 metres (984.3 ft.) of an elementary school, secondary school, municipal library, municipal arena, municipal pool, the Western Fairgrounds or the Boys and Girls Club. This measure shall be taken from property boundary to property boundary.”

CLINIC, METHADONE uses shall require a waiting room area of no less than 15% of the clinic’s total gross floor area. Z.-1-122090

#### 4.37 ADDITIONAL RESIDENTIAL UNITS

The provisions of this section shall apply to all additional residential units, unless specified by type directly herein.

##### 1) Permitted Zones

Additional residential units shall be permitted within any zone in association with the following uses:

- a) Single detached dwellings
- b) Semi-detached dwellings
- c) Street townhouse dwellings

Single detached dwellings, semi-detached dwellings or street townhouse dwellings containing an additional residential unit on the date of the passing of this by-law, may continue to be used for that purpose if a building permit has been issued under sections 8 or 10 of the *Building Code Act, 1992, S.O. 1992, c.23* permitting the erection, alteration, occupancy or use for the additional residential unit, and if the additional residential unit complies with the regulations of the *Fire Protection and Prevention Act, 1997, S.O. 1997, c.4.*

##### 2) Number of Additional Residential Units per Lot

A maximum of two (2) additional residential units shall be permitted per lot; including a maximum of one (1) additional residential unit in the main dwelling and a maximum of one (1) additional residential unit in an accessory or ancillary structure

##### 3) Location of Additional Residential Units

An additional residential unit shall not be permitted on a separate lot from the primary dwelling unit that it is accessory to.

An additional residential unit or part thereof shall not be permitted in a basement where the finished floor level of such basement is below the level of any sanitary sewer servicing the building or structure in which the basement is located.

An additional residential unit shall not be permitted in a flood plain as regulated by the Conservation Authority having jurisdiction for that area.

##### 4) Location of Additional Residential Units within Accessory Structures

An additional residential unit may be permitted in an accessory structure on the same lot as the primary dwelling,

An additional residential unit in an accessory structure shall be required to meet the regulations of the zone which apply to accessory structures.

An additional residential unit within an accessory structure may only be permitted in the rear yard or interior side yard.

#### 5) Floor Area Requirements

The gross floor area of additional residential unit(s) shall not be greater than 40% of the combined total gross floor area of the primary dwelling unit and the additional residential units. For the purposes of calculating gross floor area requirements for additional residential units the following shall not be included:

- a) additions to dwelling units completed after the date of passage of this by-law; and,
- b) the gross floor area of accessory structures, where an accessory structure does not include an additional residential unit.

#### 6) Number of Bedrooms

The additional residential unit(s) and primary dwelling unit together shall not exceed the total number of bedrooms permitted for the primary dwelling unit when the total number of bedrooms in the primary and additional residential unit(s) are combined

#### 7) Access to Additional Residential Units

Exterior alterations to provide for entrances to the additional residential unit within interior or rear yards of the primary dwelling unit may be permitted.

#### 8) Parking

The minimum parking requirement shall be in accordance with the primary dwelling unit. No additional parking is required for additional residential units.

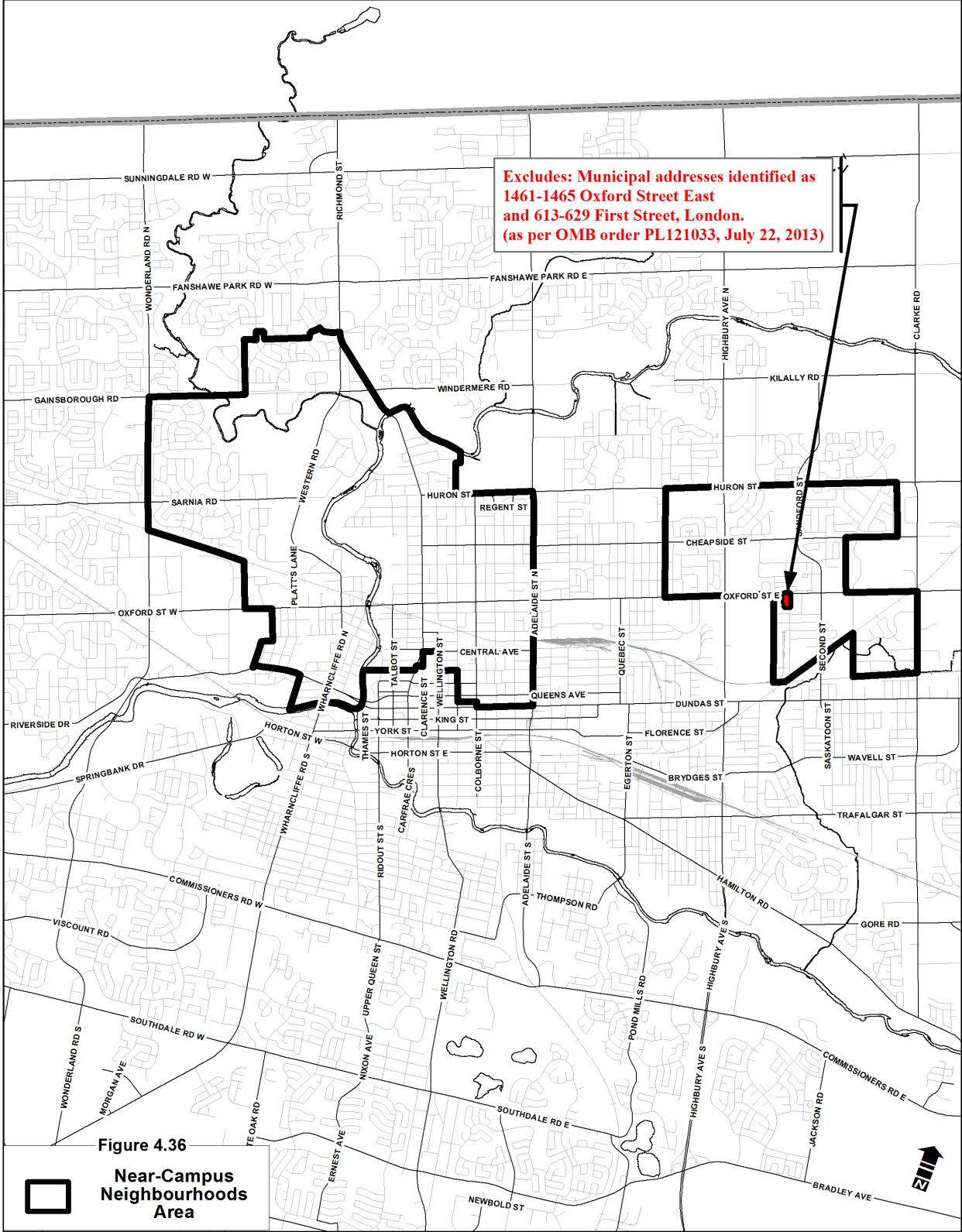
A new additional driveway in association with an additional residential unit is not permitted.

#### 9) Code Requirements

Additional Residential Units shall be required to conform to all Ontario Building Code and Ontario Fire Code regulations.  
(Z.-1-172595) (Z.-1-212896)

FIGURE 4.36

Schedule "A"  
Figure 4.36



(figure replaced by Z.-1-162503)